



WOKINGHAM BOROUGH COUNCIL

A Meeting of the **PLANNING COMMITTEE** will be held
David Hicks 1 - Civic Offices, Shute End, Wokingham RG40
1BN on **WEDNESDAY 8 MAY 2019 AT 7.00 PM**

A handwritten signature in black ink, appearing to read 'Susan Parsonage', with a long, sweeping tail.

Susan Parsonage
Chief Executive
Published on 29 April 2019

This meeting will be filmed for inclusion on the Council's website.

Please note that other people may film, record, tweet or blog from this meeting. The use of these images or recordings is not under the Council's control.



WOKINGHAM BOROUGH COUNCIL

Our Vision

A great place to live, an even better place to do business

Our Priorities

Improve educational attainment and focus on every child achieving their potential

Invest in regenerating towns and villages, support social and economic prosperity, whilst encouraging business growth

Ensure strong sustainable communities that are vibrant and supported by well designed development

Tackle traffic congestion in specific areas of the Borough

Improve the customer experience when accessing Council services

The Underpinning Principles

Offer excellent value for your Council Tax

Provide affordable homes

Look after the vulnerable

Improve health, wellbeing and quality of life

Maintain and improve the waste collection, recycling and fuel efficiency

Deliver quality in all that we do

MEMBERSHIP OF THE PLANNING COMMITTEE

Councillors

Tim Holton (Chairman)	Chris Bowring (Vice-Chairman)	Carl Doran
John Jarvis	Malcolm Richards	Angus Ross
Rachelle Shepherd-DuBey	Wayne Smith	Bill Soane

ITEM NO.	WARD	SUBJECT	PAGE NO.
85.		APOLOGIES To receive any apologies for absence.	
86.		MINUTES OF PREVIOUS MEETING To confirm the Minutes of the Meeting held on 13 March 2019.	5 - 12
87.		DECLARATION OF INTEREST To receive any declaration of interest	
88.		APPLICATIONS TO BE DEFERRED AND WITHDRAWN ITEMS To consider any recommendations to defer applications from the schedule and to note any applications that may have been withdrawn.	
89.	Shinfield South	APPLICATION NO 181499 - LAND SOUTH OF CUTBUSH LANE, SHINFIELD Recommendation: Conditional Approval subject to Legal Agreement.	13 - 108
90.	Winnersh	APPLICATION NO 190198 - TOUTLEY ROAD / OLD FOREST ROAD, WOKINGHAM Recommendation: Conditional Approval.	109 - 144
91.	Winnersh	APPLICATION NO 180753 - TRIDENT HOUSE, 2 PARK LANE STREET, WINNERSH Recommendation: Conditional Approval subject to Legal Agreement.	145 - 184
92.	Wescott	APPLICATION NO 190673 - LUCKLEY HOUSE SCHOOL, LUCKLEY ROAD, WOKINGHAM Recommendation: Conditional Approval.	185 - 222
93.	Winnersh	APPLICATION NO 190233 - LORD HARRIS COURT, MOLE ROAD, SINDLESHAM Recommendation: Conditional Approval subject to Legal Agreement.	223 - 276

Recommendation: Conditional Approval.**Any other items which the Chairman decides are urgent**

A Supplementary Agenda will be issued by the Chief Executive if there are any other items to consider under this heading.

GLOSSARY OF TERMS

The following abbreviations were used in the above Index and in reports.

C/A	Conditional Approval (grant planning permission)
CIL	Community Infrastructure Levy
R	Refuse (planning permission)
LB	(application for) Listed Building Consent
S106	Section 106 legal agreement between Council and applicant in accordance with the Town and Country Planning Act 1990
F	(application for) Full Planning Permission
MU	Members' Update circulated at the meeting
RM	Reserved Matters not approved when Outline Permission previously granted
VAR	Variation of a condition/conditions attached to a previous approval
PS	Performance Statistic Code for the Planning Application
Category	

CONTACT OFFICER

Callum Wernham	Democratic & Electoral Services Specialist
Tel	0118 974 6059
Email	democratic.services@wokingham.gov.uk
Postal Address	Civic Offices, Shute End, Wokingham, RG40 1BN

**MINUTES OF A MEETING OF THE
PLANNING COMMITTEE
HELD ON 13 MARCH 2019 FROM 7.00 PM TO 9.20 PM**

Committee Members Present

Councillors: Tim Holton (Chairman), Chris Bowring (Vice-Chairman), Carl Doran, John Jarvis, Malcolm Richards, Angus Ross, Rachele Shepherd-DuBey and Wayne Smith

Councillors Present and Speaking

Councillors: UllaKarin Clark, John Halsall and Abdul Loyes

Officers Present

Marcia Head, Lead Specialist, Development Management & Compliance
Judy Kelly, Highways Development Manager
Mary Severin, Borough Solicitor
Callum Wernham, Democratic & Electoral Services Specialist

Case Officers Present

Stefan Fludger
Senjuti Manna
Simon Taylor

77. APOLOGIES

An apology for absence was submitted from Councillor Bill Soane.

78. MINUTES OF PREVIOUS MEETING

The Minutes of the meeting of the Committee held on 13 February 2019 were confirmed as a correct record and signed by the Chairman.

MEMBERS' UPDATE

There are a number of references to the Members' Update within these minutes. The Members' Update was circulated to all present prior to the meeting. A copy is attached.

79. DECLARATION OF INTEREST

There were no declarations of interest.

80. APPLICATIONS TO BE DEFERRED AND WITHDRAWN ITEMS

There were no applications recommended for deferral, or withdrawn.

**81. APPLICATION NO 183208 - BROOK HOUSE AND READYPOWER HOUSE,
MOLLY MILLARS LANE**

Proposal: Full application for the proposed change of use from existing mixed use to Sui Generis mixed use (B1c/B2/B8) to include erection of extension to Brook House, installation of solar panels and two silos, engineering excavation work, ancillary parking and demolition of existing ReadyPower building.

Applicant: Philip Glover, Intersurgical.

The Committee received and reviewed a report about this application, set out in agenda pages 13 to 74.

The Committee were advised that the Members' Update included:

- Amended condition 12;
- Replacement of condition 14 (Employment Skills Plan, which was deleted because it instead formed part of the legal agreement) with a condition for a Project Community Liaison Group;
- Clarification of the first sentence of condition 15;
- Amended condition 21;
- Amended condition 24 (incorrectly stated as condition 23 within the Members' Update).

Emily Temple, Agent, spoke in favour of the application. She stated that Intersurgical was the second largest employer within Wokingham. Emily added that the NHS was one of Intersurgical's largest clients, and the increase in their Wokingham headquarters would allow Intersurgical to respond to the NHS's growing needs. Emily stated that the application sought to redevelop the adjacent and currently empty site to allow Intersurgical to expand their manufacturing capacity. Emily added that the site would have ample parking for staff, and that environmental surveys had been conducted as part of the application process.

Angus Ross queried whether condition 24 as stated in the Members' Update should have actually been stated as condition 23. Simon Taylor, Case Officer, confirmed that the 'Doors' condition should be stated as condition 23, as in the original agenda.

Wayne Smith queried whether there had been any comments on the application to date. Simon Taylor stated that as of the day of the Planning Committee, no comments had been received from neighbours or the Town Council. Simon added that should the Committee be minded to approve the application in principle, full planning permission would be granted subject to deferral and delegation of the final decision to the Assistant Director of Place Based Services, following consideration of representations received from neighbour notifications, and subject to a legal agreement.

Members of the Committee commented on their support for a large employment provider in the Borough such as Intersurgical, and welcomed their efforts to expand their Wokingham headquarters in line with planning policy and regulations.

Carl Doran queried why hours of use for delivery by heavy goods vehicles (HGVs) was not conditioned. Simon Taylor clarified that there would be on average four HGV movements per day. Simon added that the HGVs originated from Lithuania, making it hard to predict when they would arrive. Simon stated that there had been no complaints regarding noise from the current HGVs to date.

Tim Holton queried how many of the proposed car parking space would be affected by flooding. Simon Taylor stated that 14 car parking spaces would be situated within flood zone 3B, which equated to a 1 in 15 year flood risk. Simon added that these spaces would be signposted to inform users of the flooding risks, and that there were early warning systems in place with the Environment Agency.

RESOLVED: That application 183208 be approved in principle, subject to deferral and delegation of the final decision to the Assistant Director of Place Based Services, following consideration of representations received from neighbour notifications; conditions and informatives as set out in agenda pages 14 to 24; amended conditions 12, 21, 24 and replaced condition 14 as set out in the Members' Update; and subject to legal agreement.

82. APPLICATION NO 183380 - THE DAY CENTRE, SOUTH LAKE CRESCENT, WOODLEY

Proposal: Full application for the proposed erection of 10no 3 bed semi-detached, affordable dwellings at the Former Age Concern site, South Lake Crescent.

Applicant: Ms Holly Messenger, Wokingham Housing Ltd

The Committee received and reviewed a report about this application, set out in agenda pages 75 to 118.

The Committee were advised that the Members' Update included:

- Amended condition 3(b);
- Amended condition 6;
- Amended condition 10;
- Amended condition 15;
- Amended condition 21;
- New condition 24;
- New informative 6;
- Correction to the final sentence of paragraph 86 of the report;
- Amended recommendation to reflect the requirement for a legal agreement to secure the employment skills plan;
- New informative 7.

David Bragg, Woodley Town Council, spoke in objection to the application. He stated that Woodley Town Council had recommended the application for refusal, based predominantly on parking related issues. David added that the Town Council want to see the current site redeveloped, however they wished for this scheme to include communal parking. David stated that there was no restriction for on street parking within South Lake Crescent and the street parking was currently used by existing residents. David added that the road width of 4.8m at its most narrow point was difficult to allow two large cars to pass, and could restrict emergency vehicles access to the properties if any cars were parked on the street. David stated that there would be an increase in the risk of collision on the road as a result of the proposed dwellings, specifically at the bend of the road where there was a blind spot. David stated that most residents would have to drive to access local amenities, and added that the Town Council would prefer less dwellings on the site with provision for communal parking. David asked the Committee to consider implementing a one way system should they be minded to approve the application.

Martin Few, Architect, spoke in favour of the application. Martin stated that he had worked alongside the applicant (Wokingham Housing Ltd) to redevelop the current site. Martin added that the plans and designs of the proposal had been tweaked throughout the process as a result of feedback and community engagement. Martin stated that the public consultations had been very positive, with most of the concerns being focussed on car parking. Martin added that the proposals, including car parking provisions, were in line with policy and regulations and were in keeping with the street scene.

Abdul Loyes, Ward Member, spoke in favour of the application. Abdul stated that Woodley needed more affordable housing, and was of the opinion that the proposed 3 bedroom semi-detached properties would be a welcome addition to the affordable housing supply within Woodley. Abdul added that he welcomed condition 13 (Off Site Works). Abdul

stated that the public consultation had been very positive, however he acknowledged resident concerns regarding car parking. Abdul stated his support for the proposals, and hoped that the Committee would consider and approve more affordable housing schemes in the future. Abdul asked that the Committee approve the application, whilst taking in to account the concerns of the residents.

Judy Kelly, Highways Development Manager, responded to concerns raised by some of the speakers regarding parking issues. She stated that the parking provision at the proposed development met Wokingham's car parking standards. Judy added that there was currently spare capacity with the on street parking, and the 10 new vehicular access points would help to prohibit parking in this area. Judy stated that the provision of a footway would improve visibility on the street, and added that the 4.8m minimum road width was deemed acceptable and clarified that the road was situated in a quiet location. Judy stated that changing the road to a one way system would be outside of the scope of the application and would not stand up at a subsequent appeal. Judy noted that the site was previously used as a day care centre, which would not have a dissimilar trip rate to that of the proposed application.

Simon Taylor, Case Officer, clarified that the proposed development was within walking distance to the Woodley Town Centre, with a selection of bus stops on the route.

The Committee welcomed the development of high quality affordable housing schemes within the Borough such as this application.

Carl Doran queried why the affordable housing mix was split 60/40 (affordable renting to shared ownership) rather than 70/30 as stated in the core strategy. In response, Simon Taylor stated that the 70/30 split formed a part of the 2010 core strategy, and as such these figures could change over time. Simon added that the percentage split had been assessed by a policy Officer and was deemed acceptable.

RESOLVED: That application 183380 be approved subject to conditions and informatives as set out in agenda pages 75 to 83, amended conditions 3(b), 6, 10, 15 and 21 as set out in the Members' Update, new condition 24 and new informatives 6 and 7 as set out in the Members' Update, and subject to legal agreement as set out in the Members' Update.

83. APPLICATION NO 183289 - 5 HATCHGATE COTTAGES, HATCHGATE LANE, COCKPOLE GREEN

Proposal: Householder application for proposed erection of part single, part two storey side/rear extensions, single storey front extension, plus erection of garden room.

Applicant: Mr and Mrs C & J Copland

The Committee received and reviewed a report about this application, set out in agenda pages 119 to 142.

The Committee were advised that the Members' Update included a correction to the table at paragraph 16.

Adrian Gould, Agent, spoke in favour of the application. He stated that the application had local precedent with other properties having similar works done to their properties, and fall-back position case law. Adrian was of the opinion that common sense needed to be applied to this application, as the applicant already had permission to build large

outbuildings which would cause more harm and leave a bigger footprint on the greenbelt than the current proposals before the Committee.

Chris Copland, Applicant, spoke in favour of the application. He stated that his property had not been extended to date, whereas neighbouring properties had been. He added that his original application was refused due to the lack of a fall-back position, and as a result the application before the Committee was smaller in volume and footprint than the original application and included a valid fall-back position. Chris stated that the current proposals were less disruptive and impactful on the green belt than the fall-back position, however they were fully prepared to implement the fall-back position should the application be refused.

John Halsall, Ward Member, spoke in favour of the application. He stated that the Planning Case Officer had done a good and thorough job with regards to this application. John was of the opinion that in this particular instance the planning policy was an absurdity, which would cause more harm to the green belt. John stated that he was a strong defender of the green belt, however the green belt required families to protect and maintain it. John added that the NPPF stated that harm to the green belt should be accounted for, and if it would be more harmful to apply planning policy to this application then the policy should not be applied. John stated that the current proposals were less harmful to the greenbelt than the approved fall-back position. John was of the opinion that the Case officer shared this opinion, however they were guided by policy in this instance. John urged the Committee to approve the application as it would cause less harm to the greenbelt than the already approved proposals.

Angus Ross queried where the permitted side extension was approved. Stefan Fludger, Case Officer, clarified that the side extension was approved under delegated powers and amounted to a 34% increase in the volume of the original dwelling.

A number of Member queried why the fall-back position would be unlikely to be implemented. Stefan Fludger stated that the permitted development rights outbuildings would need to be fully constructed prior to the commencement of the side extension. He added that it would appear to be impractical to build the rear outbuildings prior to the commencement of the side extension, and added that it would be difficult for the applicant to use the outbuildings for residential use as they were separated from the main dwelling.

Stefan Fludger clarified that if construction began on the side extension prior to the completion of the permitted development rights construction, then the permitted development rights would be revoked. Mary Severin, Borough Solicitor, confirmed this to be correct.

A number of Members queried what constituted very special circumstances. In response, Stefan Fludger stated that a fall-back position constituted very special circumstances, however Officers had only put limited weight on this particular fall-back position, as it was not deemed feasible for the applicant to live across several outbuildings across the site. Stefan added that the question for the decision maker was to decide how likely the fall-back position was to occur.

A number of Members were of the opinion that the fall-back position was feasible, and would cause greater harm to the green belt than the current application.

Chris Bowring was of the opinion that the ideal situation for the green belt was to refuse the application, as Officers had deemed it unlikely that the fall-back position would be implemented, thereby leaving a lesser footprint on the greenbelt.

Marcia Head, Lead Specialist - Development Management & Compliance, stated that if Members were minded to approve the application, the applicant could build the permitted development rights outbuildings and complete them prior to beginning work on the current application. A number of Members queried whether a legal agreement could be implemented whereby the permitted development rights could be revoked should the current application be approved. Marcia stated that a unilateral agreement could be conditioned should the Committee request it, however stated that the applicant could have submitted a unilateral agreement as part of the application but had not.

The Committee sought clarification from the Agent with regards to the unilateral agreement. Adrian Gould stated that a unilateral agreement had been discussed with the Case Officer and that he and the applicant had expected the application to be recommended for approval subject to a unilateral agreement, and commented that the applicant had already agreed to legal fees of £1000 to support this. In Adrian's opinion, there was a late change of direction from Senior Planning Officers and subsequently the application was recommended for refusal without a unilateral agreement.

RESOLVED: That application 183289 be refused, as the proposed extensions would enlarge the volume of the original dwelling beyond 35% within the greenbelt with no very special circumstances existing which would clearly outweigh the harm identified, and the proposals would therefore be contrary to policies CP12, TB01 and section 13 of the NPPF.

84. APPLICATION NO 190421 - THE EMMBROOK SCHOOL, EMMBROOK ROAD, WOKINGHAM, RG41 1JP

Tim Holton relinquished the Chair to Chris Bowring for the duration of this item.

Proposal: Application to vary condition 3 of planning consent 181565 for an artificial grass pitch with flood lights. Condition 3 refers to the hours of operation; condition 3 to be altered to state: 'The facility shall only be used between the hours of 07:30 until 22:00 hours Monday to Friday and 08:00 hours to 20:00 hours on Weekends and Bank Holidays'.

Applicant: Wokingham Borough Council

The Committee received and reviewed a report about this application, set out in agenda pages 143 to 158.

The Committee were advised that the Members' Update included:

- Updated reason for condition 3;
- Amended condition 5;
- New informative 1 in connection with condition 5;
- Additional representation received;
- Correction to paragraph 22;
- Appended minutes of the Planning Committee meeting dated 10 October 2018, agenda item 43.

Craig Hunter, on behalf of the Applicant, spoke in favour of the application. He stated that the project would boost the attractiveness of the school to pupils of years 8 to 11, by

providing better facilities and reducing the usage of the currently boggy grassed areas. He added that an increase in pupil admission numbers as a result of the increased attractiveness of the school would result in an increase in funding for the school. Craig stated that the development would allow staff to increase their own health and wellbeing and would allow the school to host tournaments for the community. Craig added that the development would make the school a hub of the community and would improve the scope of student opportunities.

UllaKarin Clark, Ward Member, spoke in favour of the application. She stated that the 3G football pitch would be a wonderful addition to the Emmbrook School and would allow for local football clubs to train on the site. UllaKarin added that the development would provide a huge benefit for the local community, allowing the school to host tournaments and improve the health of its staff, students and wider community. Ullakarin stated that there would be strong management strategies in place on the site, and that should the hours of use not be extended then the business case would be affected which would in turn remove the funding for the project. UllaKarin stated that all users of the site would have to sign a code of conduct prior to usage of the facilities, which would allow management to remove offenders. Ullakarin added that acoustic barriers and padding could be used in the future as a noise prevention strategy, if it was deemed necessary.

In response to the point raised regarding acoustic barriers, Senjuti Manna, Case Officer, stated that they did not form a part of this application however the applicant could implement them in the future if it was deemed necessary.

Rachelle Shepherd-DuBey raised concerns over the noise management plan, as it had not yet been submitted. Senjuti Manna stated that the noise management plan needed to be detailed and approved before development was first used, and clarified that this was conditioned.

Senjuti Manna clarified that noise levels had been monitored from 10m behind the goal line. The computer model had been created using this data and had suggested that the nearest residential properties were 55m and 65m behind the goal lines and would not be subject to unacceptable noise levels.

Angus Ross stated that since the previous application came to Committee, a much more detailed noise impact assessment had been carried out as well as a detailed management plan. He added that the scheme was now far more robust and would better serve the community.

Carl Doran queried why acoustic barriers were not part of the application. Senjuti Manna clarified that this would require further planning permission, however this could be conditioned within the noise management plan if substantiated complaints relating to noise above 50 decibels had been received and verified by a noise monitoring Officer.

RESOLVED: That application 190421 be approved, subject to the conditions as set out in agenda pages 144 to 146, updated condition 3 and amended condition 5 as set out in the Members' Update, and new informative 1 as set out in the Members Update.

Tim Holton resumed the Chair.

This page is intentionally left blank

Agenda Item 89.

Application Number	Expiry Date	Parish	Ward
181499	EXT	Shinfield	Shinfield South

Applicant	Bellway Homes and the University of Reading
Site Address	Land South of Cutbush Lane, Shinfield, RG2 9AG
Proposal	Full planning application for the erection of 249 dwellings, new public open space, landscaping, surface water attenuation, access and associated works at land to the south of Cutbush Lane Shinfield.
Type	Full
PS Category	1
Officer	Christopher Howard
Reason for determination by committee	Major application

FOR CONSIDERATION BY	Planning Committee on 8/5/19
REPORT PREPARED BY	Assistant Director Delivery & Infrastructure

SUMMARY
<p>The application is for full planning application for the erection of 249 dwellings, new public open space, landscaping, surface water attenuation, access and associated works at land to the south of Cutbush Lane Shinfield. The site was allocated for housing as part of the housing allocation for the South of the M4 Strategic Development Location (SDL) and was identified in the Spatial Framework Plan in the adopted Supplementary Planning Document (SPD). In total, 375 units were identified for north and south of Cutbush Lane and 126 units have been delivered / under construction to the north of Cutbush Lane at Littlebrook and the balance of the proposed 249 makes up the SPD allowance.</p> <p>It is important to note that there was a timing restriction for the land put forward for the Shinfield West appeal by the University of Reading. This had a restrictive clause within the Unilateral Undertaking to not develop on land south of Cutbush Lane until 2026. The council has however removed this clause and agreed to release the land through an Executive Decision on the 27th of July 2017 which is appended to the report. This also identified land south of the current development parcel to the north of Arborfield Road for housing to be delivered along the entire western edge of the Eastern Relief Road (ERR). The reason for this was to ensure comprehensive and well planned development of the area. This will help the council to continue to meet and exceed its requirement for a 5 year housing land supply and this should help to limit speculative sites coming forward. In addition, this allows for a comprehensive approach to the delivery of housing on the eastern side of Shinfield and an application is expected for the residual land to the north of Arborfield Road in 2019.</p> <p>The overarching vision of the SDL is to provide a co-ordinated approach to the delivery of infrastructure and services ensuring that developments are of a high quality and are sustainable. This includes the provision of schools, community facilities, good quality open space and appropriate local transport and links.</p> <p>The application includes heads of terms for a S106 agreement to deliver further on site and off site infrastructure including 35% affordable housing and SANG off-site. A S106</p>

legal agreement will secure the proportion of infrastructure attributable to the development and identify suitable triggers for delivery.

The principle of development in this location has been established through its allocation by policy CP19 of the Core Strategy and through the Spatial Framework Plan within the South of the M4 Supplementary Planning Document. In addition, with the exception of the pumping station and public open space, the site is located within the development limits as identified in Policy 1 of Shinfield Parish Neighbourhood Plan.

The application is before the Planning Committee as it is a major development that is recommended for approval. It is considered that the development would be a sustainable urban extension to the existing settlement whereby residents would have good access to services and facilities. The development would not have a significant detrimental impact on the character of the area or on existing residents.

In regard to access, the applicant has worked with the council to ensure that this is delivered predominantly from the roundabout adjacent to the Science Park through the Littlebrook development site located to the north of Cutbush Lane. This approach means that a further access point is not required from the ERR and since the road within the Littlebrook estate has been designed to be 6.1m wide to accommodate buses, the access strategy is acceptable.

The proposal would provide for an adequate level of amenity for the future occupants of the dwellings together with an acceptable impact on ecology, traffic, highway safety and flood risk in addition to contributing to the delivery of infrastructure provided jointly by the wider SDL such as the Eastern Relief Road.

In design terms, the proposal meets all the standards and in particular internal space and parking. The proposal is considered to be acceptable, represents good design. Therefore the proposals are in accordance with adopted policy and standards and it is recommended that the application is approved subject to conditions outlined below and completion of a legal agreement to secure the necessary infrastructure. An alternative recommendation for refusal is included should a legal agreement not be secured which is set out below.

In terms of background, the application was deferred at the February Planning Committee as an application for the Langley Mead SANG extension was refused by Members. At this point in time, the current application had to be deferred as there was no SANG provision to support the houses. Following a review of existing consented SANG, there is sufficient provision at the Ridge SANG to support the housing (see paragraphs 83-34 below). On this basis, the application is acceptable in regard to the Thames Heath SPA mitigation.

It should also be noted that there has been some enabling works on the site since the deferral of the application at Planning Committee in February. This has involved tree felling and landscape works to existing hedgerows. As the trees are not covered by a Tree Preservation Order or Conservation Area, works on these do not require consent by the Local Planning Authority. The landscaping works were however overseen by an ecologist and our compliance officer visited the site on several occasions. All trees that are shown to be kept on the plans under consideration have also been retained.

PLANNING STATUS

- Strategic Development Location (SDL) as identified on the Core Strategy
- South of the M4 Strategic Development Location SPD
- Infrastructure Delivery and Contributions SPD
- Countryside
- Within 5-7km of the Special Protection Area (SPA)
- Mineral consultation zone

RECOMMENDATION

That the committee authorise the GRANT OF PLANNING PERMISSION subject to the following:

A) Completion of a legal agreement to secure the following:

- i. Highway improvements/upgrades including the Magpie and Parrot roundabout (southern roundabout) on the Eastern Relief Road.
- ii. Footpath improvements both on and off site
- iii. Contributions for South of the M4 Bus Strategy
- iv. Contributions for My Journey
- v. Provision of off-site sports facilities
- vi. Provision of play space
- vii. 35% Affordable housing provision on site with a payment in lieu of 0.15 dwelling
- viii. Employment skills plan/contribution
- ix. Maintenance sums
- x. SAMM contributions
- xi. Monitoring Fees
- xii. SANG Management plan to be agreed through the S106 for the Ridge SANG

B) Conditions and informatives:

Timeframe for implementation

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason: In pursuance of s91 of the Town and Country Planning Act 1991 (as amended by s51 of the Planning and Compensation Act 2004).

Plans

2. This permission is in respect of the plans listed in the table below and the development hereby permitted shall be carried out in accordance with the approved details unless otherwise agreed in writing by the Local Planning Authority.

Architect Drawings

Document	Document Ref	Revision
Presentation Planning Layout	041611-BEL-TV-01	D
Supporting Layout	041611-BEL-TV-02	D

Storey Height Layout	041611-BEL-TV-03	C	
Tenure Layout	041611-BEL-TV-04	D	
Unit Type Layout	041611-BEL-TV-05	C	
Location Plan	041611-BEL-TV-06	/	
Materials Layout	041611-BEL-TV-07	D	
Perspective 01	041611-PER01	A	
Perspective 02	041611-PER02	A	
Perspective 03	041611-PER03	A	
Perspective 04	041611-PER04	A	
Street Scene 02	041611-SS02	A	
Street Scene 03	041611-SS03	A	
Street Scene 04	041611-SS04	A	
Street Scene 05	041611-SS05	A	
Street Scene 06	041611-SS06	A	
Street Scene 07	041611-SS07	A	
The Potter – Brick Elevations	PO-2B-2S-TB-E	/	
The Potter – Feature Brick Elevations	PO-2B-2S-TF-E	/	
The Potter – Tile Elevations	PO-2B-2S-TT-E	/	
The Potter – Floor Plans	PO-2B-2S-P1	/	
The Turner – Feature Brick Elevations	TU-3B-2S-TF-E	/	
The Turner – Tile Elevations	TU-3B-2S-TT-E	/	
The Turner – Floor Plans	TU-3B-2S-P1	/	
The Thatcher – Tile Elevations	TH-3B-2S-TT-E	/	
The Thatcher – Floor Plans	TH-3B-2S-P1	/	
The Quilter – Brick Elevations	QU-3B-2S-TB-E	/	
The Quilter – Feature Brick Elevations	QU-3B-2S-TF-E	/	
The Quilter – Floor Plans	QU-3B-2S-P1	/	
The Mason – Brick Elevations	MA-3B-2S-TB-E	/	
The Mason – Feature Brick Elevations	MA-3B-2S-TF-E	/	
The Mason – Floor Plans	MA-3B-2S-P1	/	
The Scrivener – Brick Elevations	SC-4B-2S-TB-E	/	

The Scrivener – Feature Brick Elevations	SC-4B-2S-TF-E	/	
The Scrivener – Floor Plans	SC-4B-2S-P1	/	
The Scrivener – Tile Elevations	SC-4B-2S-TT-E	/	
The Scrivener – Floor Plans	SC-4B-2S-P2	/	
The Milliner – Tile Elevations	MI-4B-2S-TT-E	/	
The Milliner – Floor Plans	MI-4B-2S-P1	/	
The Bowyer – Tile Elevations	BO-4B-2S-TT-E	/	
The Bowyer – Floor Plans	BO-4B-2S-P1	/	
The Weaver – Feature Brick Elevations	WE-4B-2S-TF-E	/	
The Weaver – Tile Elevations	WE-4B-2S-TT-E	/	
The Weaver – Floor Plans	WE-4B-2S-P1	/	
H20 – Tile Elevations	041611-H20-TT-E	/	
H20 – Floor Plans	041611-H20-P1	/	
H30 – Brick Elevations	041611-H30-TB-E	/	
H30 – Feature Brick Elevations	041611-H30-TF-E	/	
H30 – Tile Elevations	041611-H30-TT-E	/	
H30 – Floor Plans	041611-H30-P1	/	
Apartment Block 4 - Elevations sheet 1	041611-B4-E1	A	
Apartment Block 4 - Elevations sheet 2	041611-B4-E2	A	
Apartment Block 4 - Ground Floor Plan	041611-B4-P1	A	
Apartment Block 4 - First Floor Plan	041611-B4-P2	A	
Apartment Block 4 - Second Floor Plan	041611-B4-P3	A	
Apartment Block 1 - Elevations sheet 1	041611-B1-E1	A	
Apartment Block 1 - Elevations sheet 2	041611-B1-E2	A	
Apartment Block 1 - Ground Floor Plan	041611-B1-P1	A	
Apartment Block 1 - First Floor Plan	041611-B1-P2	A	
Apartment Block 1 - Second Floor Plan	041611-B1-P3	A	
Apartment Block 2 – Elevations sheet 1	041611-B2-E1	A	
Apartment Block 2 – Elevations sheet 2	041611-B2-E2	A	
Apartment Block 2 – Ground Floor Plan	041611-B2-P1	A	
Apartment Block 2 – First Floor Plan	041611-B2-P2	A	

Apartment Block 2 – Second Floor Plan	041611-B2-P3	A	
Apartment Block 3 – Elevations sheet 1	041611-B3-E1	A	
Apartment Block 3 – Elevations sheet 2	041611-B3-E2	A	
Apartment Block 3 – Elevations sheet 2	041611-B3-E3	/	
Apartment Block 3 – Ground Floor Plan	041611-B3-P1	A	
Apartment Block 3 – First Floor Plan	041611-B3-P2	A	
Apartment Block 3 – Second Floor Plan	041611-B3-P3	A	
The Potter – Brick Elevations	PO-2B-2S-CB-E	/	
The Potter – Floor Plans	PO-2B-2S-P1	/	
The Saddler – Brick Elevations	041611-SA-CB-E	/	
The Saddler – Floor Plans	041611-SA-P1	/	
The Turner – Brick Elevations	TU-3B-2S-CB-E	/	
The Turner – Tile Elevations	TU-3B-2S-CT-E	/	
The Turner – Floor Plans	TU-3B-2S-P1	/	
The Thatcher – Tile Elevations	TH-3B-2S-CT-E	/	
The Thatcher – Render Elevations	041611-TH-CR-E	/	
The Thatcher – Floor Plans	TH-3B-2S-P1	/	
The Quilter – Brick Elevations	QU-3B-2S-CB-E	/	
The Quilter – Floor Plans	QU-3B-2S-P1	/	
The Mason – Brick Elevations	MA-3B-2S-CB-E	/	
The Mason – Tile Elevations	MA-3B-2S-CT-E	/	
The Mason – Floor Plans	MA-3B-2S-P1	/	
The Scrivener – Brick Elevations	SC-4B-2S-CB-E	/	
The Scrivener – Floor Plans	SC-4B-2S-P1	/	
The Scrivener – Render Elevations	041611-SC-CR-E	/	
The Scrivener – Tile Elevations	SC-4B-2S-CT-E	/	
The Scrivener – Floor Plans	SC-4B-2S-P2	/	
The Milliner – Brick Elevations	MI-4B-2S-CB-E	/	
The Milliner – Floor Plans	MI-4B-2S-P1	/	
The Bowyer – Render Elevations	041611-BO-CR-E	/	
The Bowyer – Floor Plans	041611-BO-P1	/	

The Philosopher – Brick Elevations	PH-4B-2S-CB-E	/	
The Philosopher – Tile Elevations	PH-4B-2S-CT-E	/	
The Philosopher – Floor Plans	PH-4B-2S-P1	/	
The Weaver – Tile Elevations	WE-4B-2S-CT-E	/	
The Weaver – Floor Plans	WE-4B-2S-P1	/	
The Weaver – Render Elevations	041611-WE-CR-E	/	
The Weaver – Floor Plans	041611-WE-P1	/	
H20 – Brick Elevations	041611-H20-CB-E	/	
H20 – Floor Plans	041611-H20-P1	/	
H30 – Brick Elevations	041611-H30-CB-E	/	
H30 – Tile Elevations	041611-H30-CT-E	/	
H30 – Floor Plans	041611-H30-P1	/	
Carport Type 01 – Elevations & Plan	041611-CP01	A	
Carport Type 02 – Elevations & Plan	041611-CP02	/	
Carport Type 03 – Elevations & Plan	041611-CP03	/	
Garage Type 01 – Elevations & Plan	041611-GAR01	/	
Garage Type 02 – Elevations & Plan	041611-GAR02	/	
Garage Type 03 – Elevations & Plan	041611-GAR03	/	
Garage Type 04 – Elevations & Plan	041611-GAR04	A	
Garage Type 05 – Elevations & Plan		/	
Sub Station – Elevations & Plan	041611-SUB01	/	
Cycle Shed 01 – Elevations & Plan	041611-SH01	/	
Cycle Shed 02 – Elevations & Plan	041611-SH02	/	

Engineering Drawings

Document	Document Ref	Revision	
General Arrangement Sheet 1	A106-RM-11	P6	
General Arrangement Sheet 2	A106-RM-12	P6	
Street Lighting Plan Sheet 1	A106-RM-15	P3	
Street Lighting Plan Sheet 2	A106-RM-16	P3	

Long Sections Sheet 1	A106-RM-21	P3	
Long Sections Sheet 2	A106-RM-22	P3	
Long Sections Sheet 3	A106-RM-23	P3	
Long Sections Sheet 4	A106-RM-24	P3	
Long Sections Sheet 5	A106-RM-25	P3	
Long Sections Sheet 6	A106-RM-26	P3	
Materials Plan Sheet 1	A106-RM-31	P6	
Materials Plan Sheet 2	A106-RM-32	P6	
Drainage Layout Sheet 1	A106-RM-51	P5	
Drainage Layout Sheet 2	A106-RM-52	P5	
Drainage Details Sheet 1	A106-RM-57	P6	
Drainage Details Sheet 2	A106-RM-58	P6	
Engineering Layout Sheet 1	A106-RM-61	P6	
Engineering Layout Sheet 2	A106-RM-62	P6	
Highway Construction Details Sheet 1	A106-RM-71	P2	
Highway Construction Details Sheet 2	A106-RM-72	P2	
Fire Tender Vehicular Tracking Sheet 1	A106-RM-81	P6	
Fire Tender Vehicular Tracking Sheet 2	A106-RM-82	P6	
Large Refuse Vehicular Tracking Sheet 1	A106-RM-83	P6	
Large Refuse Vehicular Tracking Sheet 2	A106-RM-84	P6	
Bus Vehicular Tracking Sheet 1	A106-RM-85	P5	
Bus Vehicular Tracking Sheet 2	A106-RM-86	P5	
Site Access Arrangement onto Cutbush Lane – General Arrangement	A106-S278-101	P5	

Landscape

Document	Document Ref	Revision	
ASP1 Location Plan	6454.SLP.01	A	
ASP2 Site & Setting Plan	6454.SSP.02	A	
ASP3 Landscape Designation Plan	6454.LDP.ASP3	A	

ASP4 Landscape Character Plan	6454.LCP.ASP4	A	
ASP5 Landscape Strategy Plan for info only subject to further refinement for conditions discharge	6454.LSP.ASP5	H	
ASP6.0 Planting Plan - Overview	6454.PP.ASP6.0	B	
ASP6.1 Planting Plan – 1 of 7	6454.PP.ASP6.1	B	
ASP6.2 Planting Plan – 2 of 7	6454.PP.ASP6.2	B	
ASP6.3 Planting Plan – 3 of 7	6454.PP.ASP6.3	B	
ASP6.4 Planting Plan – 4 of 7	6454.PP.ASP6.4	B	
ASP6.5 Planting Plan – 5 of 7	6454.PP.ASP6.5	B	
ASP6.6 Planting Plan – 6 of 7	6454.PP.ASP6.6	B	
ASP6.7 Planting Plan – 7 of 7	6454.PP.ASP6.7	B	
ASP7 Play Space Details Plans	6454.PS.ASP7	D	
ASP8.0 Hard Surface Plan - Overview	6454.HSP.ASP8.0		
ASP8.1 Hard Surface Plan – 1 of 7	6454.HSP.ASP8.1		
ASP8.2 Hard Surface Plan – 2 of 7	6454.HSP.ASP8.2		
ASP8.3 Hard Surface Plan – 3 of 7	6454.HSP.ASP8.3		
ASP8.4 Hard Surface Plan – 4 of 7	6454.HSP.ASP8.4		
ASP8.5 Hard Surface Plan – 5 of 7	6454.HSP.ASP8.5		
ASP8.6 Hard Surface Plan – 6 of 7	6454.HSP.ASP8.6		
ASP8.7 Hard Surface Plan – 7 of 7	6454.HSP.ASP8.7		
ASP9 Illustrative Cross Sections	6454.IC.S.ASP9		

Trees

Document	Document Ref	Revision
Arboricultural Impact (Tree Loss) Assessment	CC37-1035	
Tree Protection Plan & Arboricultural Method Statement	37-1035.02 Sheets 1 and 2	

Reason: to ensure that the development is carried out in accordance with the application form and associated details hereby approved.

Phasing

3. Prior to the commencement of development a strategy for the sub-phasing of the development hereby approved shall be submitted to and approved in writing by the Local Planning Authority. The Phasing Strategy will define:
- i. the development to be delivered within each sub-phase of the development;
 - ii. timescales;
 - iii. details of the coordination of housing and infrastructure delivery including triggers for delivery of infrastructure and the arrangements to prevent interruption of delivery across sub-phase and phase boundaries;

Development shall be carried out in accordance with the approved Phasing Strategy.

Reason: to ensure comprehensive planning of the site, to ensure the timely delivery of facilities and services and to protect the amenity of the area in accordance with Wokingham Borough Core Strategy Policies CP1, CP2, CP3, CP4, CP5, CP6, CP17 and CP19 and the South of the M4 Supplementary Planning Document (October 2011).

Reason for imposing as a pre-commencement condition: The phasing of the works needs to be understood to ensure that services are provided and in order to ensure the comprehensive delivery of the site.

Materials / boundary treatments

4. Prior to residential development above finished floor level, samples and details of the materials to be used in the construction of the external surfaces of the buildings shall have first been submitted to and approved in writing by the Local Planning Authority. Development shall be carried out in accordance with the approved details.

*Reason: To ensure that the external appearance of the buildings are satisfactory.
Relevant Policies: Core Strategy policies CP1 and CP3.*

Reason for imposing as a pre-commencement condition: Flexibility has been built in here to enable groundworks although materials will need to be agreed ahead of housing delivery in the interests of visual amenity.

Permitted development

5. Notwithstanding the provisions of the Town and Country Planning, (General Permitted Development) Order 1995 (as amended) (or any Order revoking and re-enacting that Order with or without modification), no additional windows or similar openings shall be constructed in the first floor level or above in any elevation of the buildings hereby permitted except for any which may be shown on the approved drawing(s).

*Reason: To safeguard the residential amenities of neighbouring properties.
Relevant Policies: Core Strategy policies CP2, CP3 and Design Guide Supplementary Planning Document.*

6. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 1995 (as amended) (or any Order revoking and re-enacting that Order with or without modification), no buildings, extensions or alterations permitted by Classes A, B, C, D, and E of Part 1 of the Second Schedule of the 1995 Order (or any order revoking and re-enacting that order with or without modification) shall be carried out.

Reason: To safeguard the amenities of the occupiers of neighbouring properties, preserve the character of the settlement and to ensure the proper planning of the area, in accordance with Wokingham Borough Core Strategy Policies CP1 and CP3 and the Wokingham Borough Council Design Supplementary Planning Document (February 2010).

7. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 1995 (as amended) (or any Order revoking and re-enacting that Order with or without modification), no external lighting shall be installed on the site or affixed to any buildings on the site except within rear gardens and front door lamps or in accordance with details that have first been submitted to and approved in writing by the Local Planning Authority.

Reason: To safeguard amenity and highway safety. Relevant Policies: Core Strategy policies CP1, CP3 and CP6.

8. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) (England) Order 2015 (or any Order revoking and re-enacting that Order with or without modification), the garage accommodation on the site identified on the approved plans shall be kept available for the parking of vehicles ancillary to the residential use of the site at all times. It shall not be used for any business nor as habitable space.

Reason: To ensure that adequate parking space is available on the site, so as to reduce the likelihood of roadside parking, in the interests of highway safety and convenience. Relevant policy: Core Strategy policy CP6 and Managing Development Delivery Local Plan policy CC07.

Highways

9. Prior to the first occupation of any dwelling, a Parking Management Strategy for the management of on-street parking shall be submitted to and approved in writing by the local planning authority. The development shall be implemented in accordance with the agreed strategy thereafter.

Reason: to ensure satisfactory development and in the interests of sustainable travel in accordance with Wokingham Borough Core Strategy Policies CP1, CP6 and CP19.

10. No part of any dwelling(s) hereby permitted shall be occupied until the vehicle parking and turning space serving that dwelling has been provided in accordance with the approved plans, unless otherwise agreed in writing by the Local Planning Authority. The vehicle parking shall not be used for any other purpose other than parking and the turning space shall not be used for any other purpose other than turning vehicles.

Reason: To ensure adequate on-site parking provision in the interests of highway safety, convenience and amenity. Relevant policy: Core Strategy policies CP3 & CP6 and Managing Development Delivery Local Plan policy CC07.

11. Prior to the first occupation of any dwelling within the development, a Walking and Cycling Strategy for the whole development shall be submitted to and approved in writing by the Local Planning Authority. The development shall be implemented in accordance with the agreed strategy thereafter.

Reason: to ensure satisfactory development in the interests of sustainable travel in accordance with Wokingham Borough Core Strategy Policies CP1, CP6 and CP19

12. Prior to commencement of development above finished floor level, details for an Electric Vehicle Charging Strategy serving the development shall be submitted for approval in writing by the Local Planning Authority. This strategy should include details relating to on-site infrastructure, installation of charging points and future proofing of the site unless otherwise agreed in writing by the Local Planning Authority.

Reason: In order to ensure that secure electric vehicle charging facilities are provided so as to encourage the use of sustainable modes of travel. Relevant policy: NPPF Section 9 (Sustainable Transport) and Core Strategy policies CP1, CP3 & CP6 and Managing Development Delivery Local Plan policy CC07 and Appendix 2 and the Council's Parking Standards Study Report (2011).

Reason for imposing as a pre commencement condition: To ensure sustainable modes of transport are designed in from the outset of the development

13. No part of any dwelling(s) hereby permitted shall be occupied until details of secure and covered bicycle storage/ parking facilities serving that dwelling for the occupants of and visitors to the development shall be submitted to and approved in writing by the local planning authority. The cycle storage/ parking shall be implemented in accordance with such details as may be approved before occupation of the development hereby permitted, and shall be permanently retained in the approved form for the parking of bicycles and used for no other purpose.

Reason: In order to ensure that secure weather-proof bicycle parking facilities are provided so as to encourage the use of sustainable modes of travel. Relevant policy: NPPF Section 9 (Sustainable Transport) and Core Strategy policies CP1, CP3 & CP6 and Managing Development Delivery Local Plan policy CC07.

Reason for imposing as a pre-commencement condition: Flexibility has been provided for the timing although the sheds will need to be provided prior to occupation of the dwellings

14. Prior to the commencement of the development there shall be submitted to and approved in writing by the local planning authority, details of the proposed vehicular access to include visibility splays of 43 metres. The access shall be formed as so-approved and the visibility splays shall be cleared of any obstruction

exceeding 0.6 metres in height prior to the occupation of the development. The access shall be retained in accordance with the approved details and used for no other purpose and the land within the visibility splays shall be maintained clear of any visual obstruction exceeding 0.6 metres in height at all times.

Reason: In the interests of highway safety and convenience. Relevant policy: Core Strategy policies CP3 & CP6.

Reason for imposing as a pre-commencement condition: In the interests of highway safety and convenience

15. Each dwelling shall not be occupied until visibility splays of 2.0 metres by 2.0 metres serving that dwelling, have been provided at the intersection of the driveway and the adjacent footway. (Dimensions to be measured along the edge of the drive and the back of the footway from their point of intersection). The visibility splays shall thereafter be kept free of all obstructions to visibility above a height of 0.6 metres.

Reason: In the interests of highway safety and convenience. Relevant policy: Core Strategy policies CP3 & CP6.

16. Prior to the commencement of development, full details of the construction of roads and footways, including levels, widths, construction materials, depths of construction, surface water drainage and lighting shall be submitted to and approved in writing by the local planning authority. The roads and footways shall be constructed in accordance with the approved details to road base level for the dwellings(s) served by the approved roads and footways before the development is occupied and the final wearing course will be provided within 3 months of occupation, unless other minor variations are agreed in writing after the date of this permission and before implementation with the Local Planning Authority.

Reason: To ensure that roads and footpaths are constructed to a standard that would be suitable for adoption as publicly maintainable highway, in the interests of providing a functional, accessible and safe development. Relevant policy: Core Strategy policies CP3 & CP6.

Reason for imposing as a pre-commencement condition: In the interests of highway safety and convenience

Construction Management Plan

17. Before the development hereby permitted is commenced a Construction Environmental Management Plan (CEMP) which shall include details including construction access (es) has been submitted to and approved in writing by the Local Planning Authority shall have been submitted to and approved in writing by the Local Planning Authority. Construction shall not be carried out otherwise than in accordance with each approved CEMP. The CEMP shall include the following matters:

- i. a construction travel protocol or Green Travel plan for the construction phase including details of parking and turning for vehicles of site personnel, operatives and visitors;

- ii. loading and unloading of plant and materials;
- iii. storage of plant and materials;
- iv. programme of works (including measures for traffic management and operating hours);
- v. piling techniques;
- vi. provision of boundary hoarding;
- vii. protection of the aquatic environment in terms of water quantity and quality;
- viii. details of proposed means of dust suppression and noise mitigation;
- ix. details of measures to prevent mud from vehicles leaving the site during construction;
- x. details of any site construction office, compound and ancillary facility buildings. These facilities shall be sited away from woodland areas;
- xi. lighting on site during construction;
- xii. measures to ensure no on site fires during construction
- xiii. monitoring and review mechanisms;
- xiv. implementation of the CEMP through an environmental management system;
- xv. details of the haul routes to be used to access the development; and
- xvi. details of the temporary surface water management measures to be provided during the construction phase
- xvii. details of the excavation of materials and the subsurface construction methodology
- xviii. Details of mitigation measures for protected species to be applied.
- xix. appointment of a Construction Liaison Officer.

Reason: To protect occupants of nearby dwellings from noise and disturbance during the construction period, in the interest of highway safety and convenience and to minimise the environmental impact of the construction phase in accordance with Wokingham Borough Core Strategy Policies CP1, CP3 and CP6

Reason for imposing as a pre-commencement condition: To ensure that the impact of construction activities are suitably mitigated and minimised

Landscape and Trees

18. No development shall take place until a measured survey of the site and a plan prepared to scale of not less than 1:500 showing details of existing and proposed finished ground levels (in relation to a fixed datum point) and finished roof levels shall be submitted to and approved in writing by the local planning authority, and the approved scheme shall be fully implemented prior to the occupation of the building(s).

Reason: In order to ensure a satisfactory form of development relative to surrounding buildings and landscape. Relevant policy: Core Strategy policies CP1 and CP3 and Managing Development Delivery Local Plan policy TB21.

Reason for imposing as a pre-commencement condition: To ensure ground remodelling works are acceptable.

19. Prior to the commencement of the development, full details of both hard and soft landscape proposals shall be submitted to and approved in writing by the local planning authority. These details shall include, as appropriate, proposed finished floor levels or contours, means of enclosure/boundary treatments, car parking layouts, other vehicle and pedestrian access and circulation areas, hard surfacing

materials and minor artefacts and structure (e.g. furniture, refuse or other storage units, signs, lighting, external services, etc.). Soft landscaping details shall include planting plan, specification (including cultivation and other operations associated with plant and grass establishment), schedules of plants, noting species, planting sizes and proposed numbers/densities where appropriate, and implementation timetable.

All hard and soft landscape works shall be carried out in accordance with the approved details prior to the occupation of any part of the development or in accordance with a timetable approved in writing by the local planning authority. Any trees or plants which, within a period of five years after planting, are removed, die or become seriously damaged or defective, shall be replaced in the next planting season with others of species, size and number as originally approved and permanently retained.

Reason: In the interests of visual amenity. Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21 (and TB06 for garden development)

Reason for imposing as a pre-commencement condition: To ensure a satisfactory landscaping approach.

20. No trees, shrubs or hedges within the site which are shown as being retained on the approved plans shall be felled, uprooted wilfully damaged or destroyed, cut back in any way or removed without previous written consent of the local planning authority; any trees, shrubs or hedges removed without consent or dying or being severely damaged or becoming seriously diseased within 5 years from the completion of the development hereby permitted shall be replaced with trees, shrubs or hedge plants of similar size and species unless the local planning authority gives written consent to any variation.

Reason: To secure the protection throughout the time that development is being carried out, of trees, shrubs and hedges growing within the site which are of amenity value to the area. Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21

21.

- i. No development or other operation shall commence on site until the tree protection measures as shown on the Arboricultural Method Statement 37-1035.02 Sheets 1 & 2 are implemented in complete accordance with the Approved Scheme for the duration of the development (including, unless otherwise provided by the Approved Scheme) demolition, all site preparation work, tree felling, tree pruning, demolition works, soil moving, temporary access construction and or widening or any other operation involving use of motorised vehicles or construction machinery.
- ii. No development (including any tree felling, tree pruning, demolition works, soil moving, temporary access construction and or widening or any other operation involving use of motorised vehicles or construction machinery) shall commence until the local planning authority has been provided (by way of a written notice) with a period of no less than 7 working days to inspect the

implementation of the measures identified in the Approved Scheme on-site.

- iii. No excavations for services, storage of materials or machinery, parking of vehicles, deposit or excavation of soil or rubble, lighting of fires or disposal of liquids shall take place within an area designated as being fenced off or otherwise protected in the Approved Scheme.
- iv. The fencing or other works which are part of the Approved Scheme shall not be moved or removed, temporarily or otherwise, until all works including external works have been completed and all equipment, machinery and surplus materials removed from the site, unless the prior approval of the local planning authority has first been sought and obtained.

Reason: To secure the protection throughout the time that the development is being carried out of trees shrubs or hedges growing within or adjacent to the site which are of amenity value to the area, and to allow for verification by the local planning authority that the necessary measures are in place before development and other works commence Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21

Reason for imposing as a pre-commencement condition: To ensure that trees are protected throughout the build out phase.

22. Prior to commencement of development, technical detail on the trenchless thrust boring method within the RPA of veteran tree 7010 shall be submitted for approval by the Local Planning Authority. This shall demonstrate that there is minimal disturbance of the tree. In the event that the details are not agreed, further detail of the connection of the ditch outside of the root protection area of veteran tree 7010 shall be submitted for approval by the Local Planning Authority. Development shall be carried out in accordance with the approved details.

Reason: To secure the protection throughout the time that development is being carried out, of trees, shrubs and hedges growing within the site which are of amenity value to the area. Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21

Reason for imposing as a pre-commencement condition: To ensure the veteran tree is protected.

23. Prior to the commencement of the development (excluding access and groundworks) a landscape management plan, including long term design objectives, management responsibilities, timescales and maintenance schedules for all landscape areas, other than privately owned, domestic gardens, shall be submitted to and approved in writing by the local planning authority. In addition, details a maintenance schedule for measures to maintain ecological permeability and/or provide biodiversity enhancement in the public open space shall be included. The landscape management plan shall be carried out as approved.

Reason: In order to ensure that provision is made to allow satisfactory maintenance of the landscaping hereby approved. Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and

Reason for imposing as a pre-commencement condition: To ensure that the landscape objectives are safeguarded.

Environmental Health

24. No development shall take place until a detailed remediation scheme that describes how the site will be made suitable for the intended use must be submitted to the local planning authority for written approval. The remediation schema shall include, the proposed remediation objectives and remediation criteria, details of all works to be undertaken, the timetable of works and site management procedures. The remediation scheme shall ensure that the site cannot be declared as being contaminated under part 2A of the Environmental Protection Act 1990, in relation to the intended use, after remediation works are completed.

Reason: To ensure that future users of the site are protected from the harmful effects of contamination

Reason for imposing as a pre-commencement condition: To ensure that contamination is taken into account before development commences

25. The approved remediation scheme shall be implemented before other groundworks or construction works commence unless a phased approach has been agreed as part of the approved remediation scheme or unless written approval is given by the Local Planning Authority. The applicant or contractor must give at least two weeks written notice before remediation works commence. Following completion of remediation works at the site, or upon completion of each phase a verification report shall be submitted to the Local Planning Authority for written approval.

Reason: To ensure that future users of the site are protected from the harmful effects of contamination. Relevant Policies CP1 and CP3

26. If unexpected contamination is found at any time during development this shall be reported in writing as soon as possible to the Local Planning Authority. An investigation and risk assessment shall be carried out, and where remediation work is necessary, a remediation scheme must be prepared and submitted for written approval to the local planning authority. Following the completion of measures set out in the approved remediation scheme a verification report shall be submitted to and approved by the local planning authority prior to occupation.

Reason: To ensure that future users of the site are protected from the harmful effects of contamination. Relevant Policies CP1 and CP3

27. Prior to commencement of development, a scheme detailing rat, vermin and other pest control measures shall be submitted to the Local Planning Authority for approval in writing. The scheme shall include:

- i). details of the methods used to reduce existing rat populations and discourage dispersal of rats from the site during the groundworks and construction phase

- ii). a methodology for assessing the ongoing effectiveness of different rat and vermin and pest deterrent methods;
- iii). an objective methodology for assessing when rodenticides / insecticide shall be applied;
- iv). details of control measures and ongoing assessment of effectiveness in relation to mammalian pests;
- v). provision for submission of records relating to the implementation of the foregoing measures to the Local Planning Authority.

The approved scheme shall thereafter be implemented for the duration of the construction activities associated with the delivery of the development.

Reason: To minimise the migration of rats and other vermin to surrounding areas during site clearance and groundworks. Relevant Policies CP1 and CP3

Reason for imposing as a pre-commencement condition: To ensure that the migration of rats caused by groundworks are minimised to protect the amenity of neighbouring properties

28. The dwellings hereby approved shall be designed and/or insulated so as to provide attenuation against externally generated noise in accordance with a mitigation scheme to be submitted to and approved in writing by the Local Planning Authority before commencement of development (excluding access and groundworks) within any sub-phase. The scheme shall ensure that all noise implications are mitigated so that internal ambient noise levels for dwellings shall not exceed 35 dB LAeq (16 hour) 07:00-23:00 during the daytime and 30 dB LAeq (8 hour) 23:00-07:00 during the night. The design and/or insulation measures identified in the scheme shall ensure that ambient internal noise levels for the dwellings meet the BS8233/1999 Sound insulation and noise reduction for buildings – Code of Practice. For gardens, the steady noise level should not exceed 55dB LAeq,T in areas used for recreation the approved mitigation measures to serve each dwelling shall be implemented prior to occupation and retained thereafter.

Reason: To protect future residents from the harmful effects of high noise levels, in accordance with Wokingham Borough Core Strategy Policy CP1.

29. Before construction of the sub-station commences technical specifications, to include a noise assessment and mitigation report identifying any attenuation measures to ensure that the substation is designed and insulated to mitigate against the noise produced by the substation (whether directly or indirectly), shall be submitted to and approved in writing by the Local Planning Authority. The attenuation measures shall be implemented maintained and retained thereafter in accordance the approved details.

Reason: To protect the amenity of the area and to ensure that the development is not un-neighbourly in accordance with Wokingham Borough Core Strategy Policies CP1 and CP3.

Hours of Construction

30. No work relating to the development hereby approved, including works of demolition or preparation prior to building operations, shall take place other than

between the hours of 08:00 and 18:00 Monday to Friday and 08:00 to 13:00 Saturdays and at no time on Sundays or Bank or National Holidays

Reason: In the interests of the amenities of neighbouring occupiers. Relevant Policies CP1 and CP3

Delivery Times

31. No deliveries of materials or equipment relating to the development hereby approved shall take place other than between the hours of 8:00a.m. and 6:00p.m. Monday to Friday and 8:00 a.m. to 1.00p.m. Saturdays and at no time on Sundays or bank or national Holidays.

Reason: In the interests of the amenities of neighbouring occupiers. Relevant Policies CP1 and CP3

Flood mitigation

32. No development shall take place until full details of the drainage system for the site have been submitted to and approved in writing by the LPA. The details shall include:

- i. Calculations indicating the Greenfield runoff rate from the site.
- ii. BRE 365 test results demonstrating whether infiltration is achievable or not.
- iii. Use of SuDS following the SuDS hierarchy, preferably infiltration.
- iv. Full calculations demonstrating the performance of soakaways or capacity of attenuation features to cater for 1 in 100 year flood event with a 40% allowance for climate change and runoff controlled at 30l/s, or better.
- v. Calculations demonstrating that there will be no flooding of pipes for events up to and including the 1 in 100 year flood event with a 40% allowance for climate change, or how any flooding will be managed within the site.
- vi. Groundwater monitoring to confirm seasonal high groundwater levels.
- vii. A drainage strategy plan indicating the location and sizing of SuDS features, with the base of any SuDS features located at least 1m above the seasonal high water table level.
- viii. Details demonstrating how any SuDS for this development would be managed throughout the lifespan of the development and who will be responsible for maintenance.

Reason: To prevent increased flood risk from surface water run-off. Relevant policy: NPPF Section 10 (Meeting the Challenge of Climate Change, Flooding and Coastal Change), Core Strategy policy CP1 and Managing Development Delivery Local Plan policies CC09 and CC10.

Reason for imposing as a pre-commencement condition: To ensure that flood risk is managed and impacts are minimised to all users

33. No properties shall be occupied until confirmation has been provided that either:- all wastewater network upgrades required to accommodate the additional flows from the development have been completed; or- a housing and infrastructure phasing plan has been agreed with Thames Water to allow additional properties to be occupied. Where a housing and infrastructure phasing plan is agreed no occupation shall take place other than in accordance with the agreed housing and infrastructure phasing plan.

Reason - The development may lead to sewage flooding and network reinforcement works are anticipated to be necessary to ensure that sufficient capacity is made available to accommodate additional flows anticipated from the new development. Any necessary reinforcement works will be necessary in order to avoid sewer flooding and/or potential pollution incidents. Relevant policy: NPPF Section 10 (Meeting the Challenge of Climate Change, Flooding and Coastal Change), Core Strategy policy CP1 and Managing Development Delivery Local Plan policies CC09 and CC10.

Ecology

34. Prior to commencement of development full details of a strategy to maintain the ecological permeability of the site (especially with regard to reptiles, amphibians and hedgehogs) shall be submitted to and approved in writing by local planning authority. The strategy shall include:
- i. Details of measures to be applied to boundary treatments and retaining walls.
 - ii. Details of measures to prevent entrapment of animals in the surface water drainage scheme.
 - iii. Locations of the above measures within the detailed layout.
- The mitigation and contingency measures contained within the plan shall be implemented in accordance with the approved plan unless otherwise approved in writing by the local planning authority.

Reason: To ensure compliance with policy TB23 and protect species of principal importance.

Reason for imposing as a pre-commencement condition: To ensure Ecology is safeguarded pre and post construction.

35. Prior to commencement of development above finished floor level, full details of a biodiversity enhancement strategy to provide a mixture of bat boxes, bird boxes, hedgehog houses, and insect homes / habitat piles suitable for priority species across the development site at a ratio of at least 1 per 2 dwellings shall be submitted to and approved in writing by local planning authority. The measures contained within the strategy shall be implemented in accordance with the approved plan unless otherwise approved in writing by the local planning authority.

Reason: To promote protection and recovery of priority species and secure net gains for biodiversity as per NPPF paragraph 174.

Reason for imposing as a pre-commencement condition: To ensure Ecology is safeguarded pre and post construction.

36. Prior to commencement of development, full details of a reptile mitigation strategy shall be submitted to and approved in writing by the local planning authority. The surveys, mitigation and contingency measures contained within the plan shall be implemented in accordance with the approved plan unless otherwise approved in writing by the local planning authority.

Reason: To secure appropriate mitigation for reptile species of principal importance and make the development acceptable per policy CP7.

Reason for imposing as a pre-commencement condition: To ensure Ecology is safeguarded pre and post construction.

Archaeology

37. No development shall take place within the application area until the applicant, their agents or successors in title have secured and implemented a programme of archaeological field evaluation in accordance with a written scheme of investigation, which has been submitted by the applicant and approved in writing by the Local Planning Authority. The results of the evaluation will inform the preparation of a subsequent mitigation strategy which will be submitted by the applicant and approved in writing by the Local Planning Authority prior to the commencement of the development. The mitigation strategy will provide for:

- i) A programme of site investigation and recording, or alternative appropriate mitigation, within the area of archaeological interest. Development will not commence within the area of archaeological interest until the site investigation has been satisfactorily completed.
- ii) A programme of post investigation assessment, analysis, publication, dissemination and archiving. This part of the condition shall not be discharged until these elements of the programme have been fulfilled in accordance with the programme set out in the WSI or unless otherwise agreed in writing by the Local Planning Authority

Reason:

To ensure that any archaeological remains within the site are adequately investigated and recorded or preserved in situ in the interest of protecting the archaeological heritage of the borough.

Reason for imposing as a pre-commencement condition: To ensure Archaeology is safeguarded pre and post construction.

Informatives

- i. The development accords with the policies contained within the adopted development plan and there are no material considerations which warrant a different decision being taken.
- ii. This permission should be read in conjunction with the legal agreement under Section 106 of the Town and Country Planning Act (yet to be finalised) the contents of which relate to this development.
- iii. The Corporate Head of Environment at the Council Offices, Shute End, Wokingham should be contacted for the approval of the access construction details before any work is carried out within the highway. This planning permission does NOT authorise the construction of such an access.
- iv. Adequate precautions shall be taken during the construction period to prevent the deposit of mud and similar debris on adjacent highways. For

further information contact Corporate Head of Environment on tel: 0118 974 6302.

- v. If it is the developer's intention to request the Council, as local highway authority, to adopt the proposed access roads etc. as highway maintainable at public expense, then full engineering details must be agreed with the Corporate Head of Environment at the Council Offices, Shute End, Wokingham. The developer is strongly advised not to commence development until such details have been approved in writing and a legal agreement is made with the Council under S38 of the Highways Act 1980.
- vi. Any works/events carried out either by, or at the behest of, the developer, whether they are located on, or affecting a prospectively maintainable highway, as defined under Section 87 of the New Roads and Street Works Act 1991, or on or affecting the public highway, shall be coordinated under the requirements of the New Roads and Street Works Act 1991 and the Traffic management Act 2004 and licensed accordingly in order to secure the expeditious movement of traffic by minimising disruption to users of the highway network in Wokingham.
- vii. Any such works or events commissioned by the developer and particularly those involving the connection of any utility to the site, shall be co-ordinated by them in liaison with Wokingham Borough Council's Street Works Team, (telephone 01189 746302). This must take place at least three month in advance of the works and particularly to ensure that statutory undertaker connections/supplies to the site are coordinated to take place wherever possible at the same time.
- viii. The attention of the applicant is drawn to the requirements of Section 60 of the Control of Pollution Act 1974 in respect of the minimisation of noise on construction and demolition sites. Application, under Section 61 of the Act, for prior consent to the works, can be made to the Environmental Health and Licensing Manager.
- ix. In respect of surface water it is recommended that the applicant should ensure that storm flows are attenuated or regulated into the receiving public network through on or off site storage. When it is proposed to connect to a combined public sewer, the site drainage should be separate and combined at the final manhole nearest the boundary. Connections are not permitted for the removal of Ground Water. Where the developer proposes to discharge to a public sewer, prior approval from Thames Water Developer Services will be required. They can be contacted on 0845 850 2777.
- x. Licences, consents or permits may be required for work on this site. For further information on environmental permits and other licences please visit <http://www.businesslink.gov.uk/bdotg/action/layer?r.s=tl&r.lc=en&topicId=1079068363>
- xi. The attention of the applicant is drawn to the requirements of Section 60 of the Control of Pollution Act 1974 in respect of the minimisation of noise on construction and demolition sites. Application, under Section 61 of the Act,

for prior consent to the works, can be made to the Environmental Health and Licensing Manager

- xii. The council advises that the developer produces a strategy to install superfast broadband infrastructure for future occupants of the site. The strategy should ensure that upon occupation of a dwelling the new home owner has access to a superfast broadband service through a site-wide network. It is also advised that the developer keeps occupants fully informed of any delays to superfast broadband connection in before they purchase/occupy their new homes

C) Alternative recommendation

That the committee authorise the Head of Development Management to refuse planning permission in the event of an S106 agreement not being completed to secure the services, affordable housing and infrastructure within six months of the date of the committee resolution (unless a longer period is agreed by the Head of Development Management in consultation with the Chairman of Planning Committee) for the following reasons:

- 1) In the absence of a planning obligation to secure suitable contributions / on site and off works for the following:
 - i. Highway improvements/upgrades including the Magpie and Parrot roundabout (southern roundabout) on the Eastern Relief Road.
 - ii. Footpath improvements both on and off site
 - iii. Contributions for South of the M4 Bus Strategy
 - iv. Contributions for My Journey
 - v. Provision of off-site sports facilities including securing a cricket pitch and pavilion at land at the Manor, Brookers Hill
 - vi. Provision of play space
 - vii. 35% Affordable housing provision on site with a payment in lieu of 0.15 dwelling and a further financial contribution
 - viii. Employment skills plan/contribution
 - ix. Maintenance sums
 - x. Provision of SANG land at the Ridge SANG
 - xi. SAMM contributions
 - xii. Monitoring Fees
 - xiii. SANG Management for the Ridge SANG

the Local Planning Authority is unable to satisfy itself that the proposal includes adequate mitigation measures to prevent the proposed development from having an adverse effect on infrastructure, services and would fail to provide affordable housing. This is contrary to the requirements of the NPPF and would compromise the delivery of the necessary infrastructure within the South of the M4 SDL. This is contrary to policies CP1, CP2, CP3, CP4, CP5, CP6, CP7, CP8, CP10, CP19 and Appendix 7 of the Core Strategy, policies CC01, CC08, TB08, TB12 of the Managing Development Delivery Development Plan Document, saved policy NRM6 of the South East Plan and the Council's adopted South of the M4 SDL SPD and Infrastructure Delivery and Contributions SPD.

- 2) In the absence of a planning obligation to secure suitable avoidance and mitigation measures and access management monitoring arrangements, in terms that are satisfactory to the Local Planning Authority (LPA), the LPA is unable to satisfy itself that the proposals include adequate mitigation measures to prevent the proposed development from having an adverse effect on the integrity of the Thames Basin Heaths SPA, in line with the requirements of Regulation 63 of the Conservation of Habitats and Species Regulations 2017 and Article 6(3) of Directive 92/43/EEC. The proposal would be contrary to Policy NRM6 of the South East Plan, Policies CP8 and CP4 of the Core Strategy.

PLANNING HISTORY

No specific recent relevant planning history for the site, planning permissions for major sites for the South of the M4 SDL include those listed below.

For ease of reference outline planning approvals for the sites are shown in bold and S73 variations / reserved matters in standard text

Application ref	Description	Outcome
O/2010/1432	Outline application for a residential development of up to 1 200 dwellings a further 150 units of specialist housing (including sheltered housing) for elderly persons a local centre to include a foodstore (2 500 sqm) and other retail and office uses a community building proposed extension of existing primary schools erection of a new primary school public open space sports pavilion suitable alternative natural greenspace (SANG) and access and landscaping	Appeal Approved 08/11/2012
VAR/2014/0624	Application to remove condition 48 of planning consent O/2010/1432 (the development shall be implemented in accordance with the approved Residential Travel Plan) – <i>Note this in effect becomes the outline consent for the above scheme</i>	Approved 22/10/14
160183	Application for Reserved Matters submission in respect of the primary infrastructure phase including access roads, suds ponds, foul and surface water drainage and associated landscaping.	Approved 15/04/16
161189	Reserved Matters application pursuant to Outline planning permission VAR/2014/0624 (Pursuant to O/2010/1432) for the erection of 517 dwellings including access roads, garages, parking spaces, open space and landscape treatment of Phase 1, Shinfield West. (Access within the site, appearance, landscaping, layout and scale to be considered).	Approved 21/07/17

Application ref	Description	Outcome
170035	Reserved Matters application for a two form entry (2FE) primary school, including all hard & soft play areas, school car parking, attenuation pond (habitat area), landscaping and associated drop off car parking.	Approved 11/05/17
172450	Reserved Matters application pursuant to Outline planning permission O/2010/1432 (pursuant to VAR/2014/0624) for the erection of 473 dwellings including access roads, garages, parking spaces, open space and landscape treatment for Phase 2. (Access, Appearance, Landscaping, Layout and Scale to be considered)	Approved 14/12/17
180757	Reserved matters application for the erection of a 68-bed care home building (Class C2) and associated development including site access, car parking and landscaping in the part of the site identified by the outline planning consent for specialist housing. Access, Layout, Scale, Appearance and Landscaping to be considered.	Approved 20/06/18
180758	Reserved matters application for the erection of a building comprising 80 no. extra care units (Class C2) and associated development including site access, car parking and landscaping in the part of the site identified by the outline planning consent for specialist housing. Access, Layout, Scale, Appearance and Landscaping to be considered.	Approved 20/06/18
Science and innovation park		
O/2009/1027	Outline application for phase 1 development of Science & Innovation Park (Access to be considered) plus full application for the construction of access road foot and cycle ways M4 overbridge and associated works including landscaping and engineering works plus erection of boundary wall and fence adjoining Shinfield Road/Access Road. Part demolition and reformation of facade of Stable Buildings at Lane End Farm and demolition of existing farm buildings.	Approved 27/10/10
RM/2015/0630	Reserved Matters application pursuant to Outline Planning Consent O/2009/1027 for the development of phase 1A of proposed Thames Valley Science Park comprising the construction of a gateway building and all associated landscaping and ancillary works plus temporary car parking arrangements – Appearance, Landscaping, Layout and Scale to be considered.	Approved 26/08/15

Application ref	Description	Outcome
162841	Reserved Matters in relation to the development of Phase 1b of the proposed Thames Valley Science Park (TVSP) for a new cancer treatment centre, all associated landscaping, access and ancillary works	Approved 8/12/18
162818	Reserved Matters application for the car park for phase 1 of the Science Park	Approved 8/12/18
173287	Reserved Matters application pursuant to Outline Planning Consent O/2009/1027 (as extended under planning permission 152330) for the development of phase 1 of Thames Valley Science Park, comprising the construction of building 2 of the gateway building and all associated landscaping and ancillary works, plus temporary car parking arrangements - Appearance, Landscaping, Layout and Scale to be considered.	Approved 14/02/18
163609	Outline planning application for Phase 2 of the Thames Valley Science Park comprising up to 57,110 sqm research and development and innovation floor space (with occupancy restricted by a Gateway policy) inclusive of up to 5,711 sqm of amenity and supporting uses and an energy centre (all matters reserved except access to the site).	Approved 06/07/18
Eastern Relief Road		
F/2010/1428	Full application for the construction of an Eastern Relief Road (ERR) to Shinfield including the construction of road foot and cycle-ways an M4 over-bridge. Re-grading of embankments landscaping utilities creation of flood compensation areas and associated works including engineering and other operations. Erection of replacement boundary wall and fence adjoining Shinfield Road/ ERR part demolition of existing farm buildings at Lane End Farm and demolition/deconstruction of two poly tunnels south of Cutbush Lane. – Delivered and open	Appeal approved 03/06/11
Spencers Wood / Three Mile Cross		
O/2013/0346	Hybrid application with part in outline in respect of up to 900 dwellings up to a three form entry primary school public open space access and landscape (means of access into the site to be considered) and part in full in respect to Suitable Alternative Natural Greenspace (SANG) following demolition of existing dwelling and ancillary buildings including greenhouses sheds stables and	Approved 24/12/2013

Application ref	Description	Outcome
	garage (214a Hyde End Road). - Approved 24/12/2013	
F/2013/0347	Full planning permission for the erection of 276 dwellings with associated access landscaping and open space following demolition of existing dwelling and ancillary buildings (214a Hyde End Road). - Note part of the 900 dwellings as referred to under O/2013/0346 -above	Approved 24/12/2013
162829	Application for Reserved matters application for 175 dwellings including internal access roads, garages, parking places, open spaces, allotments, Multi-Use Games Area (MUGA) and Locally Equipped Area of Play.	Approved 10/03/17
163457	Reserved Matters application pursuant to Outline Planning Consent O/2013/0346 for the primary infrastructure phase including Spine Road, Suds Ponds, Foul and Surface Water Drainage and Associated Landscaping	Approved 23/5/17
170010	Reserved Matters application pursuant to Outline Planning Consent O/2013/0346 for 363 dwellings including internal access roads, garages, parking spaces, SuDS attenuation, open space, Play Areas and associated landscaping enhancements.	Approved 26 th July 2017
170239	Application for approval of reserved matters following outline approval (O/2013/0346) for a sustainable transport link to serve public transport (buses), emergency vehicles and pedestrian and cycle links.	Approved 7/12/18
Cutbush North, Shinfield		
O/2013/0101	Outline planning permission for a residential development comprising up to 126 dwellings, public open space, children's play areas, accesses to Cutbush Lane and the Shinfield Eastern Relief Road, Landscaping, Ecological buffer zone, ground modelling, sustainable drainage systems and associated infrastructure.	Approved 24/12/2013
RM/2014/2561	Reserved Matters application pursuant to Outline Planning Consent O/2013/0101 for a residential development comprising up to 126 dwellings public open space, children's play areas, accesses to Cutbush Lane and the Shinfield Eastern Relief Road, Landscaping, Ecological Buffer Zone, ground modelling, sustainable drainage and associated infrastructure.	Approved 24/06/15

Application ref	Description	Outcome
Land South of Reading Road and Arborfield Road, East of Chestnut Crescent, West of the River Loddon		
181631	Full planning application for the change of use of 21.7ha of land from agricultural use to informal recreation (Suitable Alternative Natural Greenspace SANG) and associated infrastructure including pedestrian and vehicle access, a car park, footpath network and landscaping.	Refused 26/02/19
The Manor, Shinfield		
O/2011/0204	Outline application for demolition of existing buildings and a residential development comprising up to 126 dwellings, a sports pavilion, public open space, landscaping and associated works (means of access to be considered: appearance, landscaping, layout, and scale reserved).	Appeal approved 30/05/12
VAR/2013/0602	Application to vary conditions 1, 9, 14, 15, and 29 and to removal of conditions 50 and 53 of Appeal planning consent O/2011/0204.	Approved 17/06/13
RM/2014/2062	Reserved Matters application pursuant to planning consent VAR/2013/0602 for a development of 126 dwellings, a sports pavilion, public open space, landscaping and associated works. This application comprises details of appearance, landscaping, layout and scale.	Approved 30/06/17
153435	Reserved Matters application pursuant to planning consent VAR/2013/0602 for development of a sports pavilion and associated vehicle parking, public open space (including a cricket pitch and informal open space), landscaping and associated works. This application comprises details of appearance, landscaping, layout and scale.	Approved 26/05/16
Land north of Hyde End Road, Spencers Wood		
161255	Full application for the proposed erection of 32no dwellings with associated vehicular access, parking and landscaping	Approved 16/08/17

SUMMARY INFORMATION	
For Residential	
Site Area	9.26
Existing units	0
Proposed units	249
Proposed density - dwellings/hectare	27
Number of affordable units proposed	87 + commuted sum
Previous land use	Agricultural currently grassland
Proposed Public Open Space	1767m ²
Proposed parking spaces	511 in total

CONSULTATION RESPONSES	
Royal Berkshire Fire and Rescue	Refers to building regs
Royal Berkshire Fire and Rescue	Refers to building regs

Southern Gas Networks	No objection
SEE Power Distribution	No objection
Thames Water	No objection request condition(s) 33
WBC Biodiversity	No objection request condition(s) 34-36
WBC Economic Prosperity and Place (Community Infrastructure)	Request Employment Skills Plan contributions
WBC Drainage	No objection request condition(s) 32
WBC Education (School Place Planning)	No comments received
WBC Environmental Health	No objection request condition(s) 17 & 24-31
WBC Highways	No objection request condition(s) 9-17
WBC Tree & Landscape	No objection request condition(s) 18-23
WBC Cleaner & Greener (Waste Services)	No comments received
WBC Property Services	No comments received
WBC Public Rights of Way	No objection
Berkshire Archaeology	No objection request condition(s) 37
Environment Agency	Standing advice, defers to WBC Drainage Officer
Historic England	Standing advice
Natural England	Standing advice

REPRESENTATIONS

Parish Council:

1st stage comments included and annexed to the report

With regret, Shinfield Parish Council acknowledges it is now planning policy to develop this site as part of the overall South of M4 Strategic Development Location. However, we are very concerned about the large number of reasonable objections which have been raised by local residents and that this application does not take advantage of the links to the 2011 Supplementary Planning Document.

We therefore wish to see the following conditions agreed as a minimum in order for this scheme to be deemed acceptable:

Issue raised	Officer note
Developers properly mitigate the transport implications of this scheme	A transport assessment has been submitted with the application and reviewed by the Highways Officer who is satisfied that any adverse impacts can be mitigated
A Safer Routes to Schools policy be incorporated;	A Walking and Cycling Strategy will be required by condition 11. Links to the site to Arborfield Road will be secured by a S106 agreement.
A proper Flood Risk Assessment be undertaken and approved the relevant authorities;	A full FRA has been submitted and reviewed by the Drainage Officer who raises no objections subject to condition 32-33
Proper mitigation measures are in place for bats as a protected species.	The Ecology report has been reviewed by the Ecology Officer who raises no

	objections to the development subject to conditions 34 - 36
Does not integrate with footpath 6 the other side of ERR (the current permissive path allows this)	The permissive path will be retained in the site layout although there will be a more formal alternative route along the shared streets and spaces at the south of the site. This will link back to the permissive path and the relationship with footpath 6 will be unaltered
The entirety of the development should be designed for a 20mph limit for the outset (D&A p 16)	The street hierarchy has been designed in accordance with Manual for Streets standards to keep vehicle speeds low within the development. Two areas of traffic calming measures have been applied to the main street and the secondary streets beyond this are short in distance. Shared surface streets will also discourage excessive speed within the development
The use of 3 story buildings in 'The Residential Core' is out of keeping with the rest of this side of Shinfield which are all no more than 2 story.	See report below paragraphs 14-31 specifically paragraph 28. Three storey development is present on Chrysanthemum Drive and Fuchsia Drive and provides variety in scale and design. Taller buildings are sited within the site away from existing residential development
The refuse strategy (D&A p26) refers to space for one 240 litre wheelie bin and one 55 litre recycling box, this is inadequate as we don't have wheelie bins in WBC	Noted although this will provide flexibility for any potential changes to waste collection management.
The affordable housing are all located in the same area rather than being spread across the development	Whilst this is noted, clustering affordable housing assists with management. The Affordable Housing officer has assessed the scheme and raised no objections in regard to the location of the housing
The use of conservation kerbing should be used on all primary and secondary streets	Noted although this will be subject to the s38 details for the site
To facilitate the use of the ERR and to discourage vehicles cutting through the village, the application should bring forward the reduction of the speed limit to 30mph for Hollow Lane and Arborfield Road within the boundary of Shinfield Village	This is a matter that is subject to a Traffic Regulation Order and not subject to the current application

The accident analysis does not include any data after the ERR was opened, it should be redone (Highways Transport Assessment)	It is normal practise to include roads after they have been opened for more than a year. The accident data assessed was for 5 years up to 31 st Oct 2017. However Crashmap has been assessed and there were no accidents on ERR
Point 3.83 (p21 of Traffic Assessment) is incorrect in that the policeman died as a result of this incident	The accident data reported a serious accident in 2013. The class of the casualty is set within 30 days of the accident.
Traffic Assessment - car parking - no car charging points are installed, the expectation would be for one point per dwelling, in line with Government ambitions	Condition 12 requires details in regard to this
Traffic Assessment - vehicle trip rates are too low when compared with actual measurements so grossly underestimate the vehicle movements associated with this development	The vehicle trip rates used in the transport assessment are the approved trip rates from the Wokingham Strategic Transport Model
The Parrot Farm roundabout will be over-capacity with this development so remedial measures should be implemented to facilitate traffic flow (p 34 of traffic Assessment)	See report below paragraphs 57-60
The assumption that walk/cycle trips out of the development would use FP7 and then Oatlands Road would put pressure on an un-adoptive road. The development should provide that a suitable flat path along this route. (p40 of traffic Assessment)	The developer has agreed to upgrade the footpath between the site and Oatlands Road
Second round consultation comments (other than those identified above). Full comments included and annexed to the report.	
The priority junction on Cutbush Lane should be changed back to a roundabout to limit the speed of vehicles using the roads for safety.	The junction has been modelled as a priority junction as part of the assessment and the Highways Officer has raised no objections to this aspect. The removal of the roundabout results in the loss of less hedgerow in the local area which will help maintain the character of Cutbush Lane
Lack of integration of site with footpath 6 the other side of ERR. A suitable, all-weather path with appropriate gates	This is noted although the section of the permissive path between the site exit and ERR is within in root protection

should be provided down and across the ERR.	areas. A formal path here would have an adverse impact on these specimens
Traffic calming measures should be introduced into Cutbush Lane, such as priority islands and narrows, to stop vehicles speeding.	Cutbush Lane from Holly Lane to the ERR is signed as a 30mph speed limit. Cutbush Lane terminates at the ERR in the vicinity of the site, so vehicles would naturally be unlikely to speed.
Parking for the apartments is woefully inadequate.	The Highways Officer has assessed the application and parking is in accordance with WBC standards
Construction and site traffic should all use Cutbush Lane rather than cutting through a housing estate, this needs to be conditioned and enforced.	This will be subject to the Construction Environmental Management Plan (CEMP)
Site should be registered with the Considerate Construction Scheme	This will be determined when a contractor for the build is appointed
Detailed construction management plan needs to be submitted to ensure that site and construction traffic is routed correctly.	This will be subject to the CEMP
Number of advertising banners should be restricted to a maximum of two; they are unsightly and produce unwanted noise, detrimental to the surrounding properties.	This matter is covered by the Advertisement Regulations and would be address in any future applications.
To protect occupants of nearby dwellings from noise and disturbance no works in respect of the construction of the development shall be undertaken outside the hours of 08:00 - 1800 on Mondays to Fridays (inclusive); outside the hours of 0800 - 1300 on Saturdays; and on Sundays and on public holidays, in accordance with Wokingham Borough Core Strategy Policy CP3.	Noted and condition 31 refers
Adequate precautions, such as wheel washing and road sweeping, shall be taken during the construction period to prevent the deposit of mud and similar debris on adjacent highways and pavements	This will be subject to the CEMP
The apartment blocks should all have fully integrated reception systems (IRS) into each dwelling to stop the proliferation of satellite dishes and aerials which negatively affect the look of the development	The apartments will be ducted for cable services which should prevent satellite dish installation being a significant issue.
Loss of the tree would be detrimental to the character of the countryside contrary to CP1, CP3, CP11 of the Core Strategy	Whilst this is noted, there will be significant planting within the site and to the east along the ERR. Please see report below paragraph 32-34

and CC01, CC02, CC03, TB21 of the MDD Plan	
To protect the environment and the local residents it should be conditioned that no fires be lit on the site for the burning of rubbish, clearing of the site, or any other purpose. All waste and excess materials should be recycled where possible or disposed of through the correct method.	This will be subject to the CEMP
Each dwelling should have at least one car charging point installed as standard.	Condition 12 refers
Vehicle trip rates are too low when compared with actual measurements so grossly underestimate the vehicle movements associated with this development.	The vehicle trip rates used in the transport assessment are the approved trip rates from the Wokingham Strategic Transport Model
The play area is too small for a site of this size.	The Green Infrastructure Officer has assessed the application, the scheme meets the standards and is therefore satisfied with the play area provision
Suitable fencing and gates should be provided to the eastern side of the site to ensure that there is no access, especially for children, to the ERR. This is especially important around the southern part of the site near the play area.	This is noted and the layout of the play area will be subject to details that will be secured through the legal agreement.
The use of the pumping station should be blocked, and suitable foul water and drainage provision should be provided as there are historical instances of these pumping stations not being maintained correctly.	Thames Water have been consulted and are satisfied with the foul arrangements subject to condition 33
All responsibility for the maintenance and upkeep of the attenuation features, green areas, play areas, and open space should be handed over to WBC (or SPC) and not given to maintenance companies who have often charged excessive rates	This will be subject to the adoption plan and commuted sums will be sought for future maintenance where land is adopted. It is not uncommon in modern developments for parts of the open space to be maintained by a management company
Bus route unidentifiable on plan; it does not show where the bus will exit.	This development is providing the infrastructure through the site to enable a bus to run sometime in the future. A contribution to assist this new service has been agreed
Concerns about construction traffic going through an already occupied estate.	This will be subject to the CEMP

Local Members: No comments received

Neighbours: 1st stage consultation resulted in 117 letters of objection / comment and the planning issues are summarised below

Issue raised	Officer comment
Non-compliance with Core Strategy and land not identified for development in the SPD	The SDL was identified in the Core Strategy. Further work in respect to site allocation for the South of the M4 SPD and the site was identified through the Spatial Framework Plan. The application for the housing is located in the area identified by the SPD and was identified as a Reserve Site. It is acknowledged that the formal public open space is located beyond the land identified in the SPD, however given the intended associated use of this space and likely future development, this is considered acceptable. Assessment against Core Strategy Policies are made in the report below
Process of releasing the land before 2026	The land was released through and Executive Decision on 27 th July 2017. The Shinfield West Unilateral Undertaking had the flexibility to allow the council to call on the land prior to 2026. The release of the site will ensure that the borough maintains a 5 year housing land supply which is a statutory government requirement.
In excess of housing identified in south of the M4 SPD	There was no maximum housing figure applied to the Core Strategy. Each site proposal is assessed on its own merits. The borough is required to maintain a five year housing land supply by central Government.
No further development in Shinfield	Shinfield has been identified a sustainable location given its location and access to facilities and services, which can accommodate additional housing growth which was determined through the allocation of the area as a SDL
Inappropriate development in the countryside	The site has been identified as being suitable for housing and was identified in the South of the M4 SPD. The merits of the play space outside the SPD allocated site is discussed below in paragraph 6.
Development should be on brownfield land	The site has been identified as being suitable for residential development through the allocation in the Core Strategy and South of the M4 SPD

Need for additional housing / overdevelopment	The borough is required to provide a five year housing land supply by national policy. Our housing need is derived through the Housing Market Assessment.
Loss of recreational access to the countryside	The public right of way and permissive footpath will be retained in the site layout. The SANGs provide significant additional access to the countryside outside of the application site but close by.
Use of Hawthorne as a bus route	Hawthorne has always been identified as a bus route and was constructed to 6.1m in order to accommodate buses
Inappropriate location for development	The identification of the site through the South of the M4 SPD means that it is a suitable location subject to the considerations outlined below.
Impact on wildlife / protected species / ecology / flora and fauna	The Ecology Officer has assessed the impact of the development and supports the application subject to conditions 34-36
Wildlife surveys inaccurate	These have been assessed by the Ecology Officer and they raise no objection.
Impact on utilities (water, sewage etc.)	Thames Water have been consulted for the development and raise no objections subject to condition 33
Impact on archaeology	The application has been assessed by Berkshire Archaeology and they raise no objections in regard to the report and have requested condition 37.
Impact on health services	Shinfield surgery was built in order to accommodate additional housing growth and is currently not at capacity. Delivery of health care is the responsibility of the Clinical Commissioning Group
Maintenance of assets such a SUDS	A private management company will be set up for the SUDs. The play area will be transferred to WBC with a commuted sum for maintenance
Impact on flooding / local watercourses	The application has been assessed by the flood risk officer who supports the approach subject to condition 32
Impact on wider highway network	The application has been assessed by the Highway Officer who raises no objection subject to conditions 9-17
Impact of access to the site via Cutbush Lane / Hawthorne Road / impact on highway safety	The assessment has shown a level of traffic using Hawthorne to get to the ERR. Hawthorn will eventually become a route from the science park

	<p>roundabout to Arborfield Road. The road has been built to 6.1m wide road to enable the potential for buses to be able to use the road. The level of traffic from this development would not affect the safety of this road.</p> <p>The application has been assessed by the Highway Officer who raises no objection subject to conditions 9-17</p>
Impact on highways for construction traffic	This will be managed by the CEMP which condition 17 refers
Payment of travel plan contributions to WBC is corrupt	WBC operate a borough wide Travel Plan through My Journey. This ensures a joined up approach to sustainable transport provision. .
Provision of parking on site	The application has been assessed by the Highway Officer who raises no objection subject to conditions 9-17
Access for emergency services	A secondary emergency access point is proposed and included in the site layout
Impact on highway safety for cyclists on Cutbush Lane	The application has been assessed by the Highway Officer who raises no objection subject to conditions 9-17
Impact on infrastructure (Schools, Hospital, GPs)	The application will generate CIL income to support local roads, provision of school spaces, green infrastructure etc. (see 123 list for full details). In regard to healthcare Shinfield surgery was built in order to accommodate additional housing growth. Delivery of health care is the responsibility of the Clinical Commissioning Group
Poor links to public transport	The site will be well served by public transport locally which is being delivered via the South of the M4 Public Transport Strategy. The application has been assessed by the Highway Officer who raises no objection. The site also offers opportunity for a bus route through the site.
Impact on character – three storey flats	This is discussed in paragraphs 14-31 and specifically paragraph 28 below
Housing density too high / out of character with the surrounding area	This is discussed in paragraphs 14-31 below
Loss of agricultural land	The land is not considered high grade agricultural land and the suitability of the site has been identified by the allocation of the site through the South of the M4 SPD

Impact on footpath 7	The alignment of this is maintained in the site layout and upgrades will be made to this via the S106.
Impact of noise	This is discussed in paragraph 48 below
Impact on amenity (overlooking, loss of light and overbearing)	This is discussed in paragraphs 39-48 below
Impact on amenity in regard to construction activities (noise, dust etc.)	This is discussed in paragraphs 48 below and condition 17 for a CEMP is attached.
Lack of access to local facilities (shops, community facilities, recycling etc.)	The site is will connected to local shops in Shinfield and it is expected that further facilities will be delivered in the centre
Lack of recreational facilities (sports pitches)	Sports pitches will be delivered at the Manor which is subject to the S106
Lack of existing access to children's play areas	The proposal includes a play area
Lack of public consultation	Public consultation events have been undertaken by the developer
Lack of provision of affordable housing	The proposal will secure a policy compliant 35% provision of affordable housing.

2nd stage consultation resulted in 40 letters of objection / comment and the planning issues raised other than those listed above are summarised below

Issue raised	Officer Comment
Revised layout results in further loss of privacy	This is discussed in paragraphs 39-47 below
Not all residents re consulted	A full re-consultation was undertaken for the revised scheme which included the Environmental Statement on the 30/10/18
Revised junction unsafe	The application has been assessed by the Highway Officer who raises no objection subject to conditions 9-17. Further road safety audits will be carried out during the design process
Comments from Thames Water ignored	Thames Water were consulted and the request for a condition is included as referred to by condition 33
Combining phase 1 and phase 2 for the purposes of the Environmental Statement	An assessment needs to be made in regard to the cumulative impacts of the development together with any other development sites.
Lack of broadband facilities	The Council is not responsible for this service provision however have ensured services can be provided. Openreach will be responsible for providing broadband to new residents.

Other plans uploaded to website with no formal neighbour consultation	The revised plans are in regard to minor amendments on points in technicality which would not have an impact on residential amenity	
---	---	--

PLANNING POLICY		
National Policy	NPPF	National Planning Policy Framework
Technical Guidance to the National Planning Policy Framework		
National Planning Policy Guidance		
Draft revised National Planning Policy Framework		
Adopted Core Strategy DPD 2010	CP1	Sustainable Development
	CP2	Inclusive Communities
	CP3	General Principles for Development
	CP4	Infrastructure Requirements
	CP5	Housing mix, density and affordability
	CP6	Managing Travel Demand
	CP7	Biodiversity
	CP8	Thames Basin Heaths Special Protection Area
	CP9	Scale and Location of Development Proposals
	CP10	Improvements to the Strategic Transport Network
	CP11	Proposals outside development limits (including countryside)
	CP17	Housing delivery
	CP19	South of the M4 Strategic Development Location
Appendix 7 – Additional Guidance for the Development of Strategic Development Locations		
Adopted Managing Development Delivery Local Plan 2014	CC01	Presumption in Favour of Sustainable Development
	CC02	Development Limits
	CC03	Green Infrastructure, Trees and Landscaping
	CC04	Sustainable Design and Construction

	CC05	Renewable energy and decentralised energy networks
	CC06	Noise
	CC07	Parking
	CC08	Safeguarding alignments of the Strategic Transport Network & Road Infrastructure
	CC09	Development and Flood Risk (from all sources)
	CC10	Sustainable Drainage
	TB05	Housing Mix
	TB07	Internal Space standards
	TB09	Residential accommodation for vulnerable groups
	TB12	Employment Skills Plan
	TB21	Landscape Character
	TB23	Biodiversity and Development
	TB24	Designated Heritage Assets
	SAL05	Delivery of avoidance measures for Thames Basin Heaths Special Protection Area
Shinfield Parish Neighbourhood Development Plan		
	Policy 1	Location of Development
	Policy 2	General Design Principles
	Policy 3	Sustainable Development
	Policy 4	Accessibility and Highway Safety
	Policy 5	Parking
	Policy 6	Trees, Hedgerows and Woodlands
	Policy 7	Biodiversity
	Policy 8	Flooding
	Policy 9	Community Assets
	Policy 10	Community and Sports Facilities
	Policy 12	Broadband Provision
Supplementary Planning Documents (SPD)	BDG	Borough Design Guide
	SDC	Sustainable Design and Construction Supplementary Planning Document (28 May 2010)
	SoM4SPD	South of the M4 Strategic Development Location Supplementary Planning Document (October 2011)

	IDC	Infrastructure Delivery and Contributions Supplementary Planning Document (October 2011)
	AH	Wokingham Borough Affordable Housing Supplementary Planning Document (2 June 2011)
Other relevant policy		DCLG – National Internal Space Standards

PLANNING ISSUES

Description of Development:

1. The proposal is for full planning application for the erection of 249 dwellings, new public open space, landscaping, surface water attenuation, access and associated works. Principal access to the site would be from the roundabout adjacent to the Science Park on the ERR and through the Littlebrook development and an alternative access would be via Cutbush Lane. Extensive landscaping is proposed adjacent to the ERR in order to limit the visual impact of the development.

Principle of development and infrastructure delivery

2. The application site forms part of a larger area designated under the Wokingham Borough Core Strategy as the South of the M4 Strategic Development Location (SDL).
3. Wokingham Borough Core Strategy policy CP17 establishes a requirement to provide at least 13,487 new dwellings with associated development and infrastructure in the period 2006-2026. The majority of this new residential development will be in four SDLs, of which South of the M4 SDL is one of these. It is anticipated that a phased development incorporating approximately 2,500 dwellings and associated infrastructure will be delivered on 77 hectares of land within the South of the M4 SDL (Appendix A7.14). It should be noted that the approximate numbers set out in the adopted policies and Supplementary Planning Documents are not intended to be maximum figures.
4. Core Strategy policy CP19 is amplified by Appendix 7 of the Core Strategy, the South of the M4 Strategic Development Location Supplementary Planning Document (SPD) and Infrastructure Delivery and Contributions SPD, which addresses the associated infrastructure impacts across the whole Borough. These documents establish a requirement for a sustainable, well designed, mixed use development and make clear that a co-ordinated approach to the development of the SDL will be required to deliver the necessary infrastructure, facilities and services to meet the needs of the expanded community. The development site sits within the development limits as outlined in Policy 1 of Shinfield Parish Neighbourhood Plan. Whilst it is acknowledged that a centre was included for this land in the South of the M4 SPD, due to viability and a preference to focus retail facilities at the existing village centre, it is not considered that this would be a viable location and could undermine the delivery of Shinfield Centre. The Spatial Framework Plan represents guidance and utilising the entire site for housing would make best use of land and help to maintain housing land supply.

5. The site was allocated for housing as part of the housing allocation for the South of the M4 Strategic Development Location (SDL) and was identified in the Spatial Framework Plan in the adopted Supplementary Planning Document (SPD). In total, 375 units were identified for north and south of Cutbush Lane. To date, 126 units were approved for the north of Cutbush Lane at the Littlebrook site and the majority of these dwellings have been delivered. The balance proposed by the current application is 249 units which makes up the SPD allocation.
6. It is important to note that there was a timing restriction for the land put forward for the Shinfield West appeal by the University of Reading. This had a restrictive clause within the Unilateral Undertaking to not develop on land south of Cutbush Lane until 2026. The council has however removed this clause and agreed to release the land through an Executive Decision which is appended to this report 27th July 2017. This also released land south of the current development parcel to the north of Arborfield Road for housing to be delivered along the entire western edge of the Eastern Relief Road (ERR). This ensure a comprehensive and well planned development and will help the council meet and exceed its requirement for a 5 year housing land supply and should help to limit speculative sites coming forward. The ongoing supply of housing is a requirement of National Policy. In addition, the release of both sites allows for a comprehensive approach to the delivery of housing on the eastern side of Shinfield. An application for the north of Arborfield lane is likely to be submitted in 2019. The overarching vision of the SDL is to provide a co-ordinated approach to the delivery of infrastructure and services ensuring that developments are of a high quality and are sustainable. This includes the provision of schools, community facilities, good quality open space and appropriate local transport and links. The principle of development in this location has been established through its allocation by policy CP19 of the Core Strategy and through the Spatial Framework Plan within the South of the M4 Supplementary Planning Document. In addition, the site is located within the development limits as identified in Policy 1 of Shinfield Parish Neighbourhood Plan. Whilst it is acknowledged that a small area of the site is located outside of the development limits, this area will serve as a play area / pumping station. However given the likelihood of this land coming forward in the future and the proximity of the ERR which forms a barrier to development, the intended use, public benefits associated with the housing delivery and landscaping, it is considered that in this instance the proposal is acceptable and does not result in harm to the character or appearance of the local area or represent a departure from planning policy.
7. The application will include CIL payments and heads of terms for an S106 legal agreement. The legal agreement will secure the coordinated delivery of the infrastructure necessary to support the development. These included contributions towards off-site infrastructure and services such as roads, education, sports facilities, community facilities and green infrastructure as set out in the recommendation and potentially amended through the members update. In addition, affordable housing and an affordable housing contribution has been agreed in accordance with the Core Strategy.

Site description

8. The site is located on the eastern edge of Shinfield and the land has been previously used for agriculture although is grassland at present. The site is bound to the north by Cutbush Lane and to the east by the ERR. To the west is existing

housing on Oatlands Road which back onto the site and footpath 7. To the south is open land and a permissive footpath is located towards the southern boundary. The site slopes gently down from broadly from the north west to the south east. There is a hedgerow at the north and south of the site and the significant majority of this will be retained.

Affordable and specialist housing

9. Core Strategy Policy CP5, *Housing mix, density and affordability*, amplified by MDD LP Policy TB05: *Housing Mix*, South of the M4 SPD Design Principle 2b, the Infrastructure SPD and the Affordable Housing SPD, requires a mix of tenures, including 35% affordable housing within SDLs.
10. The application proposes 87 on site affordable dwellings together with a commuted sum for part of a unit that would secure the 35% affordable housing requirement by Policy CP5. The affordable housing officer is satisfied with the strategy, proposed mix and distribution of affordable dwellings.

Dwelling mix

11. MDD LP Policy TB05 requires an appropriate housing mix which reflects a balance between the character of the area and the current and projected needs of households.
12. The application proposes the dwelling mix outlined in table 1:

Table 1: Dwelling mix

Dwelling Type (bedrooms)	Number provided on site	2 Storeys	2.5 Storeys	3 Storeys
1 bed apartment	22			22
2 bed apartment	19			19
2 bed house	67	67		
3 bed house	92	92		
4 bed house	49	49		

13. Officers raise no objections in terms of the dwelling mix. Taking the constraints of the site into account, this approach is considered acceptable.

Masterplan – Layout

14. Core Strategy Policies CP1, *Sustainable Development* and CP3, *General Principles for Development* requires high quality design that respects its context. This requirement is amplified by MDD LP Policies CC03, *Green Infrastructure, Trees and Landscaping* and TB21, *Landscape Character* and South of the M4 SPD which requires development proposals to protect and enhance the Borough’s Green Infrastructure, retaining existing trees, hedges and other landscape features and incorporating high quality - ideally native – planting as an integral part of any scheme, within the context of the Council’s Landscape Character Assessment.
15. Core Strategy policy CP19 sets out the concept rational for the design parameters for the South of the M4 SDL and these are outlined in further detail in Appendix 7 of the Core Strategy. This states that:

a. *Development in a series of locations around the periphery of the three villages is required. This approach should consider the relationship between the current built areas and the open countryside. Opportunities to form new edges to the existing villages exist, allowing a managed transition between urban and rural.*

16. Policy 1 of Shinfield Parish Plan sets out the development limits. These follow those identified in the South of the M4 SPD. The development site is mostly within these limits with the exception of the play area and pumping station. Whilst these elements are located in countryside, these are not particularly urbanising features and the benefits of the housing delivery together with landscaping will minimise the impact of these on the countryside. It is therefore considered that there is no departure from planning policy.

17. Policy 2 sets out general design principles and requires that development is complementary to the built environment surrounding the development site and layouts should acknowledge existing landscape constraints. Policy 6 is concerned with Trees, Hedgerows and Woodland in new development and sets out a landscape led design approach with retention and appropriate buffers to existing important vegetation.

18. Further design guidance is provided by the South of the M4 SPD in section 4. Design principle 3 is concerned with character and states that there should be diversity and distinction in the SDL which should be enhanced through the application of character typologies.

19. The application is accompanied by a Design and Access Statement (DAS) which includes a character appraisal of the existing built form within the local area and uses these to inform design principles for the proposed houses and the layout. This identifies four character areas for the site which respond to the site constraints and opportunities to inform the layout. It should be noted that the applicant has made significant amendments to the layout in response to feedback from Officers. This has resulted in considerable improvements in the design approach for the development and the layout is appended to the report.

20. On the eastern part of the development, is the Eastern Fringe character area. The applicant has carefully considered the layout of the site in regard to the landscape constraints of the surrounding as the site is adjacent to the Loddon River Valley which is recognised in the Wokingham Landscape Character Assessment. For this part of the site, the applicant has provided lower density housing and a landscape buffer to the ERR which will be planted with a significant amount of trees. As the landscape setting of this part of the site is reasonably sensitive, revisions to this part of the site have been made to lower housing density here and improve the edge of the development.

21. For the site frontage that is directly to the south of Cutbush Lane, the applicant has applied a similar style of built form that currently exists on the road called the Cutbush Lane Frontage. This follows on the established built form from the Littlebrook site on north of Cutbush Lane and linear style of development which predominantly characterises Cutbush Lane. For this element there is a separate access that would serve twelve dwellings. The layout of the houses mean that

they front on to Cutbush Lane whilst at the same time, retaining the significant majority of the hedgerow which will help soften the impact of the new dwellings.

22. The main spine road is proposed at the centre of the site which ultimately links onto *Hawthorn* the development road serving the Littlebrook development site to the north of the site. The approach to the layout of this part of the site is to deliver the main primary road through the site. Houses are well set back from this street and there is a strong landscaping for the central street which has been called The Green Ribbon character area. In addition there is a large area of open space as you enter the site which helps to soften the built form. The road has been designed to be 6.1m wide which will accommodate buses should these route through the site in future years. Visitor parking is accommodated in bays which helps to reduce the visual impact of cars. Once the landscaping becomes established the principle road should have an avenue style appearance.
23. Behind the principle street and other character areas is a series of smaller roads called The Lanes character area. This area runs along the rear of the site on the western boundary. This area has a less formal approach although the applicant has used a good range of house-types to ensure there is diversity in design and opportunities to introduce landscaping has been incorporated where this is possible. Whilst requests have been made by representations for further landscaping in this area, due to a sewer easement, it was not possible to plant further trees. The amenity impact of the development is further discussed below in paragraphs 37-48.
24. At the south of the site there is an area of play space which has been designed to include a LEAP and MUGA. The delivery of this will ensure that there is play equipment for infants through to teenagers and is in accordance with the relevant standards. This has good links to the existing residents as there is a permissive footpath runs through to Oatlands Road.
25. The overall approach to the layout is acceptable and the applicant has worked with Officers to overcome design issues raised by the original submission and made significant amendments. The site has been planned to capitalise on the constraints and incorporate these into the design where possible. A strong landscape buffer will be delivered adjacent to the ERR and this lessens the impact on the Loddon River Valley. Overall the layout has been well considered and represents sound urban design principles incorporating perimeter blocks which will maximise natural surveillance to the public realm and this represents a high quality design approach.

Masterplan – Design and appearance

26. In terms of the detailed design of the buildings, the South of the M4 SPD recommends that the existing built form should be used to inform the design which is echoed by the Shinfield Parish Plan policy 2. As stated above, the applicant has undertaken a character appraisal of the existing built form to help guide their design approach for the new development which is set out in the DAS.
27. The application of the character areas helps to establish diversity in the built form as different approaches and finishes are applied to the house types of each

character area. This helps build a clear and legible neighbourhood and ensures that there is a good balance of variety within the development.

28. In regard to building heights, the significant majority of the development will be two stories dwelling houses. There will be four apartment blocks which are three stories in height but these have been well located within the development to ensure that the heights of the buildings diminishes towards the edge of the site. This also prevents impacts to adjoining existing residential amenities. This helps to provide a degree of transition between the built form and countryside and locating these away from existing residential development decreases the amenity impact for existing residents.

29. The overall design approach for all of the dwellings provide continuity in approach and is considered to respect the context of the site location and advice provided by the NPPF, policies CP1 and CP3, Policy 2 of the Shinfield Parish Neighbourhood Plan together with the South of the M4 SPD and Supplementary Planning Guidance and is therefore acceptable.

Masterplan – Density

30. Design principles 3 (a-g) are outlined in the South of the M4 SPD and are concerned with the character of the development, including the density of development. The Core Strategy approach for the South of the M4 SDL in respect to density is outlined in Appendix 7 which indicates an average density of 30-35dph across the SDL.

31. The overall density proposed by the application is 27dph. The density of the site is higher towards the centre of the site and near the apartments which is to be expected. The approach means that there is a degree of transition from the site to the countryside / existing residential areas on the western boundary. As this has been well considered within the site layout, the proposed density is acceptable.

Masterplan – Landscaping

32. As advised, the applicant has carefully considered and responded to officers advice in regard to the eastern boundary of the site. The density of this element is lower and a road has been removed here to make a greater buffer from the built form to the open space between the site and the ERR. A significant amount of landscaping has been incorporated in this space and through working with engineers, the SUDs pond has been designed to allow for planting along the banks. This has resulted in a strong landscape buffer between the site and the ERR which is supported by the landscape officer.

33. Within the site itself, there are a couple of pockets of open space on adjacent to Cutbush Lane and within the site towards the main avenue. Planting has been incorporated along the avenue and in areas where there is space to include this. The planting will be secured by condition 19.

34. The landscaping strategy and the layout has been assessed by the Landscape Officer who raises no objections subject to conditions 18-23 and supports the approach.

Masterplan conclusion

35. The design approach taken to inform the layout and appearance of the buildings is considered to respond well to the existing landscape features, context of the site and in respect to the surrounding area. The applicant has strongly demonstrated a comprehensive approach to the delivery of the houses under consideration through the application of the principles established in the DAS within the layout and design choices of the units themselves.
36. Overall, it is considered that the proposal would not have a significant detrimental impact on the character of the area and is broadly in accordance with the principles of the relevant planning policies and supplementary planning advice.

Residential amenity: the impact upon existing neighbouring properties

37. Core Strategy policy CP3 requires that new development should be of a high quality of design that does not cause detriment to the amenities of adjoining land users. Policy 2 of the Shinfield Parish Plan seeks to ensure that new development does not harm existing residential amenity
38. In respect to separation standards, these are set out in section 4.7 of the Borough Design Guide. The layout means that the proposed dwellings would in the main, front onto the rear gardens of Oatlands Road which is the area closest to the application site. The Borough Design Guide does not have a prescribed separation distance for this relationship but this would essentially be the same as a back to back relationship given that the separation standards are applied in respect to privacy and sense of enclosure). On this basis the back to back threshold (22m minimum) will be applied. For existing dwellings that side onto the development site, again no prescribed standard is identified in the Borough Design Guide. For this relationship, the back to flank minimum separation distance of 12m will be applied as once again, these are identified within in respect to privacy and enclosure.

65 Cutbush Lane, 1 and 2 Hawthorne, Oak Tree Barn and Barn Close Cottage

39. The level of separation of the proposed houses (plots 1 - 2 and 236-246) exceeds any threshold set out in the Borough Design Guide and at a minimum, this is 38m which is sufficient to prevent any significant amenity harm.

66 Cutbush Lane

40. Plot 1 is set at an oblique angle to no.66 Cutbush Lane which limits the impact of overlooking to the main dwelling and amenity space. There is around 13m to the common boundary of this site and given the siting of plot 1, no significant amenity impacts are envisaged. For plot 32 and 34, these would be sited towards the end of the rear garden of no.66 and have a front to flank relationship. There would however be around 14m separation between plot 32 and the flank boundary and this relationship is considered acceptable.

90 Oatlands Road

41. No. 90 Oatlands Road sides onto the front elevations of plots 26-30. The level of separation to the common boundary is around 18m and 21m to the main existing dwelling. This is above the flank to rear standard identified by the Borough Design Guide. Landscaping is also proposed along the side elevation of no.90 for much of the main part of the rear amenity space. Further landscaping was requested by the occupants of no.90 to extend this along the entire boundary but

due to a sewer easement, this cannot be achieved. The level of separation for to the proposed houses is well above the minimum for a side to flank and therefore is considered acceptable.

3 and 4 Dearlove Place

42. Plots 21 -25 would front onto the rear elevation of 3 and 4 Dearlove Place and the level of separation between the proposed dwellings and existing houses varies from around 27m to 29m. This is in excess of the back to back relationship outlined in the Borough Design Guide and the relationship is considered acceptable.

21 Oatlands Chase

43. Plot 20 would face the flank elevation of 21 Oatlands Chase. There is however 23m separation between the flank wall of the existing house and proposed dwelling which exceeds both the rear to flank and back to back standards. Furthermore there is around 17m to the flank boundary of this site and the relationship is considered acceptable.

23-39 Oatlands Chase

44. All of the proposed dwellings (plots 33-42) would front onto the rear of no's 23-39 Oatlands Chase. The level of separation between the proposed dwellings and existing built form varies from around 25-28m which exceeds the Borough Design Guide standards. As such, the relationship is acceptable.

41-47 Oatlands Chase

45. Plot 59 would side onto the existing houses. There is however a minimum of 23m between the side elevation of the proposed dwelling and existing residential development. This exceeds both the front to flank and rear to rear separation standards.
46. As outlined above the development is in accordance with the separation standards outlined in the Borough Design Guide. Where there is no prescribed standard, a comparable relationship has been applied. Whilst there is a degree of ground remodelling required for drainage, the degree of this is not considered significant and given the onsite conditions, the relationship between existing residents and the new dwellings is acceptable. It is therefore considered that there would be no significant issues arising in terms of loss of light, overbearing or overlooking. Planning conditions 5-7 will ensure that this relationship remains in the future.
47. Given these relationships and the onsite conditions, that is comparative levels across the site and adjoining land, it is considered that there would be no significant amenity impacts to existing residents in terms of overlooking, loss of light and overbearing.

Residential amenity – noise

48. Whilst it is acknowledged that there would be a greater level of activity in the area arising from the delivery and future occupation of the housing, this would not be to an extent that is unusual in a residential area. As the site has been identified as being suitable for housing, the principle of residential development in this area has been established. Construction activities would be temporary and in order to reduce the impact for the surrounding residents, these can be

controlled by conditions 17 30 and 31. It is considered that the proposal would not result in any significant harm to the amenity of the existing residents and is in accordance with policies CP1 and CP3 and supplementary planning guidance.

Residential amenity: outdoor amenity space

49. The Borough Design Guide sets a minimum garden depth of 11m. In terms of the submitted masterplan, the proposed layout complies with these requirements. Overall, with the level of outdoor amenity space is considered to provide acceptable levels of private amenity space and privacy for the future occupants of the dwellings.

Internal Space Standards

50. With respect to internal floor space of the units, since 1st October 2015 national space standards have been in place as the Technical housing Space Standards. The dwellings comply with National Space standards, meeting requirements for overall area, bedroom sizes and storage allowances. The apartment units have also been designed to Lifetime Homes requirements.

Security

51. The layout takes into account security and all of the communal areas of open space are well overlooked by the proposed housing, as are the areas of parking. The layout also means that there are back to back relationships which restricts access to private amenity space. The layout and design are considered acceptable.

Noise

52. Core Strategy Policy CP1 and MDD LP Policy CC06 direct development away from areas where noise would impact upon amenity and require mitigation where noise cannot be completely avoided.
53. The main source of noise to the development site would be from the ERR. Noise surveys have been undertaken since the opening of the ERR which concluded that a buffer from the proposed dwellings could be as narrow as 5m with acoustic measures. The buffer proposed is some 35m which is well above the threshold identified and Environmental Health have assessed this aspect and raise no objections.

Sustainable Design and Construction

54. Core Strategy Policy CP1 requires development to contribute towards the goal of achieving zero carbon development by including on-site renewable energy features and minimising energy and water consumption. MDD LP policies CC04, CC05 and the Sustainable Design and Construction Supplementary Planning Document (May 2010) also emphasise this. Policy 3 of the Shinfield Parish Neighbourhood Plan also echoes these principles.
55. The Design and Access Statement makes a commitment towards sustainable design and construction. In order to ensure that this is commitment is achieved and in accordance with the requirements part L of the building regulations. The council seeks both fabric first approach (such as housing insulation and low energy appliances) and on site renewables and as such, it is not necessary to duplicate by way of condition.

56. A key element of new housing should be to reduce water consumption and waste. As part of delivering sustainable development on the site, the Council expects measures to reduce water consumption to be included within the development. In the case of water, this can be achieved by the inclusion of aerated taps, shaped low capacity baths, dual flush toilets, low flow rate showers and the provision of water butts. Again building regulations covers this aspect and a condition is not required.

Access and movement

57. The NPPF seeks to encourage sustainable means of transport and a move away from the reliance of the private motor car. Core Strategy policies CP1, CP4, CP6 and CP10 broadly echo these principles and indicate that new residential development should mitigate any adverse effects on the existing highway network. The application is accompanied by a Transport Assessment (TA) which assesses the impact of development, both in terms of the traffic generated by the development itself and in the context of the cumulative impact of additional residential development in respect of the wider SDL.

58. Policy 4 of the Shinfield Parish Neighbourhood Plan requires development to provide good accessibility by car, cycle and foot and ensure highway safety. Encouragement of use of public transport is also promoted. Policy 5 of the Shinfield Parish Neighbourhood Plan sets out parking standards. These should be in line with WBC standards and well set out with good surveillance to parking courts.

59. To encourage use of non-car modes, a contribution has been sought to assist delivery of the South of the M4 bus strategy. The development also will include improved permeability within and from/to the site by walking and cycling. The design of the development will also include a 6.1 m wide north-south corridor which would enable buses operations. Whilst no bus stops are proposed on site, the location of bus stops which are identified for delivery on the Littlebrook site and potential of stops to the south of the site would improve access to buses.

60. Assessment of local junctions has been modelled and has been assessed as part of the Transport Assessment. This concluded that the traffic from the development would not have an adverse impact on the operation and safety of the local highway network subject to upgrades to the Eastern Relief Road Magpie and Parrot roundabout which will be secured through the S106.

Site Access

61. In regards to access, the principle access to the site would be from the roundabout adjacent to the Science Park on the ERR through the adjacent Littlebrook Development to the north. Development traffic would also be able to use Cutbush Lane so there would be dispersed movements to each point of access however the levels of traffic on Cutbush should remain low as this is not the main road to the wider network. The road in Littlebrook has been designed to accommodate buses and it is 6.1m wide which is sufficient for the development traffic. Restrictions on the use of the access point adjacent to the Science Park for buses only have been removed in consultation with the Highways Officer.

62. The Executive Decision to release the land to the south of the site means that there is a strong likelihood that a central spine road will be delivered between the

site and Arborfield Road. This will potentially provide an additional route for future users and help distribute vehicles.

63. The road layout of Cutbush Lane will be altered in order to provide priority for the link in and out from the Science Park and the layout has been amended to reduce the level of hedgerow loss. This has been assessed by the Highways Officer and they are satisfied with the principle road hierarchy.

64. As outlined above, there will be a secondary access to the site serving 12 units towards the eastern end of Cutbush Lane. This approach is acceptable and means that frontage development will be delivered on Cutbush Lane.

65. A further emergency access is proposed adjacent to the ERR which is acceptable and helps limit the loss of hedgerow.

66. A Stage 1 Road Safety Audit with Designer Comments was submitted and concluded that the design was safe to all users. Further road safety audits will be carried out at appropriate times through the design process.

Access and movement - Site layout / Pedestrian and Cycle Links

67. The principle access road will run broadly through the centre of the site. This has been designed to be 6.1m wide to accommodate buses. Ultimately it is envisaged that the road will link through the parcel to the south to Arborfield Road and whilst this is not a matter for consideration for the current application, providing the additional width will safeguard this route for buses. In addition the stretch of Cutbush Lane from the new junction eastwards will be narrowed to improve access for pedestrians.

68. The road hierarchy beyond the main road diminishes to a series of secondary and tertiary streets. The secondary routes include pavements and these lead off onto the tertiary streets which are shared surface.

69. For the eastern side of the site, a pedestrian and cycle link is proposed which could link onto the Arborfield Road and through to the Science Park (via Cutbush Lane west) when an application comes forward for the southern parcel. In the interim a temporary footpath will be provided through the southern parcel. The scheme will therefore help to secure off road pedestrian and cycle links as currently none exist on the ERR and these will be secured through the S106.

70. A contribution for sustainable improvements on the A327 has also been sought.

71. The layout also secures east / west connectivity at the south of the application site. The permissive footpath which links onto the ERR through to Oatlands Road will be retained in its current alignment although links onto estate roads will be provided which will provide bonded routes to the play space

72. Overall the layout will create a place with clear and legible links within it and to the surrounding area. The application has been assessed by the Highways Officer and they are satisfied with the layout. The application has taken into account the probability of land coming forward to the south as released by the Executive Decision and it would not prejudice development of this parcel which demonstrates a comprehensive approach.

Access and movement - Car parking

73. In terms of parking, in total parking spaces provided by the application (including visitor and unallocated but excluding garages) is 511 which equates to a provision of 2.05 spaces per dwelling. An additional 133 garage spaces will also be provided so if these are counted in accordance with the Wokingham Parking Standards, these provide an additional 66 spaces bringing the parking figure up to 2.32 per dwelling. The parking provision is broken down in table 2.

Table 2 Parking provision

On plot parking	Visitor / Unallocated	Garages
412	99	133

74. The proposal is considered to be in accordance with the parking standards and there are an additional 99 visitor / unallocated spaces have been incorporated. The unallocated and visitor parking is well distributed throughout the development site. The provision of electric vehicle charging points will be secured by condition.

Access and Movement - Cycle storage

75. The proposed level of cycle storage is in line with Borough standards. The cycle storage for the blocks of flats will be in communal storage areas which are of sufficient size to ensure the number of spaces can be easily accessed. For the houses with single garages the cycle storage would need to be in rear gardens. Details of the cycle spaces have been secured by condition.

76. For the properties with double garages, cycle storage can be easily contained in these garages.

Access and Movement - Access to public transport

77. As advised, the main spine road has been designed to be 6.1m in width which would allow for buses to route directly through the site if the land to the south of the development is brought forward. In the meantime, bus stops are being provided on the Littlebrook estate which will be within the 400m catchment for each house. With the south of the M4 Bus Strategy delivering high frequency bus services, there will be a good alternative to the private motor car. A contribution to deliver this service has been secured in the s106 agreement.

Flooding and Drainage

78. Core Strategy Policy CP1 and MDDL Policies CC09 and CC10 establish that new development should avoid increasing and where possible reduce flood risk (from all sources) by first developing in areas with lowest flood risk, carrying out a Flood Risk Assessment (FRA) where required and managing surface water in a sustainable manner. These requirements are reinforced by the South of the M4 SPD Design Principle 1c(ii) which requires provision of a comprehensive system for water management, which takes account of existing features and includes proposals for effective sustainable urban drainage (SUDS), measures to avoid flood risk and new ponds. Policy 8 of the Shinfield Parish Neighbourhood Plan

echoes these principles and looks to retain existing watercourses in new development.

79. The area of the site identified for residential development is entirely in Flood Zone 1 where the risk of flooding is low and all uses, including more vulnerable residential uses and less vulnerable uses (the proposed school, shops and community uses) are appropriate. There is a small area of the site in Flood Zone 2 to the south east adjacent to the ERR although this area is to be used for landscaping. It should be noted that the site was allocated in the Core Strategy, which was subject to a Strategic Flood Risk Assessment.
80. The application is supported by a Flood Risk Assessment and Environmental Impact Assessment in regard to flooding which assesses the impact of flooding on the application site. This identified flood risks across the whole site and propose measures to manage surface water runoff including the 1 in 30 year and 1 in 100 year storm events (+40% for climate change) for the areas of residential development.
81. In order to ensure that the increase in surface water runoff associated with the additional built form is managed, a strategic approach to drainage is proposed. This included a SUDS ponds in the eastern landscape area adjacent to the ERR. Additional drainage management is also proposed in the form of geocellular storage.
82. The Flood Risk Assessment and Environmental Impact Assessment have been assessed by the council's Drainage Officer. They have raised no objection to the proposed package of measures in terms of the impact to existing neighbouring residents and future occupiers of the site. On this basis the proposals are considered acceptable subject to conditions 32 and 33.

Thames Basin Heaths Special Protection Area

83. The Thames Basin Heaths Special Protection Area (SPA) was designated under European Directive due to its importance for heathland bird species. Core Strategy policy CP8 establishes that new residential development within a 7km zone of influence is likely to contribute to a significant impact upon the integrity of the SPA. The South of the M4 SDL falls within this zone of influence and, in accordance with Core Strategy policy CP17 and Design Principle 1c (vi), mitigation in the form of Suitable Alternative Natural Greenspace (SANG) is being provided. An appropriate assessment has been undertaken and is annexed to this report.
84. The development will be mitigated by land at the Ridge where there is sufficient land to accommodate the development. Since several other developments are reliant on land at the Ridge, the S106 will require the entire Ridge SANG to be open upon occupation any dwelling to the south of Cutbush Lane subject to the current application. The implementation of this will be controlled by the S106 agreement and the SANG will need to be open for public use prior to the occupation of any of the dwellings. Full planning permission has been approved for the SANG area and as such this meets the Natural England's *'Guidelines for the Creation of Suitable Alternative Natural Greenspace' (SANG) (2008)* in terms of having all the essential features required to attract recreational users away

from the SPA. Natural England and the Biodiversity Officer are satisfied with this approach.

Ecology

85. Core Strategy Policy CP7, carried forward by MDD LP Policy TB23, requires appropriate protection of species and habitats of conservation value. Policy 7 of the Shinfield Parish Neighbourhood Plan seeks mitigation and measures to enhance biodiversity. Design Principle 1b of the South of the M4 SPD (i-ii) is concerned with protection of ecological habitat and biodiversity features, together with mitigation of any impacts that do arise. A detailed Ecological Mitigation Strategy has been submitted and demonstrates that the site layout has considered impacts on ecological permeability. In addition no advisee impacts are envisaged to flora and fauna.

86. In terms of existing vegetation, the applicant has demonstrated that there would be an adequate buffer zone from the proposed development to these features. The Biodiversity Officer has assessed the application and is satisfied that there would no significant impact on ecology subject to conditions 34-36.

Heritage

87. Policy TB24 of MDD LP policy seeks to ensure that development conserves and where possible, enhances the important character and special architectural or historic interest of listed buildings. The nearest listed building to the development site is Cutbush Barn which is on the eastern side of the ERR. This is considered acceptable and would not have a significant adverse impact on the character and setting of the listed building.

Archaeology

88. Core Strategy Policy CP3 and MDD LP Policy TB25 require the archaeological impact of development to be taken into consideration. The Archaeological Officer has assessed the application and requested condition 37.

CONCLUSION

The application is compliant with the Council's adopted policies and those within the Shinfield Parish Neighbourhood Plan and guidance for development within the South of the M4 SDL. It is considered that the application will deliver high quality development and related infrastructure in accordance with the Council' spatial strategy and vision and therefore can be recommended for approval.

This page is intentionally left blank

TITLE	Housing Land Supply and Reserve Sites
FOR CONSIDERATION BY	The Executive on 27 July 2017
WARD	None specific
DIRECTOR	Josie Wragg, Interim Director of Environment
LEAD MEMBER	David Lee, Executive Member for Strategic Planning and Highways

OUTCOME / BENEFITS TO THE COMMUNITY

Maintaining a five year housing land supply will help ensure the effective management of future development so that it occurs in suitable and sustainable locations and that it is well-designed.

RECOMMENDATION

The Executive is advised to:

- 1) note the current five year housing land supply position;
- 2) approve the release of the allocated reserve sites to boost housing supply in the short term;
- 3) approve the invitation of an application on land south of Cutbush Lane.

SUMMARY OF REPORT

National planning policy and guidance requires the council to maintain supply of land sufficient to provide five years' worth of housing.

The council's latest assessment undertaken as of 31st March 2017 indicated a 5.27 year supply against the level of housing need identified by the Berkshire (including South Bucks) Strategic Housing Market Assessment (SHMA).

The level of supply and housing need have been, and continue to be, challenged through appeal decisions. This means that whilst we can show a five year housing land supply in the annual update against the SHMA, Inspectors who accept partial updates to the deliverable land supply or find that housing need is higher, are likely to find a periodic deficit as not all sources of supply are considered.

The five year housing land supply position coupled with the previous appeal decisions continue to place undue pressure on the council from opportunist appeals.

Adopted planning policy SAL03 in the Managing Development Delivery plan allocates six reserve sites for residential development that could be released in the absence of a five year housing land supply. The area of land south of Cutbush Lane is currently constrained prior to 2026 by a unilateral s106 agreement. This report recommends their release to boost housing supply in the short term.

Background

Introduction

The National Planning Policy Framework (NPPF) sets out the government's planning policies. With regards to housing one of the key requirements is that local planning authorities should:

*"...identify and update annually a supply of specific deliverable sites sufficient to provide five years' worth of housing against their housing requirements with an additional buffer...moved forward from later in the plan period to ensure choice and competition in the market for land"*¹

In the absence of a five year supply the NPPF advises that housing applications should be considered in the context of the presumption in favour of sustainable development and that housing supply policies should not be considered up-to-date.²

Housing need

The Berkshire (including South Bucks) Strategic Housing Market Assessment (published in 2016) (hereafter referred to as the SHMA) is the latest, full assessment of housing needs covering Wokingham Borough.³ It was jointly commissioned by the six Berkshire local authorities and the Thames Valley Berkshire Local Enterprise Partnership and has been agreed by each party under the Duty to Cooperate.

The SHMA identified housing need for Wokingham Borough to be 856 additional homes per year between 1 April 2013 and 31 March 2036.

The level of housing need has been subject to challenge through the planning application appeal process. Inspectors, when determining two recent appeals, concluded that housing need was higher at 862-890 or 894 additional homes per year. The adjustments were made to reflect worsening affordability and past under delivery in housing.

An appellant in a recent planning appeal progressed the position that housing need should be 960 additional homes per year. Further, a submission by the University of Reading to the Local Plan Update Issues and Options consultation suggested housing need should be 1,120 additional homes

Appeal	Inspector's Objectively Assessed Need - Dwellings per Annum (DPA)	SHMA Objectively Assessed Need – Dwellings per Annum	Difference
Stanbury House	862 dpa	856 dpa	6 dpa

¹ NPPF, paragraph 47.

² NPPF, paragraph 14 and 49.

³ Planning Practice Guidance: Housing and Economic Land Availability Assessment advises that housing requirements in up-to-date local plans should be used as the starting point for calculating the five year housing land supply, where a local plan has become out dated, information provided in the latest full assessment of housing needs should be considered (ID-3-030-20140306).

	890 dpa		34 dpa
Park Lane and Broughton Farm	894 dpa		38 dpa
Barkham Road	960 dpa		104 dpa
University of Reading	1,120 dpa		264 dpa

The government in the housing white paper⁴ has stated their intention to introduce a new standard methodology for calculating housing needs. This is likely to err on the side of higher housing supply.

Housing land supply

The council's latest assessment of the housing land supply was undertaken as at 31st March 2017. This calculated a deliverable supply of 6,563 dwellings. This equates to a 5.27 year housing land supply when calculated against the recommended housing need figure of 856 additional homes from the SHMA plus the 20% additional buffer required by the NPPF.⁵

Through the appeal process, appellants have disputed the deliverable housing supply across a number of specific sites and advanced the argument that a lapse / non-implementation rate should be applied.

Notwithstanding the clear requirement to assess the housing land supply position annually, Inspectors, at various appeals, have reached their conclusion based only on partially updated information, i.e. only taking into account more recent information on specific developments without considering projected delivery on other sites, or the number of dwellings being completed or granted elsewhere.

The number of dwellings completed in 2016/17⁶ was 933 net additional dwellings, an improvement over previous years. Notwithstanding the latest assessment showing a five year housing land supply is maintained against the housing need identified in the SHMA and the improvements to the process to ensure its robustness, the supply has less of a buffer than when assessed at 31st March 2016. We remain under intense pressure because the latitude given to Inspectors.

Analysis of Issues

With the five year land supply position similar to that held over the last few years, the council is likely to see continued challenged from speculative landowners and developers.

a) The challenge to good planning

The council's Core Strategy approach to development was to concentrate the majority of residential development in four Strategic Development Locations, each delivering well

⁴ [Fixing Our Broken Housing Market, DCLG February 2017.](#)

⁵ Paragraph 47 of the NPPF requires an additional buffer to be applied on top of the housing requirement to ensure choice and competition in the market for land. The size of the buffer to be applied is either 5% or 20% depending on past delivery. At present, the council applies the 20% buffer based on under delivery against past housing requirements.

⁶ From 1 April 2016 to 31 March 2017.

planned, high quality and infrastructure rich communities. The council's New Homes Survey shows that public perception of SDLs is showing a steady increase in satisfaction as SDLs are built and occupied.

Historically the approach to development was dispersed, incremental development. Development lacked supporting infrastructure and quality. This resulted in residents seeing pressure building on existing infrastructure and services, reducing accessibility, and leading to further congestion on the existing road network, which lacked sustainable expansion, as people travelled further.

A number of allowed planning application appeals have undermined the plan-led, concentrated development approach, and the good planning benefits that led to the council electing this option.

b) The likely increase in housing need

Inspectors, in two planning appeals,⁷ have concluded that in their opinion the level of housing need is higher than that calculated by the SHMA. The introduction of a standard methodology for calculating housing need may remove lengthy debate about the validity of the calculation, but it is likely that any methodology will err towards higher housing supply as this is government policy.

Any substantial increase in housing need would reduce the five year land supply. The absence of supply would undermine the plan-led system.

c) The challenge of continuing delivery

Through the planning application process, the council has already acted to bolster the development industry's lack of delivery by approving additional large developments where there are opportunities to consolidate delivery on land within the existing SDLs and/or on land adjoining where this can be clearly demonstrated as being beneficial to the original vision and development concept, for example at Bell Farm and Keephatch Beech. Continuing this flexible approach will continue to boost housing supply.

d) The number and potential cost of planning application appeals

At the time of writing, the council has received 5 appeals, 7 planning applications and 3 pre-application approaches for proposed developments where the adequacy of the five year land supply has been raised by the applicant/appellant. The cost of 15 public inquiries is estimated to be around £1m on resources.

Options for addressing the challenge

The Local Plan Update is the medium term solution to addressing issues around housing need and land supply. Through this, the council will establish a new housing target, allocate sufficient land to meet this and coordinate supporting infrastructure.

Whilst the Local Plan Update evolves continuing actions to bolster land supply will reduce the risk of unplanned and unsustainable development occurring.

⁷ Appeals at Stanbury House and Park Lane.

Adopted planning policy SAL03 in the Managing Development Delivery plan⁸ allocates six reserve sites for residential development that could be released in the absence of a five year housing land supply (see Appendix A to this report). The challenge in land supply moving forward, coupled with the scale of the demonstrable land supply, is considered sufficient to enable release to be considered in order to establish a buffer large enough to suppress opportunist approaches via the appeal system based on the lottery of partly updating the five year land supply position.

Adopted planning policy CP19 in the Core Strategy⁹ allocates land south of the M4 as a strategic development location (SDL). Development of the area within the SDL known as land south of Cutbush Lane prior to 2026 is constrained by a unilateral s106 agreement. The agreement allows development prior to 2026 in the event that an application is invited by the Council.

These sites are considered to be sustainable, having been subject to scrutiny through the plan process for the Managing Delivery Development consultation and examination process. The reserve sites can be released through a resolution of Executive or a Neighbourhood Plan. Development on land south of Cutbush Lane (see Appendix B to this report) can similarly be invited by a resolution of Executive. Their release would supplement the other actions already being taken and put us in a stronger position to defend decisions at appeal as well as sending messages to those considering opportunist appeals that we have the matter in hand.

FINANCIAL IMPLICATIONS OF THE RECOMMENDATION

The Council faces severe financial challenges over the coming years as a result of the austerity measures implemented by the Government and subsequent reductions to public sector funding. It is estimated that Wokingham Borough Council will be required to make budget reductions in excess of £20m over the next three years and all Executive decisions should be made in this context.

	How much will it Cost/ (Save)	Is there sufficient funding – if not quantify the Shortfall	Revenue or Capital?
Current Financial Year (Year 1)	NA	NA	NA
Next Financial Year (Year 2)	NA	NA	NA
Following Financial Year (Year 3)	NA	NA	NA

Other financial information relevant to the Recommendation/Decision

There are a number of ongoing pre-application, planning application and appeals relating to the reserve sites. The release of the reserve sites would enable the withdrawal of the reason for refusal relating to land supply.

Considering at wider planning activity, at the time of writing, the council has received 5 appeals, 7 planning applications and 3 pre-application approaches for proposed developments where the adequacy of the five year land supply has been raised by the

⁸ Available via <http://www.wokingham.gov.uk/planning/planning-policy/local-plan-and-planning-policies/>

⁹ Available via <http://www.wokingham.gov.uk/planning/planning-policy/local-plan-and-planning-policies/>

applicant/appellant. The cost of 15 public inquiries is estimated to be around £1m on resources.

Cross-Council Implications

Reserve site W1111 Land off Wheatsheaf Close, Sindlesham is being considered for self-build housing. The release of the sites would facilitate an early application for the development of this land and use.

List of Background Papers

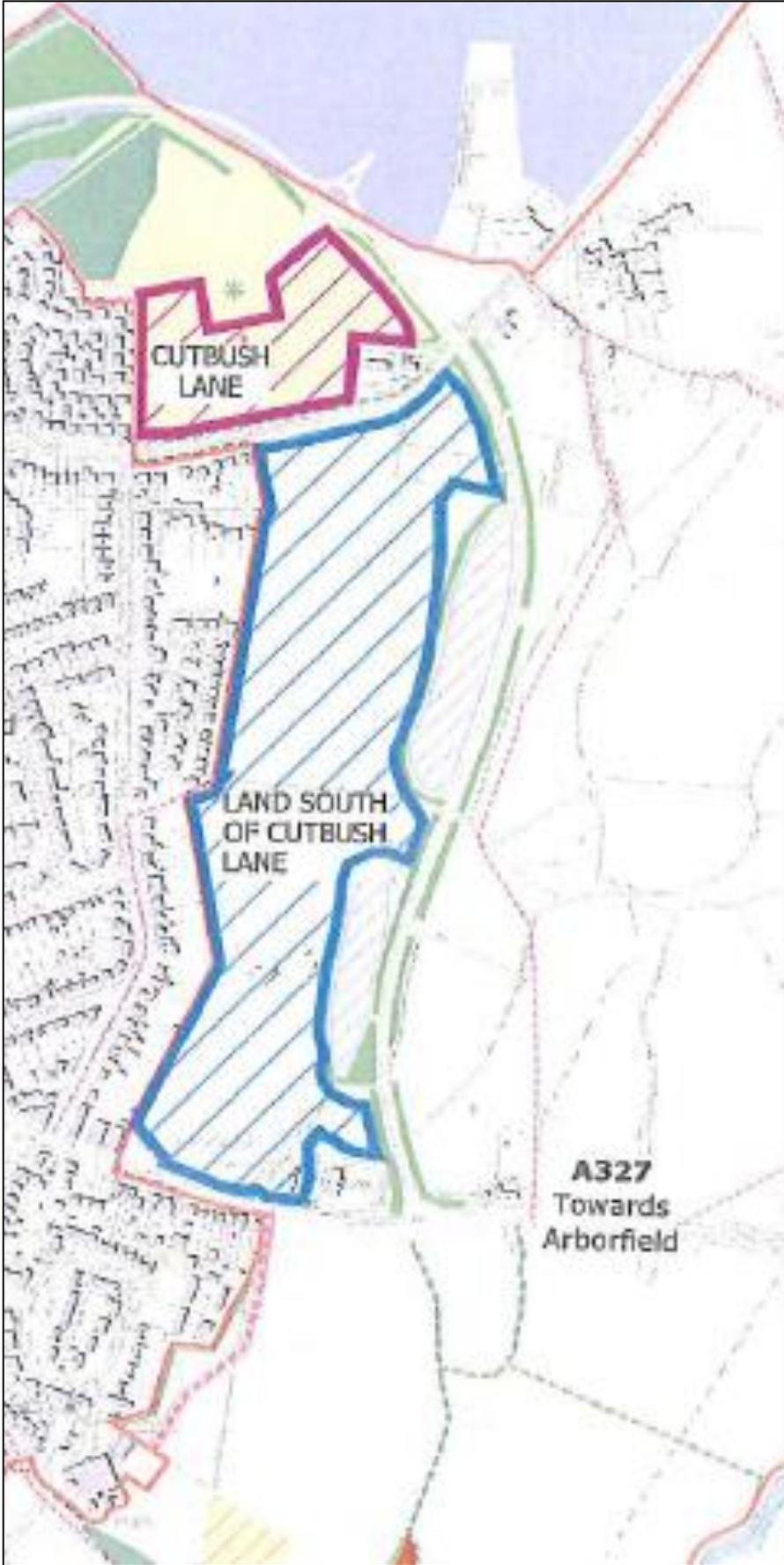
- Core Strategy.
- Managing Development Delivery/
- Strategic Housing Land Availability Assessment 31st March 2016.
- National Planning Policy Framework.

Contact Ian Bellinger	Service Environment
Telephone No 0118974 6231	Email ian.bellinger@wokingham.gov.uk
Date 26 June 2017	Version No. 7

Appendix A: Reserve sites and current activity

Reserve Site (address)	Allocated Capacity (dwellings)	Current activity (comment)
Land to rear of 216b-242a Nine Mile Ride, Finchampstead North (site FI140)	40	No recent pre-app or application.
Land rear of 328-348 Barkham Road, Barkham Hill (site WK122)	25	<p>Land Rear of 336 – 350 Barkham Road</p> <ul style="list-style-type: none"> • Appeal Allowed: 13th April 2014 for 25 dwellings. <p>Land Rear of 328b-336 Barkham Road</p> <ul style="list-style-type: none"> • APPEAL HEARING HELD • Application for 10 dwellings (ref: 160732) refused 4th July 2016. • Public Inquiry completed 13th June 2017.
Land north of The Shires (off Sandy Lane), Barkham Hill (site WK151)	5	Outline applications for either 4 or 5 dwellings (ref: 162150 & 162151) refused 30 th September 2016.
Land at Valley Nurseries, Broadwater Lane, Hurst (site SA104)	16	<p>Full application for 16 dwellings refused 22nd December 2016.</p> <p>Applicant advised appeal to be submitted.</p>
Land off Wheatsheaf Close, Sindlesham (site WI111)	24	Pre-app ongoing to provide self-build development of circa. 24 dwellings.
Land at Sonning Farm (off Glebe Gardens), Sonning (site ref SO101).	25	No recent pre-app or application.
TOTAL	135	

Appendix B: Map showing land south of Cutbush Lane



This page is intentionally left blank

Shinfield Parish Council Comments on South of Cutbush Lane 181499 (Draft)

With regret, Shinfield Parish Council acknowledges it is now planning policy to develop this site as part of the overall South of M4 Strategic Development Location. However, we are very concerned about the large number of reasonable objections which have been raised by local residents and that this application does not take advantage of the links to the 2011 Supplementary Planning Document.

We therefore wish to see the following conditions agreed as a minimum in order for this scheme to be deemed acceptable:

- Developers properly mitigate the transport implications of this scheme;
- A Safer Routes to Schools policy be incorporated;
- A proper Flood Risk Assessment be undertaken and approved the relevant authorities;
- Proper mitigation measures are in place for bats as a protected species.

We make the following specific observations which should be addressed:

1. Does not integrate with footpath 6 the other side of ERR (the current permissive path allows this)
2. the entirety of the development should be designed for a 20mph limit for the outset (D&A p 16)
3. The use of 3 story buildings in 'The Residential Core' is out of keeping with the rest of this side of Shinfield which are all no more than 2 story.
4. The refuse strategy (D&A p26) refers to space for one 240 litre wheelie bin and one 55 litre recycling box, this is inadequate as we don't have wheelie bins in WBC
5. The affordable housing are all located in the same area rather than being spread across the development
6. The use of conservation kerbing should be used on all primary and secondary streets
7. To facilitate the use of the ERR and to discourage vehicles cutting through the village, the application should bring forward the reduction of the speed limit to 30mph for Hollow Lane and Arborfield Road within the boundary of Shinfield Village
8. The accident analysis does not include any data after the ERR was opened, it should be redone (Highways Transport Assessment)
9. Point 3.83 (p21 of Traffic Assessment) is incorrect in that the policeman died as a result of this incident
10. Traffic Assessment - car parking - no car charging points are installed, the expectation would be for one point per dwelling, in line with Government ambitions
11. Traffic Assessment - vehicle trip rates are too low when compared with actual measurements so grossly underestimate the vehicle movements associated with this development
12. The Parrot Farm roundabout will be over-capacity with this development so remedial measures should be implemented to facilitate traffic flow (p 34 of traffic Assessment)
13. The assumption that walk/cycle trips out of the development would use FP7 and then Otlands Road would put pressure on an un-adopted road. The development should provide that a suitable flat path along this route. (p40 of traffic Assessment)

14. A separate cycle/foot path should be provided to the east of the site to allow cycles to avoid the ERR
15. Parking is provided by the use of tandem parking in front of garages which should be avoided
16. The play area is disappointingly small for the number of houses planned
17. Traffic calming measures should be introduced into Cutbush Lane, such as priority islands, to stop vehicles speeding down this straight road.
18. To promote the option of home-working, multiple highspeed telecoms connections to be provided to each dwelling, e.g. fibre to the premises (FTTP) .
19. The noise and traffic assessments do not seem to account for the proposed developments in Aborfield, nor the lorry movements from the potential Cemex site.
20. Standard working hours of Monday to Friday, 8am to 6pm. Saturday, 8am to 1pm should be rigorously applied.
21. A construction transport plan should be implemented to ensure that there is no contractor parking on surrounding residential roads, nor are deliveries parked up on surrounding roads waiting to deliver.
22. The site should be registered with the Considerate Construction Scheme.
23. Conservation kerbs should be used throughout the development to stop parking on pavements.
24. Access to the Eastern Relief Road should be directly from this development not via the bus gate on the existing Bellway development north of Cutbush Lane so creating a through road in the existing development which was never planned.

Christopher Howard

From: James Flynn <james.flynn@shinfieldparish.gov.uk>
Sent: 23 November 2018 16:50
To: Christopher Howard
Cc: Sue Roberts
Subject: Land to the south of Cutbush Lane - Shinfield Parish Council - 181499

Dear Mr Howard

I am writing on behalf of Shinfield Parish Council to submit comments on planning application 181499 (Land to the south of Cutbush Lane, Shinfield). [The proposal](#) is described as “Full planning application for the erection of 249 dwellings, new public open space, landscaping, surface water attenuation, access and associated works at land to the south of Cutbush Lane Shinfield.” I have tried to submit comments on the proposal through the Wokingham Borough Council planning portal, however I am unable to do so from the Shinfield Parish Council login.

Shinfield Parish Council formally objects to the proposal for the following reasons;

- The use of 3 storey buildings in 'The Residential Core' is out of keeping with the otherwise two storey building.
- The priority junction on Cutbush Lane should be changed back to a roundabout to limit the speed of vehicles using the roads for safety.
- Lack of integration of site with footpath 6 the other side of ERR. A suitable, all-weather path with appropriate gates should be provided down and across the ERR.
- Development should be designed for 20mph limit from the outset.
- The use of conservation kerbing should be used on all primary and secondary streets to stop parking on kerbs.
- To facilitate the use of the (Eastern Relief Road) ERR and discourage vehicles cutting through the village, the application should bring forward the reduction of the speed limit to 30mph for Hollow Lane and Aborfield Road within the boundary of Shinfield Village
- The assumption that walk/cycle trips out of the development would use FP7 and then Oatlands Road would put pressure on an un-adopted road. The development should make this footpath up to be in an adoptable condition (p40 of traffic Assessment).
- Traffic calming measures should be introduced into Cutbush Lane, such as priority islands and narrows, to stop vehicles speeding.
- Affordable housing should be spread across the development, not in one place.
- Parking for the apartments is woefully inadequate.
- Construction and site traffic should all use Cutbush Lane rather than cutting through a housing estate, this needs to be conditioned and enforced.
- Highways issues for the Parrot Farm roundabout have not been addressed as the Parrot Farm roundabout will be over-capacity with this development. Remedial measures should be implemented to facilitate traffic flow (p34 of traffic Assessment).
- Site should be registered with the Considerate Construction Scheme
- Detailed construction management plan needs to be submitted to ensure that site and construction traffic is routed correctly.
- Number of advertising banners should be restricted to a maximum of two; they are unsightly and produce unwanted noise, detrimental to the surrounding properties.
- To protect occupants of nearby dwellings from noise and disturbance no works in respect of the construction of the development shall be undertaken outside the hours of **08:00** - 1800 on Mondays to Fridays (inclusive); outside the hours of **0800 - 1300** on Saturdays; and on Sundays and on public holidays, in accordance with Wokingham Borough Core Strategy Policy CP3.

- Adequate precautions, such as wheel washing and road sweeping, shall be taken during the construction period to prevent the deposit of mud and similar debris on adjacent highways and pavements
- The apartment blocks should all have fully integrated reception systems (IRS) into each dwelling to stop the proliferation of satellite dishes and aerials which negatively affect the look of the development
- Loss of the tree would be detrimental to the character of the countryside contrary to CP1, CP3, CP11 of the Core Strategy and CC01, CC02, CC03, TB21 of the MDD Plan
- To protect the environment and the local residents it should be conditioned that no fires be lit on the site for the burning of rubbish, clearing of the site, or any other purpose. All waste and excess materials should be recycled where possible or disposed of through the correct method.
- Each dwelling should have at least one car charging point installed as standard.
- Vehicle trip rates are too low when compared with actual measurements so grossly underestimate the vehicle movements associated with this development.
- The play area is too small for a site of this size.
- Suitable fencing and gates should be provided to the eastern side of the site to ensure that there is no access, especially for children, to the ERR. This is especially important around the southern part of the site near the play area.
- The accident analysis does not include any data after the ERR was opened; it should be redone (Highways Transport Assessment).
- The use of the pumping station should be blocked, and suitable foul water and drainage provision should be provided as there are historical instances of these pumping stations not being maintained correctly.
- All responsibility for the maintenance and upkeep of the attenuation features, green areas, play areas, and open space should be handed over to WBC (or SPC) and not given to maintenance companies who have often charged excessive rates
- Bus route unidentifiable on plan; it does not show where the bus will exit.
- Concerns about construction traffic going through an already occupied estate.

Please can you confirm that you have received this email and confirm that the Parish Council's objection has been recognised.

Yours faithfully

James Flynn

Planning Support Assistant
Shinfield Parish Council

Click [here](#) to report this email as spam.



This drawing is produced for presentation purposes, and should be read in conjunction with drawing 02, which contains supporting layout information.

Housestype	Description	Qty
Private		
B4-18A1	1 Bed Apartment - 182P	2
B4-18A2	1 Bed Apartment - 182P	2
B4-28A1	2 Bed Apartment - 284P	1
B4-28A2	2 Bed Apartment - 283P	1
B4-28A3	2 Bed Apartment - 283P	1
B4-28A4	2 Bed Apartment - 284P	2
B4-28A5	2 Bed Apartment - 284P	2
PO	The Potter - 2 Bed House - 283P	36
TU	The Turner - 3 Bed House - 384P	20
TH	The Thatcher - 3 Bed House - 384P	15
QU	The Quilter - 3 Bed House - 384P	10
MA	The Mason - 3 Bed House - 384P	21
SC	The Scrivener - 4 Bed House - 485P	30
MI	The Milliner - 4 Bed House - 485P	5
BO	The Bowyer - 4 Bed House - 485P	4
PH	The Philosopher - 4 Bed House - 485P	4
WE	The Weaver - 4 Bed House - 485P	6
Private Total		
Affordable		
B1-18F1	1 Bed Apartment - 182P	3
B1-18F2	1 Bed Apartment - 182P	2
B1-28F1	2 Bed Apartment - 284P	3
B2-18F1	1 Bed Apartment - 182P	3
B2-18F2	1 Bed Apartment - 182P	2
B2-28F1	2 Bed Apartment - 284P	3
B3-18F1	1 Bed Apartment - 182P	3
B3-18F2	1 Bed Apartment - 182P	2
B3-18F3	1 Bed Apartment - 182P	2
B3-28F1	2 Bed Apartment - 284P	1
B3-28F2	2 Bed Apartment - 284P	1
B3-28F3	2 Bed Apartment - 284P	2
B3-28F4	2 Bed Apartment - 284P	2
SA	The Saddler - 2 Bed Coachouse - 283P	2
H20 (TV)	2 Bed House - 283P	29
H30 (TV)	3 Bed House - 384P	26
Affordable Total		
Overall Total		
041611-BEL-TV-01-249 UNITS-19.09.18-JH/ LINKED		



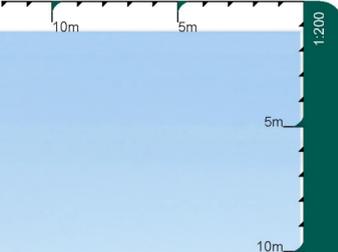
presentation planning layout

This page is intentionally left blank



Copyright © 2018 DHA Architecture Ltd. All rights reserved. Limited reproduction and distribution permitted for the sole purpose of the planning of this named development only.
 Ordnance Survey data © Crown Copyright. All rights reserved. Licence number 100042157.
 Do not scale other than for Local Authority Planning purposes.

83



street scene 07

House Type Weaver
Plot 246

House Type Philosopher
Plot 245

House Type Scrivener
Plot 244

House Type Scrivener
Plot 243

House Type Philosopher
Plot 242

House Type Milliner
Plot 241

House Type Philosopher
Plot 240

House Type Quilter
Plot 239

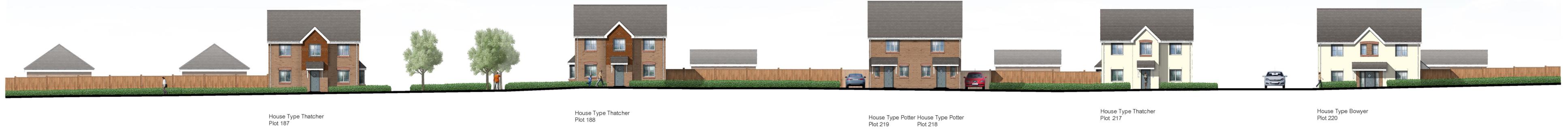
House Type Milliner
Plot 238

House Type Philosopher
Plot 237

House Type Thatcher
Plot 236

NOTE:
 Ground levels and finished floor levels are indicative only and subject to engineers' detailed design.

This page is intentionally left blank



House Type Thatcher
Plot 187

House Type Thatcher
Plot 188

House Type Potter House Type Potter
Plot 219 Plot 218

House Type Thatcher
Plot 217

House Type Bowyer
Plot 220

NOTE:
 Ground levels and finished floor levels are indicative only and subject to engineers' detailed design.

This page is intentionally left blank



House Type Bowyer
Plot 60

House Type Thatcher
Plot 117

House Type Thatcher
Plot 114

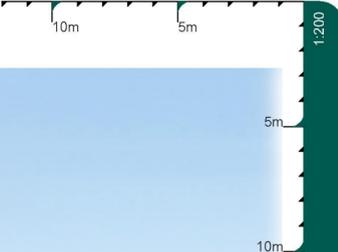
House Type H20
Plot 112 House Type H20
Plot 111

House Type Thatcher
Plot 110

House Type Thatcher
Plot 118

NOTE:
 Ground levels and finished floor levels are indicative only and subject to engineers' detailed design.

This page is intentionally left blank



Block 3
Plots 139-152

House Type Quilter
Plot 138

House Type Quilter
Plot 137

House Type Quilter
Plot 136

House Type Quilter
Plot 135

House Type Potter House Type Potter
Plot 134 Plot 133

House Type Milliner
Plot 132

House Type Milliner
Plot 80

House Type Scrivener
Plot 79

House Type H30
Plot 78

NOTE:
Ground levels and finished floor levels are
indicative only and subject to engineers'
detailed design.

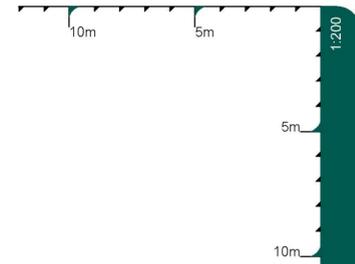


This page is intentionally left blank



House Type Milliner Plot 80 House Type Scrivener Plot 79 House Type H30 Plot 78 House Type H30 Plot 77 House Type H30 Plot 76 House Type H30 Plot 75 House Type H20 Plot 74 House Type H20 Plot 73 House Type H30 Plot 72 House Type H30 Plot 71 House Type Scrivener Plot 70 House Type Bowyer Plot 67

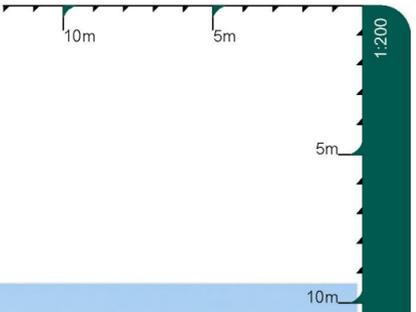
NOTE:
 Ground levels and finished floor levels are indicative only and subject to engineers' detailed design.



street scene 04



This page is intentionally left blank



93

street scene 02

House Type Thatcher
Plot 110

House Type Scrivener
Plot 109

House Type Mason
Plot 108

Block 1
Plot 100-107

House Type H30
Plot 83

House Type H30
Plot 82

House Type H30
Plot 81

House Type Milliner
Plot 80

NOTE:
Ground levels and finished floor levels are indicative only and subject to engineers' detailed design.



dha architecture ltd
 Brooklands Farm Business Park
 Bottle Lane
 Binfield
 Berkshire
 RG42 5QX
 t. 0118 934 9886
 e. surname@dhaarchitecture.co.uk
 w. www.dhaarchitecture.co.uk

land south of cutbush lane shinfield

This page is intentionally left blank

Bellway

Copyright © 2017 DHA Architecture Ltd. All rights reserved. Limited reproduction and distribution permitted for the sole purpose of the planning of this named development only.

Ordnance Survey data © Crown Copyright. All rights reserved. Licence number 100042157.

Do not scale other than for Local Authority Planning purposes.



05

perspective view 04



dha architecture ltd
Brooklands Farm Business Park
Bottle Lane
Binfield
Berkshire
RG42 5QX
t. 0118 934 9666
e. surname@dhaarchitecture.co.uk
w. www.dhaarchitecture.co.uk

land south of cutbush lane, shinfield

Perspective View 04

A 05.10.18 MH Changed to reflect Layout revision A
rev date by details

reference 041611-PER04

24.05.18 created
NTS @ A3E scaling
PII/TP/MH contact
A revision

This page is intentionally left blank

Bellway

Copyright © 2017 DHA Architecture Ltd. All rights reserved. Limited reproduction and distribution permitted for the sole purpose of the planning of this named development only.

Ordnance Survey data © Crown Copyright. All rights reserved. Licence number 100042157.

Do not scale other than for Local Authority Planning purposes.



perspective view 03



dha architecture ltd
Brooklands Farm Business Park
Bottle Lane
Binfield
Berkshire
RG42 5QX
t. 0118 934 9666
e. surname@dhaarchitecture.co.uk
w. www.dhaarchitecture.co.uk

land south of cutbush lane, shinfield

Perspective View 03

24.05.18 created
NTS @ A3E scaling
PII/TP/MH contact

A 05.10.18 MH Changed to reflect Layout revision A
rev date by details

reference 041611-PER03

A revision

This page is intentionally left blank



perspective view 02



This page is intentionally left blank



This page is intentionally left blank

**The Conservation of Habitats and Species Regulations 2017
Screening Assessment and Appropriate Assessment**

In the light of the “Sweetman Judgement” (People Over Wind and Sweetman v Coillte Teoranta, April 2018), the comments below comprise an Appropriate Assessment which includes advice on necessary avoidance and mitigation measures which is consistent with the advice provided to the Planning Inspectorate on such matters.

Summary of Response

WBC, in consultation with Natural England, has formed the view that any net increase in residential development between 400m and 5km straight-line distance from the Thames Basin Heath Special Protection Area (SPA) is likely to have a significant effect on the integrity of the SPA, either alone or in-combination with other plans or projects. An Appropriate Assessment has been carried out which includes regard to mitigation requirements.

This site is located approximately 5.1 km (measured from the access road to the application site) from the boundary of the SPA and therefore is likely to result in an adverse effect on the SPA, unless it is carried out together with appropriate avoidance and mitigation measures.

On commencement of the proposed development, a contribution (calculated on a per-bedroom basis) is to be paid to Wokingham Borough Council (WBC) towards the cost of measures to avoid and mitigate against the effect upon the Thames Basin Heaths SPA, as set out in WBC’s Infrastructure Delivery Contributions Supplementary Planning Document (SPD).

The strategy is for relevant developments to make financial contributions towards the provision of Suitable Alternative Natural Greenspaces (SANGs) in perpetuity as an alternative recreational location to the SPA and financial contributions towards Strategic Access Management and Monitoring (SAMM) measures.

In this instance, the proposed development would result in a net increase of the following no bedroom dwellings

Dwelling (bedrooms)	Type	Number provided on site
1 bed apartment		22
2 bed apartment		19
2 bed house		67
3 bed house		92
4 bed house		49
Total		249

within 5-7km of the SPA. This results in a SANG requirement of 1.03ha of land. In order to support their scheme, the applicant is proposing that surplus capacity at the

Ridge SANG is utilised (approved under planning ref: 0/2010/1432) which is supported by the Biodiversity Officer and Natural England. The delivery of this land will be secured through the S106 agreement pursuant to this application prior to the occupation of any dwelling and will require the entire area of the Ridge SANG to be open. Since the land will be managed by the University of Reading, the Council will expect step in rights for the SANG should this not be managed in accordance with the guidance. These will be detailed in the S106 agreement.

The proposed development is required to make a contribution towards Strategic Access Management and Monitoring (SAMM) which is also calculated on a per bedroom basis. Taking account of the per bedroom contributions this results in a total SAMM contribution of **£40,996.47**.

The total SPA related provision is 1.03ha of SANG in order to meet Natural England Guidance. The applicant **must agree** to enter into a S106/s111 agreement to secure this provision prior to occupation of each dwelling. Subject to the completion of the S106 agreement, the proposal would not lead to an adverse effect on the integrity of the SPA and would comply with SEP Saved Policy NRM6, policy CP8 of the Core Strategy, and the NPPF.

1. The Conservation of Habitats and Species Regulations (2017)

In accordance with The Conservation of Habitats and Species Regulations (2017) Regulation 63 a competent authority (in this case Wokingham Borough Council (WBC)), before deciding to undertake, or give any consent, permission or other authorisation for, a plan or project which—

- a. is likely to have a significant effect on a European site...(either alone or in combination with other plans or projects), and
- b. is not directly connected with or necessary to the management of that site.

must make an appropriate assessment of the implications of the plan or project for that site in view of that site's conservation objectives.

A person applying for any such consent, permission or other authorisation must provide such information as WBC may reasonably require for the purposes of the assessment or to enable it to determine whether an appropriate assessment is required.

WBC must for the purposes of the assessment consult Natural England (NE) and have regard to any representations made by that body. It must also, if it considers it appropriate, take the opinion of the general public, and if it does so, it must take such steps for that purpose as it considers appropriate. In the light of the conclusions of the assessment, and subject to Regulation 64 (Considerations of overriding public interest), WBC may agree to the plan or project only after having ascertained that it will not adversely affect the integrity of the European site.

In considering whether a plan or project will adversely affect the integrity of the site, WBC must have regard to the manner in which it is proposed to be carried out or to any conditions or restrictions subject to which it proposes that the consent, permission or other authorisation should be given.

2. Stage 1 Screening for Likely Significant Effects

WBC accepts that this proposal is a 'plan or project' which is not directly connected with or necessary to the management of a European Site. The Thames Basin Heaths Special Protection Area (SPA) is a European designated site which affects the borough, and WBC must ensure that development does not result in an adverse impact on the SPA. The potential adverse effects on the integrity of the SPA include recreational activities from inside the SPA and air pollution from inside and outside the SPA.

At this stage WBC cannot rule out 'likely significance effects' on the SPA (alone or in combination with other plans or projects) because the proposal could undermine the Conservation Objectives of these sites. This is because the proposal lies within **5.1 km** of the SPA and:

- represents a net increase of 50 dwellings within 5 – 7km of the Thames Basin Heaths Special Protection Area (SPA) which will lead to an increase in local population and a potential increase in recreational activity on the SPA

As the 'likely significance effects' cannot be ruled out at this stage an Appropriate Assessment must be undertaken.

3. Stage 2 Appropriate Assessment

Based on the information proposed by the applicant, WBC must decide whether or not an adverse effect on site integrity (alone or in combination with other plans or projects) can be ruled out. Mitigation may be able to be provided so that the proposal is altered to avoid or reduce impacts.

The following policies and guidance set out WBC's approach to relevant avoidance and mitigation measures which have been agreed with Natural England. For the majority of housing developments this will comprise the provision of (or contribution towards) Suitable Alternative Natural Greenspace (SANG) and a contribution towards the Strategic Access Management and Monitoring (SAMM) Project. The financial contributions towards SANG would be either through an obligation in a s106 agreement that requires WBC to allocate an appropriate amount of the development CIL receipt towards the provision of SANG, or through an obligation in an agreement under s111 of the Local Government Act, that requires the developer to make an appropriate financial contribution towards the provision of SANG (to be used in the event that the developer successfully seeks CIL relief). Developers will be required to secure an appropriate financial contribution to the SAMM project through an obligation in a s106 agreement.

For SDL development (and occasionally some other larger non SDL developments) within 5km of the SPA, SANG is required at a minimum of 8 ha per 1,000 new residents, constructed and delivered to Natural England's quality and quantity standards and a contribution towards pan SPA access management and monitoring (as advised by the Thames Basin Heaths Joint Strategic Partnership Board). For SDL development (and occasionally some other larger non SDL developments) between 5 and 7km, the proposals will need to be individually assessed but it is likely that SANG will be required on site in line with Natural England's quality and

quantity standards, although the exact requirement will be agreed having regard to evidence supplied.

a. Policies and Guidance

For this proposal the following guidance and policies apply:

- South East Plan (May 2009) Policy NRM6 (Thames Basin Heaths Special Protection Area)
<http://webarchive.nationalarchives.gov.uk/20100528160926/http://www.gos.gov.uk/gose/planning/regionalPlanning/815640/>
- Wokingham Borough Core Strategy (2010) Policy CP8 (Thames Basin Heaths Special Protection Area) sets out the approach WBC will take in order to protect the TBH SPA <http://www.wokingham.gov.uk/planning/planning-policy/local-plan-and-planning-policies/>
- Wokingham Borough Core Strategy (2010) Policy CP7 (Biodiversity) sets out the approach WBC will take in order to protect national and international nature conservation sites <http://www.wokingham.gov.uk/planning/planning-policy/local-plan-and-planning-policies/>
- Wokingham Borough Managing Development Delivery Local Plan (2014) Policy TB23 (Biodiversity and Development)
<http://www.wokingham.gov.uk/planning/planning-policy/local-plan-and-planning-policies/>
- Infrastructure Delivery and Contributions SPD (2011)
<http://www.wokingham.gov.uk/EasySiteWeb/GatewayLink.aspx?allId=193415>

The project as proposed would not adversely impact on the integrity of the SPA if avoidance and mitigation measures are provided as stipulated by these policies and guidance.

b. SPA Avoidance and Mitigation Measures

i) The provision of Suitable Alternative Natural Greenspace (SANG) and its ongoing maintenance in perpetuity.

In accordance with the development plan, the proposed development will be required to provide alternative land to attract new residents away from the SPA. The term given to this alternative land is Suitable Alternative Natural Greenspace (SANG).

As this development is located within a SDL, SANG will have to be provided at a minimum of 8 ha per 1,000 new residents for any dwellings within 5km of the TBH SPA, and between 1.73-2.16 ha per 1,000 new residents for any dwellings between 5-7km from the TBH SPA (based upon an individual assessment). The SANG will be constructed and delivered to Natural England's quality and quantity standards and the developer will need to make an appropriate contribution towards in perpetuity maintenance of the SANG. An occupation restriction will be included in the Section 106 Agreement in order to ensure that the SANGs are complete and open to the public prior to the occupation of the dwellings. This gives the certainty required to satisfy the Habitats Regulations in accordance with South East Plan Policy NRM6 (iii) and Core Strategy Policy CP8]

ii. Strategic Access Management and Monitoring (SAMM) Contribution

The proposed development will also be required to make a contribution towards Strategic Access Management and Monitoring (SAMM). This project funds strategic visitor access management measures on the SPA to mitigate the effects of new development on it.

The level of contributions are calculated on a per bedroom basis. The application for this development is for the following number of dwellings

Dwelling (bedrooms)	Type	Number provided on site
1 bed apartment		22
2 bed apartment		19
2 bed house		67
3 bed house		92
4 bed house		49

Depending on the dwelling mix, the level of SAMM payments are calculated as follows:

No. of bedrooms	SAMM Contribution 5-7km	Aggregate SAMM Contribution
1 bedroom	£101.69	£2,237.18
2 bedrooms	£132.93	£11,431.98
3 bedrooms	£174.52	£16,055.84
4 bedrooms	£230.03	£11,271.47
Total SAMM Contribution		£40,996.47

Prior to the permission being granted the applicant must enter into a Section 106 Agreement based upon the above measures.

4. Conclusion

An Appropriate Assessment has been carried out for this development in accordance with the Habitats Regulations 2017. Without any appropriate avoidance and mitigation measures the Appropriate Assessment concludes that the development is likely to have a significant effect upon the integrity of the SPA with the result that WBC would be required to refuse a planning application.

Provided that the applicant is prepared to make secure land (see above) towards the costs of SPA avoidance and mitigation measures, the application will be in accordance with the SPA mitigation requirements as set out in the relevant policies above.

WBC is convinced, following consultation with Natural England, that the above measures will prevent an adverse effect on the integrity of the SPA. Pursuant to

Article 6(3) of the Habitats Directive (Council Directive 92/43/EEC) and Regulation 61(5) of the Conservation of Habitats and Species Regulations (2017), and permission may be granted.

If the applicant does not agree with the above mitigation and does not enter into a Section 106 Agreement to secure the measures, then the application must be refused using the following reason for refusal.

5. Example Reason for Refusal

In the absence of a planning obligation to secure suitable avoidance and mitigation measures and access management monitoring arrangements, in terms that are satisfactory to the Local Planning Authority (LPA), the LPA is unable to satisfy itself that the proposals include adequate mitigation measures to prevent the proposed development from having an adverse effect on the integrity of the Thames Basin Heaths SPA, in line with the requirements of Regulation 63 of the Conservation of Habitats and Species Regulations 2017 and Article 6(3) of Directive 92/43/EEC. The proposal would be contrary to Policy NRM6 of the South East Plan, Policies CP8 and CP4 of the Core Strategy.

Date:	23/04/ 19	Signed:	
--------------	--------------	----------------	---

Agenda Item 90.

Application Number	Expiry Date	Parish	Ward
190198	13/05/2019	Winnersh	Winnersh

Applicant	Wokingham Borough Council C/O WSP
Site Address	Toutley Road / Old Forest Road, Wokingham, Berkshire
Proposal	Full application for the construction of the western section of the Northern Distributor Road linking Toutley Road in the north and the A329 Reading Road in the south west, incorporating a railway bridge, a new junction on Toutley Road / Old Forest Road, and associated works including crossings, drainage, flood risk improvements, shared footway/cycleway, SANG replacement and laying out of new public open space.
Type	Full
PS Category	1
Officer	Sophie Morris
Reason for determination by committee	Major application

FOR CONSIDERATION BY	Planning Committee on 8 th May 2019
REPORT PREPARED BY	Assistant Director – Place Based Services

SUMMARY
<p>The proposal is known as the West of Old Forest Road section of the Northern Distributor Road (NDR) and has been submitted by Wokingham Borough Council for consideration by the Local Planning Authority. The road is identified with the adopted Core Strategy and Supplementary Planning Documents.</p> <p>The application is informed by an EIA Scoping Opinion and, accordingly, is supported by an Environmental Statement (ES).</p> <p>The West of Old Forest Road section of the NDR is an essential piece of infrastructure that is required to mitigate the impact of the effect of additional traffic associated with planned housing growth in North Wokingham. The requirement for a distributor road is identified in the Wokingham Borough Council Core Strategy, Strategic Development Location SPDs, Infrastructure Contributions SPD and subsequent planning permissions. The NDR is an essential part of strategic infrastructure and will comprise a new road around north Wokingham from Coppid Beech / London Road in the east to Reading Road in the west, linking with the Winnersh Relief Road. Sections have been completed through existing developments at Matthewsgreen and Mulberry Gove (Kentwood Farm East) and along Bell Foundry Lane while the other sections are under construction, through Keep Hatch site and Kentwood Farm East. A planning application for the Ashridge Farm section of the NDR has recently been submitted (ref: 191010).</p> <p>The section being considered under this application will link the western end and the entire route is due to be open in 2021.</p> <p>The North Wokingham Highways study undertaken in 2012 identified a distributor road as being the most appropriate highway scheme to ensure the impacts arising from</p>

additional traffic associated with new planned development in North Wokingham would be suitably mitigated. A public consultation process commenced in 2013, where three alternatives to the route west of Twyford Road were presented. A further Refinement Study was subsequently undertaken to examine options within the preferred route alignment. The outcome of the refinement study recommended a single route for the distributor road termed the 'deliverable route option'. This was presented to and approved by WBC's Executive in September 2015 and forms the basis for the current application.

Some environmental preparation works have commenced around the SANG area on Old Forest Road which involve habitat creation, ponds and planting.

The application is submitted in full and includes details of the access, appearance, landscaping, layout and scale. This report is based on an assessment of the application against relevant policies in the Wokingham Borough Council Core Strategy, Managing Development Delivery DPD, other relevant policy and EIA regulations.

PLANNING STATUS

- Electricity Sub Station Consultation Zones
- Mineral Site Consultation Area
- Great Crested Newt Consultation Zone
- 7KM Thames Basin Heath SPA
- Ancient Woodland
- Tree Preservation Orders
- Flood Risk Zones 1,2, and 3 (mainly 1)

RECOMMENDATION

That the committee authorise the GRANT OF PLANNING PERMISSION subject to the following conditions and informatives:

Conditions:

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason: In pursuance of s.91 of the Town and Country Planning Act 1990 (as amended by s.51 of the Planning and Compulsory Purchase Act 2004).

Approved details

2. This permission is in respect of the submitted application plans and drawings numbered in the table below. The development hereby permitted shall be carried out in accordance with the approved details unless otherwise agreed in writing by the local planning authority.

70032502-TGEE-WSP-0100-001-GA General Arrangement Preliminary Design;
70022800-LA-GA-001 Rev P03 – Landscape General Arrangement

Reason: For the avoidance of doubt and to ensure that the development is carried out in accordance with the application form and associated details hereby approved.

Detailed highway design

3. Prior to the commencement of the construction of the road, full details of the construction of roads and footways, including levels, widths, construction materials, depths of construction, surface water drainage, road signage and lighting shall be submitted to and approved in writing by the local planning authority.

The lighting details submitted shall include a "lighting design strategy for biodiversity" for light sensitive species and shall:

- a) Identify those areas/features on site that are particularly sensitive for bats and that are likely to cause disturbance in or around their resting places or along important routes used to access key area of their territory, for example for foraging; and
- b) Show how and where external lighting will be installed so that it can be clearly demonstrated that areas to be lit will not disturb or prevent the above species using their territory or having access to their breeding sites and resting places.

The development shall be carried out in accordance with the approved details and maintained thereafter. Under no circumstances should any other external lighting be installed without prior consent from the local planning authority.

Reason: To ensure that roads and footpaths are constructed to a standard that would be suitable for adoption as publicly maintainable highway, in the interests of providing a functional, accessible, safe and high-quality development and to maintain favourable conservation status of the site for protected species and species of principal importance. Relevant policies: Core Strategy policies CP3, CP6 & CP7.

Visibility splays

4. Prior to the commencement of the construction of the road, details of the proposed accesses and junctions to include visibility splays for all users shall be submitted to and approved in writing by the local planning authority. The accesses shall be formed as so-approved and the visibility splays shall be cleared of any obstruction exceeding 0.6 metres in height prior to the completion of the development. The access shall be retained in accordance with the approved details and used for no other purpose and the land within the visibility splays shall be maintained clear of any visual obstruction exceeding 0.6 metres in height at all times.

Reason: In the interests of highway safety and convenience. Relevant Policy: Core Strategy policy CP6.

Construction Environmental Management Plan

5. Construction work of the development hereby permitted shall not begin until a Construction Environmental Management Plan (CEMP) which will detail the measures to be implemented to protect site workers, site users, ecology and the environment has been submitted to and approved in writing by the local planning authority. The development shall be implemented thereafter in accordance with the approved CEMP. The CEMP shall include the following matters:

- i) a construction travel protocol or Green Travel plan for the construction phase including details of parking and turning for vehicles of site personnel, operatives and visitors;
- ii) Loading and unloading of plant and materials;
- iii) Piling techniques;
- iv) Storage of plant and materials;
- v) Programme of works (including measures for traffic management and operating hours);
- vi) Provision of boundary hoarding and lighting;
- vii) Protection of important trees, hedgerows and other natural features, including a scheme for the provision and management of an 8 metre wide (minimum) buffer zone from the top of the bank of the Emm Brook;
- viii) Relevant ecological mitigation measures for badger, reptiles, breeding birds, and invertebrates, along with integration of any licence requirements for bats and amphibians.
- ix) Control of rats and other vermin particularly during site clearance
- x) Details of proposed means of dust suppression, fume mitigation and noise mitigation. The CEMP shall include a noise management plan that sets out how construction noise will be minimised to protect residential amenity (including methodology for piling and compaction and use of pumps and generators overnight). Particular attention should be given to protecting dwellings that are likely to be exposed to high levels of construction noise as identified in the application noise and vibration report
- xi) Details of measures to prevent mud from vehicles leaving the site during construction;
- xii) Details of any site construction office, compound and ancillary facility buildings. These facilities shall be sited away from woodland areas;
- xiii) Lighting on site during construction;
- xiv) Measures to ensure no on site fires during construction;
- xv) Monitoring and review of the CEMP;
- xvi) Implementation of the CEMP through an environmental management system;
- xvii) Details of the control of surface water run-off, including protection of the Emm Brook, surface water bodies and groundwater from contamination during the construction phase;
- xviii) measures to prevent new contamination from various construction activities such as re-fuelling and operation of welfare facilities
- xix) Details of the excavation of materials and the subsurface construction methodology;
- xx) Details of the haul routes to be used to access the development; and
- xxi) Appointment of a Construction Liaison Officer.

Reason: To protect occupants of nearby dwellings from noise and disturbance during the construction period, in the interest of highway safety and convenience and to minimise the environmental impact of the construction phase in accordance with Wokingham Borough Core Strategy Policies CP1, CP3, CP6 and CP7 and TB23 of the Managing Development Delivery Local Plan Policy.

Levels

- 6. No development shall take place until a measured survey of the site and a plan prepared to scale of not less than 1:500 showing details of existing and proposed

finished ground levels (in relation to a fixed datum point) shall be submitted to and approved in writing by the local planning authority.

Reason: In order to ensure a satisfactory form of development relative to surrounding buildings and landscape. Relevant policy: NPPF and Core Strategy policies CP1 and CP3.

Accommodation of Construction Vehicles and Deliveries

7. No development shall commence until provision has been made to accommodate all site operatives, visitors and construction vehicles loading, off-loading, parking and turning within the site during the construction period, in accordance with details to be submitted to and agreed in writing by the local planning authority. The provision shall be maintained as so-approved and used for no other purposes until completion of the development, or otherwise as provided for in the approved details.

Reason: To prevent queuing and parking off site, in the interests of highway safety and convenience. Relevant policy: Core Strategy Policies CP1, CP3 and CP6.

Hours of operation

8. No work relating to the development hereby approved, including works of ground clearance or preparation prior to commencement of construction operations shall take place other than between the hours of 08:00 and 18:00 hours Monday to Friday and 08:00 to 13:00 hours on Saturdays and at no time on Sundays or Bank or National Holidays. Work outside these hours is permitted in conjunction with works required for the construction of the new railway bridge whenever overnight possession of the railway line is obtained.

Reason: To protect the occupiers of neighbouring properties from noise and disturbance outside the permitted hours during the construction period. Relevant policy: Core Strategy policies CP1 and CP3 and Managing Development Delivery Local Plan policy CC06.

Noise control

9. Prior to the operational use of the road hereby permitted, further monitoring of noise levels shall be undertaken at receptor locations which have been identified within the Noise Assessment as being at risk and details shall be provided to demonstrate how attenuation against externally generated noise would be installed on these dwellings if necessary. The detailed mitigation scheme shall be submitted to and approved in writing by the Local Planning Authority. The measures to be identified in the scheme shall ensure that ambient internal noise levels for the dwellings meet the BS8233/2014 design range 'good' for living accommodation and these mitigation measures shall be implemented wherever possible prior to the opening of the road.

Reason: To protect the amenity of the existing residents and to ensure those dwellings shown to be at risk are protected from noise nuisance and disturbance. Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies, CC03, TB21 & TB24.

Flooding and Drainage

10. No development shall take place until full details of the drainage system for the site have been submitted to and approved in writing by the LPA in consultation with Highways England. The details shall include:

- i) Results of intrusive ground investigation demonstrating seasonal high groundwater levels for the site and infiltration rates in accordance with BRE365;
- ii) Demonstration that the base of SuDS features are at least 1m above seasonal groundwater level;
- iii) Full calculations demonstrating the performance of soakaways or capacity of attenuation features to cater for 1 in 100 year flood event with a 40% allowance for climate change and runoff controlled at Greenfield rates, or preferably better;
- iv) A drainage strategy plan for the proposed development, including swale, basin and outfall details with invert levels and cross sections;
- v) A maintenance management plan for the SuDS features throughout the lifetime of the development, as well as who will be responsible for the maintenance;
- vi) Demonstration that water quality enhancements measures will be included in the scheme to prevent water pollution downstream.

Reason: To prevent increased flood risk from surface water run-off. Relevant policy: NPPF Section 10 (Meeting the Challenge of Climate Change, Flooding and Coastal Change), Core Strategy policy CP1 and Managing Development Delivery Local Plan policies CC09 and CC10.

11. The development permitted by this planning permission shall be carried out in accordance with the Flood Risk Assessment (FRA) 70032502, 32502-FRA-001, January 2019, WSP and Proposed Realignment Model Schematic (70032502, 32502-HMR-103 rev A, 25/09/2018, WSP), and the flood risk mitigation measures detailed within Table 13 of the FRA.

The mitigation measures shall be fully implemented prior to occupation and subsequently in accordance with the timing / phasing arrangements embodied within the scheme, or within any other period as may subsequently be agreed, in writing, by the local planning authority.

Reason: To prevent the increased risk of flooding due to impedance of flood flows and reduction of flood storage capacity. Relevant policy: NPPF Section 10 (Meeting the Challenge of Climate Change, Flooding and Coastal Change), Core Strategy policy CP1 and Managing Development Delivery Local Plan policies CC09 and CC10.

Landscaping

12. Prior to the commencement of the development, full details of both hard and soft landscape proposals shall be submitted to and approved in writing by the local planning authority. These details shall include, as appropriate, proposed site levels or contours, means of enclosure (including fencing suitable to provide mitigation for otters), other vehicle and pedestrian access, hard surfacing materials and minor artefacts and structure (e.g. furniture, signs, lighting, external services, etc). Soft landscaping details shall include planting plan, specification (including cultivation and other operations associated with plant and grass establishment), schedules of plants, noting species, planting sizes and proposed numbers/densities where appropriate, and implementation timetable.

Soft landscaping details shall also include locations for retained or restored wildlife habitat, and quantified ecological compensation measures (demonstrating that the rate is sufficient to replace anticipated losses).

All hard and soft landscape works shall be carried out in accordance with the approved details prior to the occupation of any part of the development or in accordance with a timetable approved in writing by the local planning authority. Any trees or plants which, within a period of five years after planting, are removed, die or become seriously damaged or defective, shall be replaced in the next planting season with others of species, size and number as originally approved and permanently retained.

Reason: In the interests of visual amenity. Relevant policy: Core Strategy policy CP3, CP7 and Managing Development Delivery Local Plan policies CC03 and TB21

Landscape Management Plan

13. Prior to the commencement of the development a landscape management plan, detailing long term design objectives, management responsibilities, timescales and maintenance schedules, including for all ongoing ecological mitigation and compensation measures, for all landscape areas, shall be submitted to and approved in writing by the local planning authority. The landscape management plan shall be carried out as approved.

Reason: In order to ensure that provision is made to allow satisfactory maintenance of the landscaping hereby approved. Relevant policy: Core Strategy policy CP3, CP7 and Managing Development Delivery Local Plan policies CC03 and TB21

Landscape and Ecological Management Plan (SANG)

14. A detailed landscape and ecological management plan (LEMP) for all Old Forest Road SANG, the Old Forest Road SANG extension, and the remaining parts of Millennium Arboretum shall be submitted to and approved in writing by the local planning authority prior to the SANG extension being brought in to use. The content of the LEMP shall include the following:
- a) The phasing of any opening strategy showing no net loss of SANG at any time,
 - b) Description and evaluation of features to be managed,
 - c) Ecological trends and constraints including mitigation measures applied on the site that might influence management,
 - d) Aims and objectives of management,
 - e) Appropriate management options for achieving aims and objectives,
 - f) Prescriptions for management actions,
 - g) Preparation of a work schedule (including an annual work plan capable of being rolled forward over a five-year period),
 - h) Ongoing monitoring and remedial measures.

The approved plan will be implemented in accordance with the approved details.

Reason: To ensure that there is adequate SANG provision in order to mitigate against the impact of the development on the SPA in accordance with Core Strategy Policies CP8 and retained policy of the South East Plan NRM6

Retention of trees and shrubs

15. No trees, shrubs or hedges within the site which are shown as being retained on the approved plans shall be felled, uprooted wilfully damaged or destroyed, cut back in any way or removed without previous written consent of the local planning authority; any trees, shrubs or hedges removed without consent or dying or being severely damaged or becoming seriously diseased within 5 years from the completion of the development hereby permitted shall be replaced with trees, shrubs or hedge plants of similar size and species unless the local planning authority gives written consent to any variation.

Reason: To secure the protection throughout the time that development is being carried out, of trees, shrubs and hedges growing within the site which are of amenity value to the area. Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21.

Protection of trees

16. a) No development or other operation shall commence on site until a scheme which provides for the retention and protection of trees, shrubs and hedges growing on or adjacent the site in accordance with BS5837: 2012 has been submitted to and approved in writing by the local planning authority (the Approved Scheme); the tree protection measures approved shall be implemented in complete accordance with the Approved Scheme for the duration of the development (including, unless otherwise provided by the Approved Scheme) demolition, all site preparation work, tree felling, tree pruning, demolition works, soil moving, temporary access construction and or widening or any other operation involving use of motorised vehicles or construction machinery.

b) No development (including any tree felling, tree pruning, demolition works, soil moving, temporary access construction and or widening or any other operation involving use of motorised vehicles or construction machinery) shall commence until the local planning authority has been provided (by way of a written notice) with a period of no less than 7 working days to inspect the implementation of the measures identified in the Approved Scheme on-site.

c) No excavations for services, storage of materials or machinery, parking of vehicles, deposit or excavation of soil or rubble, lighting of fires or disposal of liquids shall take place within an area designated as being fenced off or otherwise protected in the Approved Scheme.

d) The fencing or other works which are part of the Approved Scheme shall not be moved or removed, temporarily or otherwise, until all works including external works have been completed and all equipment, machinery and surplus materials removed from the site, unless the prior approval of the local planning authority has first been sought and obtained.

Reason: To secure the protection throughout the time that the development is being carried out of trees shrubs or hedges growing within or adjacent to the site

which are of amenity value to the area, and to allow for verification by the local planning authority that the necessary measures are in place before development and other works commence Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21

Herptile Mitigation Implementation

17. The proposed mitigation measures for the protection of Great Crested Newts and reptiles affected by the development hereby permitted shall be carried out in accordance with the submitted Herptile Mitigation Strategy, ref: 70022800, dated December 2018.

Reason: To ensure that species of principal importance, protected under the Wildlife and Countryside Act 1981 (as amended), are not adversely affected by the development, in accordance with CP7.

Ecological Mitigation Measures

18. The development hereby approved shall be carried out in accordance with the proposed ecological mitigation measures as stated within Section 8.7 of the submitted Environmental Statement.

Reason: To ensure that species of principal importance, protected under the Wildlife and Countryside Act 1981 (as amended), are not adversely affected by the development, in accordance with CP7.

Archaeology

19. No development shall take place until the applicant or their agents or successors in title have secured the implementation of a programme of archaeological mitigation in accordance with a written scheme of investigation, which has been submitted by the applicant and approved in writing by the Local Planning Authority. The mitigation strategy will provide for:

i) A programme of site investigation and recording

ii) A programme of post investigation assessment, analysis, publication, dissemination and archiving. This part of the condition shall not be discharged until these elements of the programme have been fulfilled in accordance with the programme set out in the WSI

Reason: The site lies in an area of archaeological potential as identified by archaeological evaluation. Further work is required to ensure that any archaeological remains within the site are adequately investigated and recorded or preserved in situ in the interest of protecting the archaeological heritage of the borough.

Contamination

20. (a) If contamination is found at any time during site clearance, groundwork and construction the discovery shall be reported as soon as possible to the local planning authority. A full contamination risk assessment shall be carried out and if found to be necessary, a 'remediation method statement' shall be submitted to the local planning authority for written approval. Should no evidence of contamination be found during the development a statement to that effect shall be submitted to the local planning authority

(b) Works shall be carried out in accordance with the approved 'remediation method statement' (submitted to comply with condition 20) and a final validation report shall be submitted to the local planning authority upon completion of remedial works.

Reason: To ensure that any contamination of the site is identified at the outset to allow remediation to protect amenity of the site and adjacent land. Relevant policy: NPPF Section 11 (Conserving and Enhancing the Natural Environment) and Core Strategy policies CP1 & CP3.

Informatives:

1. This development may require an Environmental Permit from the Environment Agency under the terms of the Environmental Permitting (England and Wales) (Amendment) (No. 2) Regulations 2016 for any proposed works or structures, in, under, over or within 8 metres of the top of the bank of designated 'main rivers'. This was formerly called a Flood Defence Consent. Some activities are also now excluded or exempt. An environmental permit is in addition to and a separate process from obtaining planning permission. Further details and guidance are available on the GOV.UK website: <https://www.gov.uk/guidance/flood-risk-activities-environmental-permits#>
2. The applicant is advised to apply to Environmental Health for 'prior consent' under s.61 of the Control of Pollution Act 1974 in relation to the control of construction site noise
3. Adequate precautions shall be taken during the construction period to prevent the deposit of mud and similar debris on adjacent highways. For further information contact the Highway Authority on tel.: 0118 9746000
4. Any works/ events carried out by or on behalf of the developer affecting either a public highway or a prospectively maintainable highway (as defined under s.87 New Roads and Street Works Act 1991 (NRSWA)), shall be co-ordinated and licensed as required under NRSWA and the Traffic Management Act 2004 in order to minimise disruption to both pedestrian and vehicular users of the highway.

Any such works or events, and particularly those involving the connection of any utility to the site must be co-ordinated by the developer in liaison with the Borough's Street Works team (0118 974 6302). This must take place AT LEAST three months in advance of the intended works to ensure effective co-ordination with other works so as to minimise disruption.

5. The applicant's attention is drawn to the suggestion made by The Wokingham Society regarding the installation of a piece of art being located in the newly created SANG open space. The applicant is encouraged to consider this suggestion in consultation with The Wokingham Society, the Town Council, and other key local stakeholders.

6. The applicant is advised to carry out a further noise assessment and implement any mitigation measures as may be required under the Noise Insulation Regulations.

RELEVANT PLANNING HISTORY

Application Number	Proposal	Decision
173063	Application for a scoping opinion to determine the content of an Environmental Impact Assessment for a section of the proposed North Wokingham Distributor Road (NWDR) between the A329 Reading Road and West of Old Forest Road and Toutley Road.	Replied 20/12/2017
182854	Application for a certificate of lawfulness for the proposed widening/realignment and associated improvements to a section of Old Forest Road including across Toutley Bridge.	Approved 05/12/2018
183119	Application for a certificate of lawfulness for the proposed widening/realignment and associated improvements to the carriageway.	Approved 05/12/2018

SUMMARY INFORMATION

Site Area:	31ha
Previous land use:	Agriculture, open space, allotments.
Proposed land use:	Public highway, including vehicle carriageway, cycleway and footway, public open space, SANG.

CONSULTATION RESPONSES

Ramblers Association	No comments received
South East Water	No comments received
Berkshire Archaeology	No objection subject to condition requiring a programme of archaeological mitigation in accordance with a written scheme of investigation, prior to commencement
Environment Agency	No objection subject to conditions
Natural England	No objection subject to conditions securing SANG details and management
Historic England	No objection
Thames Water	No comments received
Network Rail	No comments received however Council is working closely with NR to deliver the rail bridge.
National Grid	No comments received

SSE Power Distribution	No comments received
Southern Gas Networks	No comments received
Highways England	No objection subject to condition relating to drainage details
Forestry Commission	Standard advice relating to ancient woodlands
Open Spaces Society	No comments received
South Western Railway	No comments received
Berks, Bucks and Oxon Wildlife Trust	No comments received
WBC Biodiversity	The impact in terms of ecological habitats and protected species is acceptable subject to conditions to secure implementation of various measures as specified. Various conditions recommended.
WBC Green Infrastructure Officer	No objection. Further details to be considered through conditions.
WBC Conservation Officer	No comments received
WBC Drainage	No objection. Recommends condition requiring full details of the site drainage system prior to commencement.
WBC Environmental Health	No objection. Recommends conditions relating to a further noise impact assessment, and the reporting of unexpected contamination
WBC Highways	No objection. Recommends various conditions.
WBC Tree & Landscape	No objection subject to conditions relating to retention/protection of existing trees, a landscaping scheme and landscape management plan.
WBC Public Rights of Way	No comments received

REPRESENTATIONS

Town/Parish Council:

Wokingham Town Council – No comments received

Winnersh Parish Council:

- Unable to follow the routing of individual vehicles on tracking plan; *Officer note – individual tracking plans were subsequently sent to the Parish Council and no further comments have been received*
- Consideration should be given to a raised table crossing for all junctions set back from the main carriageway by 5m – this would make the junction safer by reducing the speed of people driving and cycling at these junctions; *Officer note – this point is noted and will be considered at the detailed design stage. Matters of detailed design will be required to be submitted under condition 3.*
- There appears to be no dropped crossing facilities for pedestrians/cyclists at the new junction with Old Forest Road; *Officer note – dropped kerbs at all junctions to be used by pedestrians/cyclists will be secured at detailed design stage*

Local Members: No comments received

Emmbrook Residents Association:

- Road Traffic Noise – reference to section 10.9.17 of the ES which states that 31 dwellings could be eligible for noise insulation. A condition should be imposed relating to owner/occupiers of the dwellings being offered a noise insulation survey. *Officer note – See paras 52-57 of report / Condition 9 refers*
- Entrance to Toutley Business Estates: Proposed access results in convoluted entrance with hairpin bend. *Officer note – the original design has been amended in response to these comments to create an improved access arrangement into the Toutley Industrial Estate and metro centre businesses. No further comments received on amendments.*
- Retention of Old Forest Road Footway
Plans not clear what will happen to redundant section of OFR - should be retained for ped/cyclists. *Officer note – this comment is noted and details will be expected to be clarified as part of the detailed landscaping condition submission (to be submitted under condition 12)*
- Addition of public open space is welcomed – but some thought should be given to the layout of the paths. Good features of Bell Farm such as number of benches and notice boards are recommended to be provided. *Officer note – detailed landscaping will be secured by condition 12 which would require details such as benches and notice boards to be submitted.*

The Wokingham Society:

Comment: The Wokingham Society would like to urge that provision for one or more pieces of public art to be placed in the SANG adjacent to the NWDR. This has occurred at Buckhurst Meadow and has received widespread appreciation. *Officer note – whilst this would not be a requirement to render the application acceptable, the suggestion has been added as informative 5.*

Neighbours:

Consultation letters regarding the proposal were sent to over 2000 properties.

One response in support of the scheme has been received.

Nine other responses have been received, some of which raised concerns relating solely to the original proposed junction with Old Forest Road and the access into the metro centre. Such comments are summarised as follows:

- The application is unacceptable as the estate needs constant access for articulated lorries and HGV's
- Concern regarding the new junction at Toutley/Old Forest Road – articulated lorries will not be able to use the entry/egress due to inadequate swept paths; existing access should be maintained directly off Toutley Road
- HGV's will not be able to get into Toutley Road from Old Forest Road
- Layout with the metro centre won't work - large transporter vehicles will need to mount pavement.

Officer comment – further to the above comments regarding proposed access arrangements into the metro centre, a revised layout has been submitted in response to these and occupiers of the metro centre were consulted.

One response was received as a result, commenting that the proposed pedestrian island should be moved to a straight part of the road as in the current position it is likely to be hit by HGV's lining up to turn left into the Toutley Industrial Estate. However, the swept paths plans and Stage 1 Road Safety Audit undertaken for the new design has not raised this as an issue, but will be further considered through the detailed design/RSA process.

No other objections to the revised design have been received.

Other comments made relating to the application are summarised as follows:-

- Extra traffic on the NDR may result in the need for sound barriers and assurances should be provided that all necessary sound barriers for the NDR and M4 are included in the proposal. Concern over pollution levels as a result of proposal. *Officer note – paras 47 to 62 of below report deal with issues concerning noise and pollution.*
- Concern over construction access opposite Ashton Rd – suggests a condition of construction is for a banksman to be required to manage traffic. *Officer note – the Construction and Environmental Management Plan required to be submitted under condition 5 will need to demonstrate safe access/egress into the construction compound.*
- Unhappy that Old Forest Road will still provide a parallel path to the new road. *Officer note – the intention has always been to retain the remainder of Old Forest Road given there are a number of further accesses serving residential properties.*
- Provision of footpath under the new railway bridge – consideration should be given to extending this along the railway line under the M4 which would allow existing and new residents of Emmbrook easier access to the Winnersh area. *Officer note – this would be beyond the scope this application and on land outside the application site and Council's control.*
- Please could consideration be given to working with Highways England and Kier For cost sharing of a non-reflective sound barrier or bund along the raised sections of the NDR. This could reduce noise pollution. *Officer note – see paras 52-57/Condition 9 regarding noise associated with the scheme.*
- Significant work has already begun on site although planning permission not yet given. *Officer note – enabling works relating to habitat creation have been permitted to commence on site, prior to planning permission being obtained for the road scheme as these works are considered to be of a minor nature which are considered to fall within the remit of permitted agricultural works. These works also represent public and environmental benefit.*
- Red line boundary of the application site cuts through a back garden. *Officer note – for ease of reference, the red line boundary for the application followed a line shown to be the existing rear garden boundary lines of properties on Old Forest*

Road. No development is proposed as part of the application within the area indicated by the resident as being land within their back garden.

PLANNING POLICY		
National Policy	NPPF	National Planning Policy Framework
Adopted Core Strategy DPD 2010	CP1	Sustainable Development
	CP3	General Principles for Development
	CP4	Infrastructure Requirements
	CP6	Managing Travel Demand
	CP7	Biodiversity
	CP8	Thames Basin Heaths Special Protection Area
	CP9	Scale and Location of Development Proposals
	CP10	Improvements to the Strategic Transport Network
	CP11	Proposals outside development limits (including countryside)
	CP17	Housing Delivery
	CP20	North Wokingham Strategic Delivery Location
Adopted Managing Development Delivery Local Plan 2014	CC01	Presumption in Favour of Sustainable Development
	CC02	Development Limits
	CC03	Green Infrastructure, Trees and Landscaping
	CC06	Noise
	CC09	Development and Flood Risk (from all sources)
	CC10	Sustainable Drainage
	TB21	Landscape Character
	TB23	Biodiversity and Development
	TB24	Designated Heritage Assets
	TB25	Archaeology
	TB26	Buildings of Traditional Local Character and Areas of Special Character
Infrastructure Delivery and Contributions SPD		
North Wokingham Strategic Development Location SPD		

PLANNING ISSUES

Application site and surroundings:

1. The application site is situated to the north west of Wokingham, and is bound by the M4 corridor along the north west boundary, and Old Forest Road to the south and east, part of which falls within the application site. Reading Road lies to the south west of the site, the North Wokingham Strategic Development Location boundary lies adjacent to the north east of the application site. The metro centre and Toutley industrial estate are also situated beyond the north east boundary of the site.
2. The majority of application site itself is located on undeveloped land located in the countryside, and is bisected by the north downs (Wokingham / Reading) railway line located at the southern end of the site, and the Emm Brook which is located at the northern end. The site also contains the Berkshire Arboretum Woodland (Millennium Arboretum) which is an amenity area comprising grassland, scattered trees and small parcels of plantation woodland (including an Ancient woodland), along with ponds and a wet ditch which joins the Emm Brook to the north.
3. The eastern end of the site incorporates the western area of the Old Forest Road SANG on land to the west of the Emm Brook. Access to this open area of meadow grassland, hedges, and scrub is by an informal network of footpaths with footbridges crossing the Emm Brook. A further small portion of the site to the east is bordered by Toutley Road on the easterly edge.
4. The western-most section of the site, currently allotment gardens, is bounded by Reading Road to the west, M4 to the north and separated from the Millennium Arboretum by the railway line.
5. The topography of the site is reasonably flat, although is slightly higher in the west, and slopes downwards to the east towards the Emm Brook. The existing ground level at the western end is 54.4m Above Ordnance Datum (AOD), then gradually slopes to its lowest point of 47.2m AOD. The topography then rises slightly to reach 49.5m AOD at the eastern end of the site.

Principle of Development:

6. Wokingham Borough Core Strategy policy CP17 establishes a requirement to provide at least 13,487 new dwellings with associated development and infrastructure in the period 2006-2026. The majority of this new residential development will be in four Strategic Development Locations (SDLs), one of which is in North Wokingham.
7. Core Strategy Policy CP20 is amplified by Appendix 7 of the Core Strategy, the North Wokingham Strategic Development Location Supplementary Planning Document (SPD) and Infrastructure Delivery and Contributions SPD, which address the associated infrastructure impacts across the borough. These documents establish the requirement for a mixed-used development and make clear that a co-ordinated approach to the development of the SDL will be required to deliver the necessary infrastructure to meet the needs of the expanded community.
8. Policy CP20 seeks improvements to transport capacity along the A321 and A329 and the provision of the Northern Distributor Road (NDR). The proposed NDR

route, as outlined in WBC's adopted SPD Masterplan, will run from the A329 (near the M4 over-bridge) Forest Road in the west, through the SDL development and eventually to Coppid Beech Roundabout, where a park and ride facility is proposed.

9. The North Wokingham Highways study undertaken in 2012 identified a distributor road as being the most appropriate highway scheme to ensure the impacts arising from additional traffic associated with new planned development in North Wokingham would be suitably mitigated. A public consultation process commenced in 2013, where three alternatives to the route west of Twyford Road were presented. A further Refinement Study was subsequently undertaken to examine options within the preferred route alignment. The outcome of the refinement study recommended a single route for the distributor road termed the 'deliverable route option'. This was presented to and approved by WBC's Executive in September 2015 and forms the basis for the current application.
10. The NDR is key strategic road infrastructure considered necessary under the Core Strategy and will link with Winnersh Relief Road and forms part of the wider strategic infrastructure improvements being constructed such as the Coppid Beech roundabout, Station Link Road, Southern Distributor Road, Arborfield Relief Road, Nine Mile Ride Extension and Eastern Relief Road. The NDR is designed to be an attractive route through the new residential areas of North Wokingham, operating a 30/40mph speed limit. The NDR will take the majority of the new traffic in addition to existing development traffic. This part of the route will provide a key section of the Council's strategic NDR which as a whole is identified as a necessary piece of infrastructure required to facilitate the delivery of housing numbers identified for the North Wokingham SDL. The NDR delivery is therefore part of the Council's strategic response to providing the borough's identified housing needs and associated infrastructure necessary to accommodate the new development. The proposal is therefore considered acceptable in principle.
11. The application is informed by an EIA Scoping Opinion and, accordingly, is supported by an Environmental Statement (ES).

Description of Development:

12. This application comprises the section of the NDR which will connect with the end of the recently approved phase 2 Winnersh Relief Road scheme (ref: 180760) via a new roundabout on the A329 Reading Road, which was approved as part of that scheme. The road will link the Winnersh relief road with the existing section of the NDR which runs across the Matthewsgreen development, across land which is currently predominantly in agricultural use, but with some areas of public open space and will follow in a general south west to north east direction. The alignment would be a single carriageway, approximately 1.4km in length and approximately 7.3m wide. In addition to the proposed highway, the scheme includes provision for pedestrians and cyclists, by way of a shared use 3m wide pathway alongside each side of the carriageway and a 2m green verge along all the areas where these are achievable. The NDR extends around north Wokingham from Coppid Beech / London Road in the east to Reading Road in the west, linking with the Winnersh Relief Road. Sections have been completed through existing developments at Matthewsgreen and Mulberry Gove (Kentwood

Farm East) and along Bell Foundry Lane while the other sections are under construction, through Keep Hatch site and Kentwood Farm East. This section will link the western end and the entire route is due to be open in 2021. A planning application for the Ashridge Farm section of the NDR has recently been submitted under application reference 191010.

13. In association with the construction of the road, a large public open space will be provided to connect with the existing SANG which was approved in conjunction with the approved Matthewsgreen development site. A small section of the existing SANG would be removed due to the point at which the new road alignment would connect in with the northern part of Old Forest Road. However, a much larger area of additional open space (approximately 7ha) is being provided as mitigation for the loss of the small part of the SANG.
14. At the south western end of the site, the new section of the NDR will commence on land which is currently occupied by the Winnersh Parish Council allotments. However, planning permission has been granted for new allotments (ref: 181662) which will be located locally on a site to the north of the M4. Works are nearing completion on the approved allotments site and associated facilities. The road will then rise to continue over the north downs railway line, and onwards through the existing millennium arboretum and beyond that across existing agricultural land before tying in with Old Forest Road. Prior to the alignment joining Old Forest Road, a small spur road will link the southern section of Old Forest Road with the new road. This would provide access to residential dwellings located off the southern section of Old Forest Road.
15. Once the alignment connects in with Old Forest Road, it will follow the Old Forest Road existing alignment to cross over the Emm Brook, retaining the existing bridge before then continuing around to join with Toutley Road, where it will generally follow the existing alignment of Toutley Road to the east, before tying in with the existing Queen's Road section on the Matthewsgreen development site. Two sections of the road will incorporate improvement works being undertaken to a small section of the existing Old Forest Road and Toutley Road. Such works are permitted development, and lawful development certificates have been approved for these parts respectively (refs: 182854 & 183119). However, as these areas will be incorporated as part of this section of the NDR, they are also included as part of the red line boundary for the purposes of this application.
16. At the point where Toutley Road currently meets with Old Forest Road, a new junction arrangement would be provided and the northern end of Old Forest Road would be accessed via a junction off the NDR, whereby the priority would be for traffic travelling along the NDR. The scheme has been amended since initial submission to provide a filter lane for direct access off the NDR into the Metro Centre as the original submission provided access to the Metro Centre via the new junction onto Old Forest Road. However, that proposed arrangement received a number of objections from businesses located within the Metro Centre, who considered that it would not be acceptable for the types of vehicles i.e. HGV's needing to access and egress from the Metro Centre.
17. In response and following discussions with the occupiers, an amended design was submitted for this section of the scheme, which now proposes a separate filter lane off the NDR to facilitate maintaining direct access into the Metro

Centre. The occupants of the Metro Centre were consulted on the proposed amendment, and no objections have been received. One comment was received from a representative of the metro centre management company which states that the positioning of the pedestrian island would be likely to be hit by HGVs lining up to turn left into the metro centre. However, the swept paths and Stage 1 Road Safety Audit relating to the amended design has not picked this up as an issue, but will in any event be further considered at the detailed design stage and through the further stages of the road safety audit process.

Access and Movement:

18. The planning application is supported by a Transport Assessment. The document has fully assessed the nature and extent of transport impacts together with any cumulative impact associated with known future development.
19. The assessment draws upon modelling data derived from the Wokingham Strategic Transport Model 3 and takes into account the SDL developments together with on-site infrastructure provision and known off-site transport interventions. The base year is 2010 and the model was developed to represent average weekday AM peak hour (08:00 - 09:00), Inter peak hour (average 10:00 – 16:00) and PM peak hour (17:00 - 18:00).
20. The forecast years used for this development were 2019 and 2026. It is accepted practice to carry out an assessment 15 years after the opening year, however it was agreed to assess 2026 as being in line with the end of the current Local Plan period.
21. The following existing junction was assessed using the standard junction modelling tools:
 - Old Forest Road/Toutley Road
22. This junction was assessed for base year (2015), 2019 and design year (2026). As part of this scheme this junction would be improved including the change of priority. The modelling has predicted that the flows along Old Forest Road would reduce by between around 300 vehicles in AM peak hour and by around 600 vehicles in the PM peak hour by 2026. This reduction in traffic would lead to improvements in noise and air pollution and improving the environment for pedestrians and cyclists.

Speed, lighting and highway safety

23. The design speed proposed along the first section of the distributor road commencing at the south western end would be 40mph until a point approximately 90m west of the proposed new link road to Old Forest Road. After this point, the speed limit would reduce for the remainder of its length to 30mph, which is reflective of the transition from a more rural to residential area. The road geometry is considered acceptable for the design speeds proposed.
24. Lighting is proposed along the route, the details and specifications for which would be required to be submitted by recommended condition 3. Such details would also need to take into account any ecological sensitivity considerations.

25. High reflective road markings, high quality road studs and illuminated signs and bollards are proposed along the alignment, the details for which would be required to be submitted for approval under condition 3. There would be two uncontrolled pedestrian crossings along the route, together with a signalised pedestrian crossing to facilitate access between Embrook Meadow and the existing SANG.

26. The Transport Assessment is supported by an independently assessed Stage 1 Road Safety Audit and did not identify any significant issues. Future stages in the Road Safety Audit process would be carried out at later stages.

27. Therefore, the road alignment, layout and design is considered to be acceptable.

Public Rights of Way:

28. There are no Public Rights of Way through the site which would be affected as a result of the proposal and neither the Ramblers' Association nor the PROW officer have commented on the application.

Trees and Landscape:

29. Core Strategy Policies CP1 and CP3 require a high quality design that respects its context. This requirement is amplified by MDDL Policies CC03 and TB21 which require development proposals to protect and enhance the Borough's Green Infrastructure, retaining existing trees, hedges and other landscape features wherever possible and incorporating high quality - ideally native – planting as an integral part of any scheme, within the context of the Council's Landscape Character Assessment. Policy CC02 states that planning permission for proposals at the edge of settlements will only be granted where they can demonstrate that the development, including boundary treatments respects the transition between the built-up area and the open countryside by taking account of the character of the adjacent countryside and landscape'.

30. It is inevitable that a new road on this scale will result in some harm to the existing landscape. A Landscape and Visual Impact Assessment (LVIA) forms part of the application submission and has assessed the visual impact of the development from key visual receptors and from a variety of different representative viewpoints. The impact is also assessed in terms of the initial construction and once the development is complete and operational.

31. The development will result in some moderate to minor adverse visual effects during both the construction and operational phase from a number of key sensitive visual receptors, including residential properties. The Environmental Statement identifies a significant effect upon viewpoints within the millennium arboretum, the visual receptors being recreational users of the arboretum. Such effects will arise as a result of the arboretum being severed by the road, the point at which embankments will be required to enable the road to pass over the railway line.

32. Part of the arboretum is an Ancient Woodland area, however, the road alignment in relation to this part of the woodland area would be located approximately 40m from the edge of the Ancient Woodland, which is well beyond the 15m required

buffer zone as advised by the Forestry Commission and Natural England. Both the WBC Ecology and Trees and Landscaping Officers have considered the implications of the proposed development on the ancient woodland within the red line and are satisfied that the alignment of the road provides a suitable buffer to the existing ancient woodland boundary. They also approve of the proposed approach taken to provide additional woodland planting to provide a more substantial woodland unit in the long run.

33. As previously mentioned, the proposed alignment of the scheme requires a section of road and associated infrastructure to run through part of the existing Old Forest Meadows SANG. As this SANG was created as a necessary mitigation measure for the Thames Basin Heaths Special Protection Area (TBH SPA), it is essential that there is no net loss of area and that there is no deterioration of the quality that makes the site an attractive alternative to visiting the TBH SPA.
34. To address this, the proposal is to convert agricultural fields adjacent to Old Forest Meadows SANG into a SANG extension. This would provide an additional SANG area of approximately 7ha in addition to the existing 2.59ha, which is considerably more than will be lost to the road and associated infrastructure and as such, the proposal would not result in a net loss of SANG area.
35. Once set out, the newly created SANG area will be handed to WBC Countryside Services department to manage in perpetuity as they are the current land managers of Old Forest Road Meadows. In order to secure the ongoing maintenance and management of the extended SANG area, a Landscape and Ecological Management Plan (LEMP) will be required to show how the site will be managed as a SANG. This would be secured through condition 14.
36. In addition, a significant programme of landscape mitigation measures are proposed in conjunction with the construction of the road, summarised as follows:-
 - New wildflower and meadow grassland to replace parts of the former agricultural fields to create a new area of SANG;
 - New hedgerow planting with hedgerow trees including Elm, Lime and Oak;
 - Creation of four new ponds with aquatic planting and wet meadow grass mixes;
 - New Specimen trees, including Oak, Hornbeam and Black Poplar;
 - New native woodland and scrub planting (including in between the road and the ancient woodland area); and
 - New path network within the proposed extended SANG.
37. The Landscape Mitigation Plan provides sufficient information on the type of planting proposed both along the length of the distributor road at this stage and within the associated open spaces being provided. Detailed landscape plans would need to be agreed with the LPA prior to commencement under condition 12. This would be implemented in conjunction with a Landscape Management Plan (LMP) which would detail how the landscaped areas situated outside of the SANG management areas (i.e. those to be maintained by Highways contractors) would be secured under condition 13.

38. Although the alignment of the road would result in the loss of a number of trees and other vegetation, substantial replacement planting is indicated as part of the landscape proposals. Retained trees and hedges would be protected during the construction process (as required by condition 16) and condition 15 is proposed in order to ensure the new planting establishes successfully within the landscape.

39. In summary, the impact of this section of the NDR is considered to be acceptable in landscape terms, and the benefits of the road, subject to mitigation, are considered to outweigh the harm.

Heritage:

40. The Planning (Listed Buildings and Conservation Areas) Act 1990 establishes a statutory duty to consider the effect on heritage assets: development should preserve or enhance the character and setting of listed buildings. In considering designated heritage assets, the NPPF (Para 132) requires 'great weight to be given to the asset's conservation', and that any harm should require 'clear and convincing justification'. The MDD Local Plan (TB24) requires that '...works to or affecting heritage assets or their setting to demonstrate that the proposals would at least conserve and, where possible enhance the important character and special architectural or historic interest of the building.'

41. The Application Site does not contain any nationally designated (protected) heritage assets, such as scheduled monuments, listed buildings or registered parks and gardens. The site is also not located within a Conservation Area. Toutley Hall is a locally listed building located adjacent to the southern boundary of the site. However, the proposal is considered is not considered to result in any significant detrimental impacts to either its setting, or its character and appearance.

Flooding and Drainage:

42. Core Strategy Policy CP1 and MDDL Policies CC09 and CC10 establish that new development should avoid increasing and where possible reduce flood risk (from all sources) by first developing in areas with lowest flood risk, carrying out a Flood Risk Assessment (FRA) where required and managing surface water in a sustainable manner.

43. The majority of the application site falls within Flood Zone 1 where the risk of flooding is low. However, parts of the site are located within flood zones 2 & 3, these areas being located in proximity to the route of the Emm Brook. The application is supported by a Flood Risk Assessment, Drainage Strategy and Environmental Statement. The indicative Surface Water drainage strategy demonstrates that the proposed scheme will be drained via a network of swales which will be underlain by filter trenches, discharging to attenuation basins located within the extended SANG area adjacent to the road alignment.

44. Although Highways England raised no objection to the proposal, they conveyed a concern regarding water discharging into an existing ditch which runs along the bottom of the embankment of the M4 on land under their control. However, the existing overland flow routes would be intercepted by the road structure, and the attenuation proposed will cater for the 1 in 100 flood event with a 40% allowance

for climate change prior to controlled discharge into existing drainage ditches. Proposed condition 10 requires the submission of details to demonstrate that the existing rate of discharge would be maintained or improved. As such, the proposed attenuation measures will be required to ensure that the existing level of water flow into the ditch located on Highways England land will not increase as a result of the proposal, and may decrease. The proposal would also result in betterment to the existing drainage issues experienced by the properties located on Ashton Road and Summerfield Close through flood risk improvements to the Emm Brook located behind Summerfield Close which is currently prone to flooding issues.

45. The Environment Agency have reviewed the submitted Flood Risk Assessment and associated flood modelling details and have raised no objection to the proposal subject to the development being carried out in accordance with the submitted FRA. This is secured through condition 11.
46. The development is therefore considered acceptable in drainage terms subject to further ground investigation surveys, demonstration of SUDs feature levels, calculations demonstrating soakaway performance, and a drainage strategy and maintenance plan with water quality enhancements measures. Such details would be required to be submitted under proposed Condition 10.

Environmental Health and Residential Amenity:

47. Core Strategy Policy CP3 requires that new development should be of a high quality of design that does not cause significant detriment to the amenities of adjoining land users and their quality of life. The application Environmental Statement includes detailed assessment of noise, vibration and air quality.

Construction Noise and Vibration

48. The applicant has proposed a standard 6 day week and proposes the following working times for construction:
Monday – Friday – 0730 – 1700
Saturday 0800 – 1300
No work on Sundays or Public Holidays
49. However, the standard condition for Wokingham BC specifies a start time of 0800 Monday to Friday, and as such, it is not considered unreasonable to impose the standard working hours as a condition, given the proximity of the works to some of the nearby residential properties, particularly those located near to the north eastern end of the site on Toutley Road/Summerfield Close and those within the vicinity of the northern end of Old Forest Road. However, there would be no objection to the site operatives arriving on site at 0730 in order to commence work at 0800.
50. Evening and night time working will be required on occasions particularly for construction of the new railway bridge when possession of the railway line will be required. As such, the proposed condition would allow flexibility to the standard working hours to permit overnight working hours when possession of the railway line is obtained for the construction of the new bridge (condition 8). Where working hours are exceeded normal practice is that affected residents, local members and the Town Council would be given advance notice of planned

works. An informative is also included which refers to the requirement for prior consent under s61 of the Control of Pollution Act 1974 for control of noise during overnight activity (Informative 2)

51. The Noise and Vibration chapter of the Environmental Statement recognises the need for a Construction Environmental Management Plan (CEMP) that sets out how noise, vibration and other environmental impacts (such as construction dust) will be controlled during construction of the road. As such, a condition is recommended to secure this (condition 5).

Operational Noise

52. The Environmental Statement has considered the impact on residential dwellings and other noise sensitive receptors within the 75m calculation area within a 1km buffer. It also included new dwellings associated with ongoing developments in the vicinity. The assessment considers the impact of noise from traffic on the proposed road and from changes to traffic flows on the existing network. The existing background noise levels, notably the M4 and A329(M) which is elevated through this section are also considered.
53. Some minor decreases and minor increases in noise arising from the new road are predicted. Additional noise will be caused due to increased traffic flows along Toutley Road, Queens Road and Matthewsgreen Road and there will be noise reduction along Old Forest Road and Emmbrook Road. However, the report concludes that the majority of dwellings in the vicinity will experience a negligible change in noise.
54. The report identifies that five dwellings (89,91,93,95 & 97 Toutley Road) are likely to experience a 'moderate' increase in noise both in the short and longer term and noise mitigation measures have been explored to see if the impacts can be mitigated. The use of an acoustic screen here has been considered, as well as at locations on Old Forest Road and Toutley Road, where a minor increase in noise has been predicted. However, fencing would have a significant visual impact on the local area and such measures are prone to vandalism which is a reoccurring issue where these are used. The use of 'thin surface' (low noise surface) has been discounted because it would not be effective at low speeds (30-40mph) where road noise (tyre slap) is not dominant.
55. The ES advises that a preliminary assessment under the Noise Insulation Regulations has been carried out which identified 31 dwellings that could be eligible for a noise insulation grant. The report identifies that this number would decrease by 2 dwellings if acoustic barriers are installed and reduced by a further 4 if the barriers are extended. WBC Environmental Health officer advises that this aspect requires further consideration and recommends a suitable condition is proposed to ensure that this is given due consideration before work is completed and the road is opened for use. However, further assessment under the Noise Insulation Regulations would fall outside the remit of planning conditions, and should be undertaken as part of the Highway Authority's duty under the Noise Insulation Regulations (Informative 6 refers).
56. Therefore, in order to ensure that the construction of the road would not result in significant harm to the existing living conditions of the residential properties identified as potentially being affected by reason of noise, condition 9 is

recommended, which would require details to be submitted to demonstrate that a further noise assessment has been undertaken, and any mitigation measures identified as part of the assessment have been implemented prior to the operational use of the new road.

57. It is noted that there have been questions raised locally over noise from the M4 with respect to the installation of noise fences. This has been explored with Highways England who have determined that these cannot be installed as significant reinforcement of the embankments would be required which they are unwilling to undertake at this location.

Air quality

58. Chapter 6 of the ES considers the potential impacts on air quality both during construction and operation

Air Quality – Construction

59. The report considers exhaust fumes from construction vehicles and from dust arising (including PM10 and PM2.5). The report states that a CEMP will be implemented to minimise short term impacts on Air Quality including nuisance from dust. The submission of the CEMP would be secured through condition 5.

Air Quality – Operational

60. The report concludes that the proposed scheme will not lead to any additional exceedances of the Air Quality Objective both in the short or longer term (2019 and 2036). Substantial beneficial impacts are identified on Wiltshire Road and Broad Street, Wokingham. Moderate beneficial impacts are predicted on Rectory Road and there is a slight benefit for a Pub Garden on Reading Road. The report concludes that there will be negligible impacts elsewhere and as such, mitigation is not required. WBC Environmental Health welcomes these predicted improvements in AQ particularly in relation to the existing Air Quality Management Area (AQMA) in Wokingham Town Centre.

Contamination

61. The ES advises that some minor localised contamination has been identified but widespread contamination is not considered to be present. The ES concludes that no mitigation is required, however, WBC Environmental Health officer advises a condition is imposed relating to managing unexpected contamination encountered during construction (condition 20)

62. Construction activities such as vehicle re-fuelling, the operation of welfare facilities, earthworks, and stockpiling has the potential to contaminate the round, shallow surface water bodies, ground water and surface water bodies (e.g. Emm Brook) if not properly controlled. As such, the EH advises that mitigation measures including containment and appropriate storage of materials and waste should be included in the CEMP (condition 5).

Ecology:

63. Core Strategy Policy CP7, carried forward by MDD LP Policy TB23, requires appropriate protection of species and habitats of conservation value. Design Principle 1b (i-ii) is concerned with protection of ecological habitat and biodiversity features, together with mitigation of any impacts that do arise.

64. The ecological baseline status of the site and wider area been established through desk and field survey. A range of sites, habitats and species were considered in the Environmental Statement; including statutory and non-statutory wildlife sites ancient woodland, habitats of conservation importance, otter, bats, birds, reptiles and Great Crested Newts.
65. The results of the baseline surveys were used to assess the likely significant ecological effects of the proposed scheme both within the construction and operational phases, taking into account the various mitigation measures that are proposed.
66. In summary, while there are likely to be some moderate adverse effects to habitat during the short term construction phase, there will be residual positive ecological benefits in the longer term as a result of habitat creation. Therefore, the development would not have a significant effect on receptors.

Protected species

67. WBC Ecology officer has reviewed the application submission, and advises that the scheme, including the submitted Herptile Mitigation Strategy and compensation in the SANG extension, is capable of maintaining the favourable conservation status of great crested newts, a European Protected Species, as well as the two reptile species of principal importance identified as using the site. The implementation of the mitigation strategy is therefore secured through condition 17. A European Protected Species licence will need to be obtained from Natural England to permit the translocation of the Great Crested Newts, and WBC would expect the licence obtained to be submitted alongside the submission of the CEMP required by condition 5, in order to demonstrate that construction works are able to commence in accordance with the obtained licence.
68. With regards to bats, the proposed soft landscaping scheme is likely to increase the foraging quality of the site in the medium to long-term. WBC Ecology officer is satisfied that the risk to tree roosts has been adequately assessed and a relevant compensation level is proposed for the loss of potential roost features (for those features not completely ruled out as bat roosts). The implementation of the mitigation and compensation measures for bats as stated within Section 8.7 of the Environmental Statement is would be secured through the requirement to implement the general ecology related mitigation measures (condition 18). The proposal includes the provision of 27 bat boxes, the location of which will need to be agreed, and should therefore be included within the LEMP submission (condition 14).
69. The proposal has also been considered in respect of the implication of the proposed development on otters, which are identified as likely to be passing through this section of the river network. Once the Northern Distributor Road is created in full and the North Wokingham SDL is built out, there is likely to be more substantial traffic flow across the Emm Brook on the current Toutley Bridge. If otters are forced, due to high water flows under the bridge, to cross at road level then there is increased risk of collision.

70. WBC Ecology officer advises that the usual method of reducing this risk is to provide an otter ledge under the bridge that can work during flood events. However, as Toutley Bridge will not be subject to change in vertical alignment, it is not possible to put a viable otter ledge under this bridge to work in peak flows. The ecology officer therefore considers that the next best thing would be to encourage otters to use the new culverts for the Ashridge stream that are proposed to join in to the existing SANG. This can be done with appropriate use of fencing and planting in the SANG and on the Ashridge stream (East) side of Toutley Bridge. As such, the wording of the landscaping condition, includes provision to ensure that any proposed fencing must be suitable to provide mitigation for otters (condition 12).

Hedgerow

71. The Environmental Statement identifies that circa 654m of hedgerow will be lost at various points along the proposed alignment which would result in habitat fragmentation and a net loss for biodiversity. However, the landscape mitigation proposals include the creation of circa 1804m of new hedgerow running parallel to the road and connecting to hedgerows that have been severed and as such, this will adequately compensate for the initial loss over time. In addition, the landscape strategy proposes a greater amount of new woodland planting to act as a buffer between the remaining ancient woodland and the NDR.

Net gain for biodiversity

72. The NPPF para 170 (d) requires development to provide net gains for biodiversity where possible. WBC Ecology officer is satisfied that the proposed scheme demonstrates that there will likely be biodiversity net gain (BNG) in terms of habitats in accordance with the NPPF.

Thames Basin Heaths Special Protection Area

73. WBC commissioned a Habitats Regulations Assessment study to be undertaken in order to assess the cumulative impact of a number of infrastructure projects across the borough, and their potential impact of particulates on the Thames Basin Heaths Special Protection Area (SPA). The application scheme was included as part of the assessment, the results of which have been conveyed to Natural England who have confirmed that after reviewing the assessment they are satisfied that there would not be an adverse impact on designated sites in respect of Predicted Environmental Concentrations.

74. The Environment Agency response acknowledges that land alongside watercourses is particularly valuable as a corridor for wildlife. They have therefore requested a condition requiring a scheme for the provision and management of an 8 metre wide (minimum) buffer zone from the top of the bank of the Emm Brook. However, whilst WBC officers agree that such details are necessary, it is considered that the requested details are already covered within other recommended conditions. The requirement to provide an 8m buffer has been added to the requirements of the CEMP submission under condition 5, and details relating to planting, lighting, and ecological enhancements as detailed under the Environment Agency's recommended condition would be secured through conditions 3, 5, 12, 17 & 18. It is therefore considered that the proposal is acceptable in ecology terms, subject to the implementation of various proposed mitigation measures, as required by the referenced conditions cited above.

75. It is noted that the proposed condition from the Environment Agency also refers to a new road bridge being built over the Emm Brook. However, a new bridge is not being built in this location, although improvement works to the existing bridge will be undertaken under permitted development in order to provide the shared footway/cycleway on either side of the road.

Archaeology:

76. MDD Policy TB25 requires applicants to provide a detailed assessment of the impact on archaeological remains. If development is likely to affect an area of high archaeological potential or an area which is likely to contain archaeological remains, the presumption is that appropriate measures shall be taken to protect remains by preservation in situ. Where this is not practical, applicants shall provide for excavation, recording and archiving of the remains.

77. As part of the assessment of the impacts of the proposed road, desk-based research, followed by a programme of exploratory archaeological field evaluation, were undertaken along the proposed route. The results of this work, the baseline archaeological data and the assessment of the effects of the new road are set out in the Culture Heritage Chapter. The principal conclusions are as follows:

- There are no designated heritage assets within the red line boundary of this proposed scheme
- 108 exploratory trial trenches of varying lengths were excavated within the footprint of the proposed road line, plus a 'buffer' of 50m either side
- The trial trenches were largely devoid of any buried remains but the following features were recorded:

- a. A ditch containing a sherd of Roman or medieval pottery
- b. An undated ditch from which metalworking slag was recovered. The metalworking debris may be of Iron Age date
- c. The demolished remains of a WWII anti-aircraft battery
- d. Other buried modern (20th-century) rubble deposits, which may also have a military origin

78. Overall, the conclusions of the assessments and field evaluation were that the application area was considered to have a low to moderate potential for prehistoric remains.

79. An appropriate programme of archaeological work has been undertaken to inform the assessment of the impacts of this proposed road scheme on the buried archaeological heritage. Berkshire Archaeology advised that it is in broad agreement with the assessment's conclusions and proposals as set out in the Cultural Heritage chapter of the Environmental Statement. They also advised that while the exploratory field evaluation identified few buried remains, a focused programme of archaeological work would be merited should the scheme be permitted.

80. This might include further investigation of the area from which metalworking slag was recovered, the WWII remains, areas where exploratory trenching was limited and areas of other ancillary works, such as site compounds, temporary access roads, soil storage bunds, drainage works and landscaping. Therefore Berkshire Archaeology concurs with the report's conclusions that further archaeological

investigation of the application area is merited but advises this could be secured by condition should the scheme be permitted. This is in accordance with Paragraph 141 of the NPPF which states that local planning authorities should *'require developers to record and advance understanding of the significance of any heritage assets to be lost (wholly or in part) in a manner proportionate to their importance and the impact, and to make this evidence (and any archive generated) publicly accessible'*.

81. The development is therefore considered acceptable in archaeology terms, subject to the requirements of proposed Condition 19 being satisfied.

Community Infrastructure Levy (CIL):

82. The proposal is infrastructure development and therefore is not CIL liable.

CONCLUSION

The West of Old Forest Road section forms the final link of the NDR from Coppid Beech / London Road to Reading Road and links with the Winnersh Relief Road. The road would provide an essential piece of infrastructure that is required to support the Council's planned housing growth within the wider area and one which has been adopted as part of the Core Strategy and SPD's

The development provides for a safe, functional and well landscaped scheme, in keeping with the Council's spatial strategy. Appropriate mitigation is proposed to overcome impacts, where necessary. The proposal would provide benefits to the local area and town centre. The application complies with Development Plan policies and can therefore be recommended for approval.

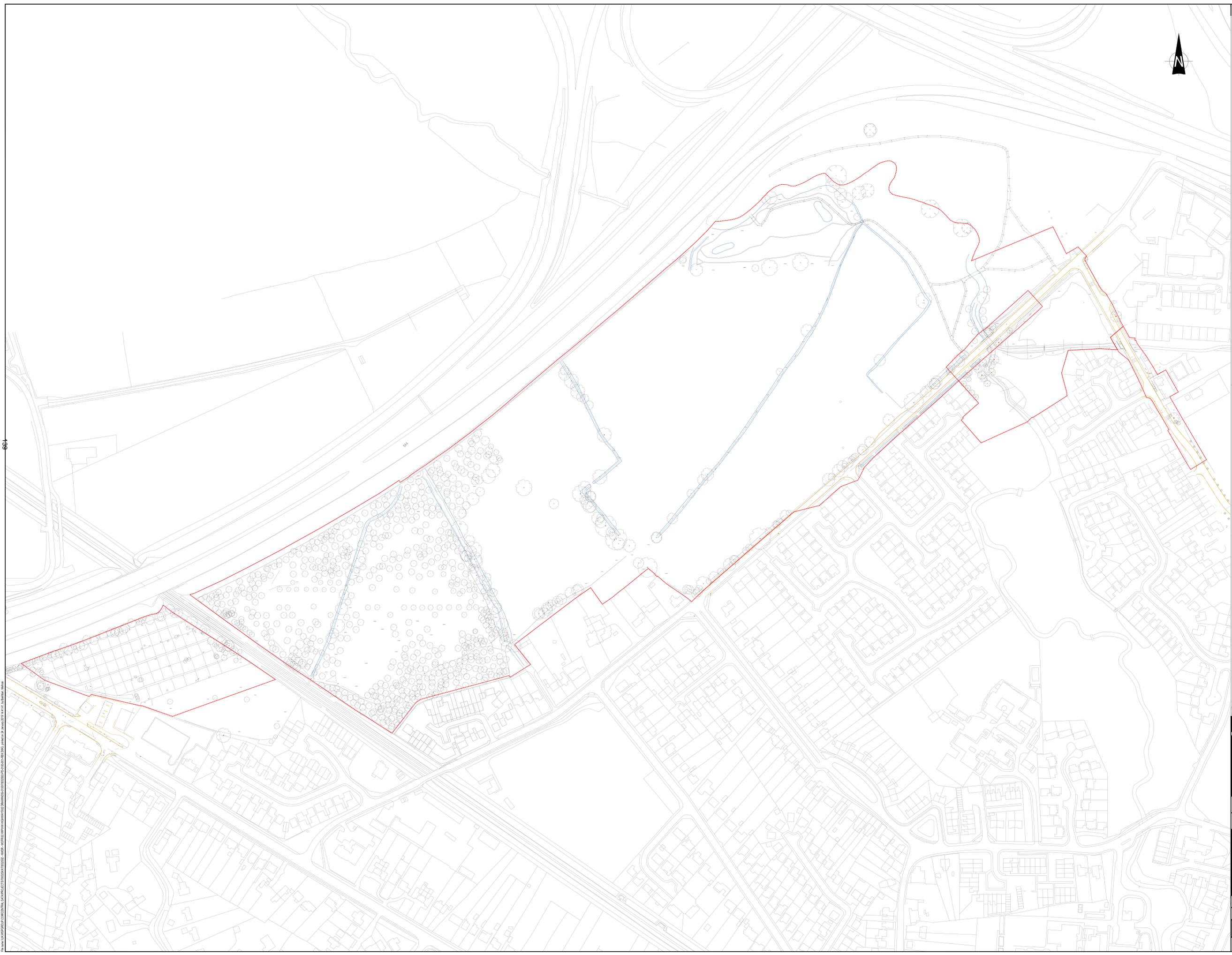
CONTACT DETAILS

Service	Telephone	Email
Development Management and Regulatory Services	0118 974 6428 / 6429	development.control@wokingham.gov.uk

Enclosures:

- 1) **Site location plan**
- 2) **General Arrangement Plan**
- 3) **Landscape General Arrangement Plan**

This page is intentionally left blank



DO NOT SCALE

- NOTES
1. THIS DRAWING SHOULD BE PRINTED IN COLOUR.
 2. ALL DIMENSIONS ARE IN METRES UNLESS OTHERWISE STATED.
 3. THIS DRAWING SHOULD BE READ IN CONJUNCTION WITH ALL OTHER RELEVANT ENGINEERING DRAWINGS.
 4. ALL PROPOSED DESIGNS ARE SUBJECT TO DETAILED DESIGN AND SHOWN INDICATIVELY.
 5. ANY DISCREPANCY SHOULD BE REPORTED TO THE ENGINEER IMMEDIATELY SO THAT CLARIFICATION CAN BE SOUGHT.



- KEY:
- WFR REDLINE BOUNDARY
 - S192 TOUTLEY BRIDGE REDLINE BOUNDARY
 - S192 TOUTLEY ROAD BRIDGE REDLINE BOUNDARY

REV	DATE	BY	DESCRIPTION	CHK	APP
DRAWING STATUS: S3 - FOR REVIEW					

wsp

2 Lansdowne Rd, Croydon, CR9 2ER, UK
T: +44 (0) 203 283 2413
wsp.com

CLIENT: Wokingham Borough Council

ARCHITECT:

PROJECT: West of Forest Road
Toutley Road

TITLE: S192 REDLINE BOUNDARY
Existing Alignment

SCALE @ A2	DESIGNED	MB	APPROVED	LS
Not to Scale	DESIGNED	MB	ERWIN	DATE
PROJECT No:	70032502	DESIGNED	MB	DATE
DRAWING No:	70032502-PD-0100-001-RBX	REV:	0	

© WSP UK Ltd

33

www.wsp.com | 2024-01-19 10:00:00 | 70032502-PD-0100-001-RBX | 0

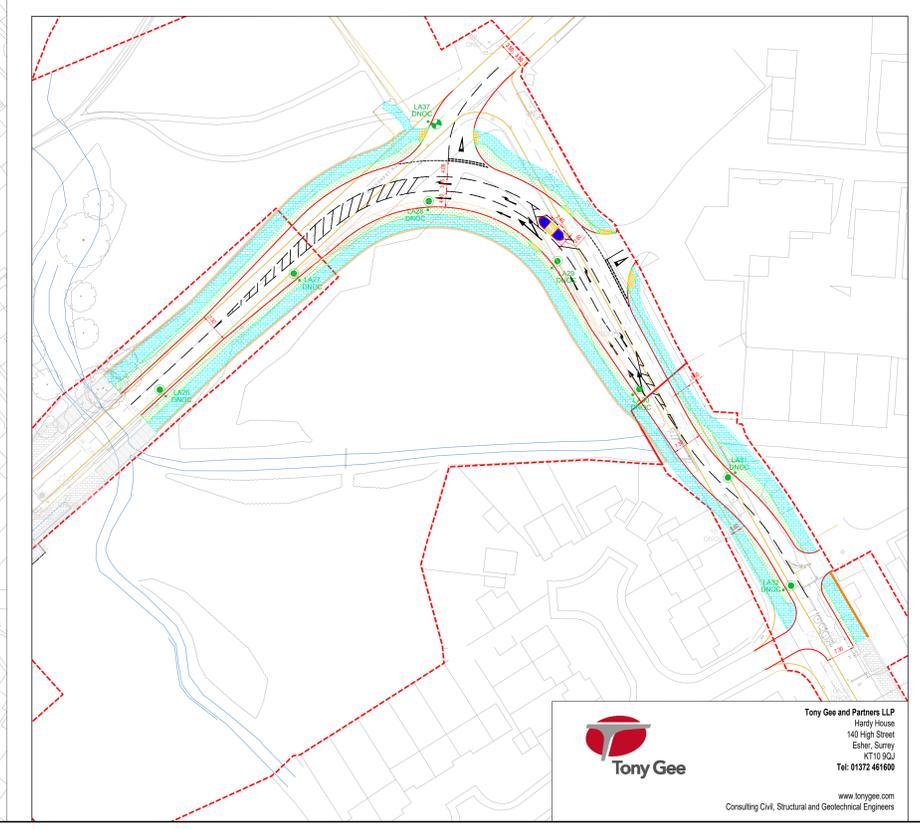
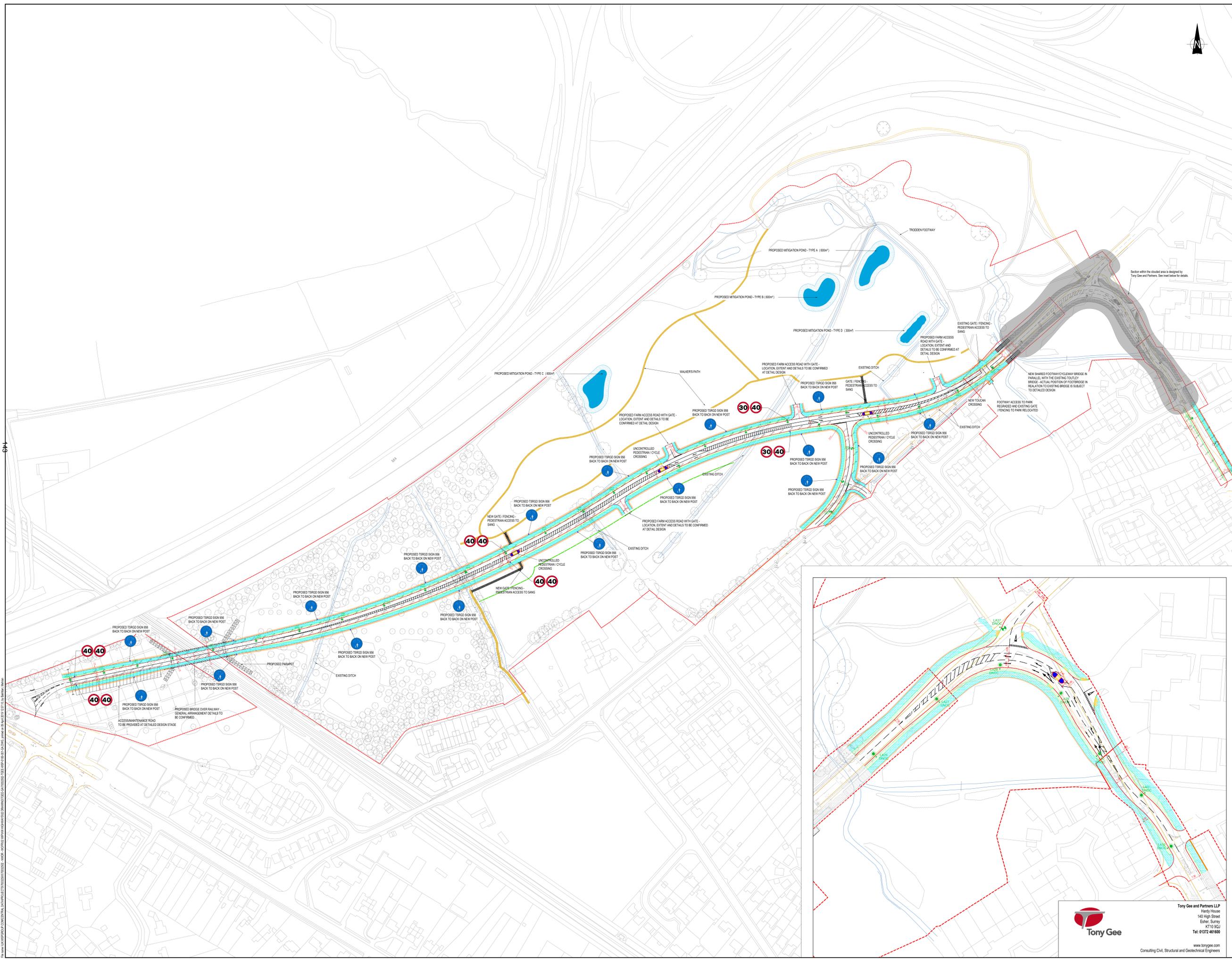
This page is intentionally left blank

This page is intentionally left blank



- GENERAL NOTES:
1. ALL DIMENSIONS ARE IN METRES UNLESS OTHERWISE STATED.
 2. DO NOT SCALE FROM THIS DRAWING. USE DIMENSIONS GIVEN IN THIS DRAWING OR BASED ON TOPOGRAPHICAL SURVEY AND AS BUILT DATA SUPPLIED BY WOKINGHAM BOROUGH COUNCIL.
 3. THIS DRAWING SHALL BE READ IN CONJUNCTION WITH ALL RELEVANT OTHER DRAWINGS AND SPECIFICATIONS.
 4. ALL WORKS TO BE CARRIED OUT IN ACCORDANCE WITH THE DEPARTMENT FOR TRANSPORT SPECIFICATION FOR HIGHWAY WORKS & SUPPLEMENTARY SPECIFICATION DOCUMENTS.
 5. ALL ROAD MARKINGS AND TRAFFIC SIGNS SHALL BE IN ACCORDANCE WITH THE TRAFFIC SIGNS REGULATION AND GENERAL DIRECTIONS 2016, AND TRAFFIC SIGNS MANUAL CHAPTERS 3, 4 AND 5.
 6. ANY DISCREPANCIES WITH THE ORIGINAL SITE SURVEY MUST BE REPORTED IMMEDIATELY TO THE SITE ENGINEER OR WOKINGHAM BOROUGH COUNCIL PROJECT MANAGER.
 7. CANOPY HEIGHT OF ALL AFFECTED TREES TO BE CUT BACK TO 5.5M.

- KEY:
- PROPOSED REINSTATED EXISTING WALKERS PATH
 - RED LINE BOUNDARY
 - PROPOSED FULL HEIGHT COUNTRY SIDE KERB 140MM WIDTH
 - PROPOSED TAPER COUNTRY SIDE KERB 140MM WIDTH
 - PROPOSED DROPPED COUNTRY SIDE KERB 140MM WIDTH
 - PROPOSED TACTILE PAVING AT CROSSING
 - PROPOSED OUTER VERGE
 - PROPOSED GRASS VERGE
 - PROPOSED SHARED FOOTWAY/CYCLEWAY
 - PROPOSED TRAFFIC ISLAND
 - PROPOSED PEDESTRIAN PATH TO SANG
 - PROPOSED ROAD MARKING
 - PROPOSED ARROWS
 - PROPOSED BRIDGE FOOTPATH/CYCLEWAY
 - PROPOSED FENCE
 - PROPOSED LAMP COLUMN
 - PROPOSED LAMP COLUMN NUMBER
 - EXISTING LAMP COLUMN TO BE REMOVED
 - EXISTING LAMP COLUMN NUMBER
 - EXISTING LAMP COLUMN TO BE RETAINED
 - EXISTING LAMP COLUMN NUMBER
 - PROPOSED TRAFFIC SIGN MOUNTED ON LAMP COLUMN
 - PROPOSED TRAFFIC SIGN



REV	DATE	BY	DESCRIPTION	CHK	APP

DRAWING STATUS: **S3 - FOR REVIEW**

wsp

2 Lansdowne Rd, Croydon, CR9 2ER, UK
T: +44 (0) 203 283 2413
wsp.com

CLIENT: **Wokingham Borough Council**

ARCHITECT: **Wokingham Borough Council**

PROJECT: **West of Forest Road
Toutley Road**

FILE: **General Arrangement
Combined design by TGE and WSP
Preliminary Design**

SCALE @ A2	DRAWN	SR	APPROVED	EH
Not to Scale	MB	SR	MB	EH

PROJECT NO.	DESIGNED	SR	DATE
70032502	MB	SR	April 2019

DRAWING NO.	REV
70032502-TGEE-WSP-0100-001-GA	0

© WSP UK Ltd

Tony Gee

Tony Gee and Partners LLP
Hardy House
140 High Street
Esher, Surrey
KT10 8QJ
Tel: 01372 461600
www.tonygee.com
Consulting Civil, Structural and Geotechnical Engineers

This page is intentionally left blank

Agenda Item 91.

Application Number	Expiry Date	Parish	Ward
180753	9 May 2019	Winnersh	Winnersh South

Applicant	Mr Sundeep Saxena
Site Address	Trident House, 2 King Street Lane, Winnersh RG11 5AS
Proposal	Full application for the proposed change of use, raising of the roof, single storey rear extension and alterations to fenestration to provide 12 flats with rear amenity space and onsite parking
Type	Full
PS Category	1
Officer	Simon Taylor
Reason for determination by committee	Major application comprising 12 dwellings

FOR CONSIDERATION BY	Planning Committee on Wednesday, 8 May 2019
REPORT PREPARED BY	Assistant Director – Place

SUMMARY
<p>The subject site is within in a very urban location, situated alongside a side road to King Street Lane, very near the main intersection with Reading Road and surrounded by several non-residential uses, including Sainsbury's supermarket and carpark opposite.</p> <p>There is an existing two storey office building on the site, which was granted prior approval for conversion to five residential flats in 2014 (OFF/2013/2542). Despite suggestions to the contrary, the Council is satisfied that the works were never commenced and were indeed never completed within the required timeframe. A subsequent application for the conversion of the existing building with extensions to create 18 units (162148) was refused in 2017 for reasons of overdevelopment, impacts upon neighbour amenity, substandard internal and external accommodation and lack of affordable housing. An appeal against this decision was dismissed on all grounds.</p> <p>The subject application is a resubmission of the appealed application, with a reduction in the built form resulting in a 12 unit scheme, with a mix of one and two bedroom units across three levels. It has also been amended twice since its original submission to address the concerns of Council's Highways Officer, Landscape Officer and Planning Officer.</p> <p>One neighbour submission was received against the proposal, citing neighbour amenity concerns, over development and pollution issues associated with the car parking at the rear of the site. Winnersh PC have also objected on the grounds of a lack of parking.</p> <p>As amended, the scheme represents an appropriate reuse of an existing brownfield office building in a highly sustainable location with ready access to public transport and facilities. The extensions to the building are modest in nature and are a significant reduction from that proposed in the previous scheme. By reducing the scale of the building, the neighbour amenity concerns have also been resolved.</p> <p>Furthermore, the Council's Highways Officer is satisfied with the on-site parking provision although vehicular and pedestrian access to and from the site remains subject to further details (Condition 7) and a Car Parking Management Plan (CPMP) (Condition</p>

8). Other relevant pre commencement conditions relate to landscaping details and the protection of a veteran Oak tree on the front boundary (Conditions 3 and 4) and a Construction Management Plan (given the location on a busy intersection) (Condition 6).

A viability report was submitted in relation to the provision of affordable housing. It was externally reviewed and a commuted sum of £2,504 was agreed, which is the equivalent of the developer return in excess of 17.5%. This will be secured by legal agreement (see Informative 1).

PLANNING STATUS

- Major Development Location
- Winnersh District Centre
- Secondary shopping frontage
- Flood zone 1
- Veteran tree 6803 (English Oak on front boundary)
- Wind turbine safeguarding zone
- Sand and gravel extraction consultation zone
- Potentially contaminated land consultation zone
- Non-classified road

RECOMMENDATION

That the committee authorise the GRANT OF PLANNING PERMISSION subject to the following:

- A. Completion of a legal agreement to secure the contributions associated with Affordable Housing (see Informative 1); and**
- B. Conditions and informatives:**

Conditions

1. Timescale

The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason: In pursuance of s.91 of the Town and Country Planning Act 1990 (as amended by s.51 of the Planning and Compulsory Purchase Act 2004).

2. Approved plans

This permission is in respect of the submitted plans numbered TRIDENT/PL07L, TRIDENT/PL20H and TRIDENT/PL21F, all dated April 2019 and received by the local planning authority on 25 April 2019. The development shall be carried out in accordance with the approved details unless other minor variations are agreed in writing after the date of this permission and before implementation with the Local Planning Authority.

Reason: For the avoidance of doubt and to ensure that the development is carried out in accordance with the application form and associated details hereby approved.

3. Landscaping details

No development shall commence until full details of both hard and soft landscape proposals have been submitted to and approved in writing by the local planning authority.

These details shall include, as appropriate, proposed finished floor levels or contours, means of enclosure, car parking layouts, other vehicle and pedestrian access and circulation areas, hard surfacing materials and minor artefacts and structure (e.g. furniture, play equipment, refuse or other storage units, signs, lighting, external services, etc).

Soft landscaping details shall include a planting plan, specification (including cultivation and other operations associated with plant and grass establishment), schedules of plants, noting species, planting sizes and proposed numbers/densities where appropriate, and implementation timetable. It must include trees and hedges with adequate space to thrive, and access for maintenance. The hedges should be a mix of native and ornamental but pollinator friendly species. Trees must be planted with adequate space to thrive and according to current guidance. Tree planting details are required to demonstrate the how the trees will be planted in the different areas across the site, e.g. depending on the site constraints for the particular location, if planted in open ground, street etc. but must include as standard irrigation. Small areas of lawn should be avoided and where such spaces exist be replaced with shrub planting.

All hard and soft landscape works shall be carried out in accordance with the approved details prior to the occupation of any part of the development or in accordance with a timetable approved in writing by the local planning authority. Any trees or plants which, within a period of five years after planting, are removed, die or become seriously damaged or defective, shall be replaced in the next planting season with others of species, size and number as originally approved and permanently retained.

Reason: In the interests of visual amenity.

Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21.

4. Protection of trees

No development or other operation shall commence on site until an Arboricultural Method Statement and Scheme of Works, which provides for the retention and full protection of the veteran Oak tree at the front boundary in accordance with BS5837: 2012, has been submitted to and approved in writing by the local planning authority. No development or other operations shall take place except in complete accordance with the details as so-approved (hereinafter referred to as the Approved Scheme). The updated details are to include the provision of subterranean services connecting to each of the dwellings.

No operations shall commence on site in connection with development hereby approved (including any tree felling, tree pruning, demolition works, soil moving, temporary access construction and or widening or any other operation involving

use of motorised vehicles or construction machinery) until the tree protection works required by the Approved Scheme are in place on site.

No excavations for services, storage of materials or machinery, parking of vehicles, deposit or excavation of soil or rubble, lighting of fires or disposal of liquids shall take place within an area designated as being fenced off or otherwise protected in the Approved Scheme.

The fencing or other works which are part of the Approved Scheme shall not be moved or removed, temporarily or otherwise, until all works including external works have been completed and all equipment, machinery and surplus materials removed from the site, unless the prior approval in writing of the local planning authority has first been sought and obtained.

Reason: To secure the protection throughout the time that the development is being carried out of trees shrubs or hedges growing within or adjacent to the site which are of amenity value to the area, and to allow for verification by the local planning authority that the necessary measures are in place before development and other works commence

Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21

5. Drainage details

No development shall commence until drainage details have been submitted to and approved in writing by the local planning authority. It must ensure that surface water from the vehicular access does not discharge onto the highway and include the following:

- a) BRE 365 test results demonstrating whether infiltration is achievable or not.
- b) Full calculations demonstrating the performance of soakaways.
- c) Groundwater monitoring to confirm seasonal high groundwater levels.
- d) A drainage strategy plan indicating the location and sizing of SuDS features, with the base of any SuDS features located at least 1m above the seasonal high water table level.
- e) Details demonstrating how any SuDS for this development would be managed throughout the lifespan of the development and who will be responsible for maintenance.

Such details as may be approved shall thereafter be installed and operational prior to first use and thereafter maintained.

Reason: This is to prevent increased flood risk from surface water run-off.

Relevant policy: NPPF Section 10, Core Strategy policy CP1 and Managing Development Delivery Local Plan policies CC09 and CC10.

6. Construction Management Plan

No development shall commence until a Construction Management Plan and Construction Method Statement has been submitted to and approved in writing by the local planning authority. The plan should include the following details:

- a) Phasing of construction
- b) Lorry routing and potential numbers
- c) Types of piling rig and earth moving machinery
- d) Temporary lighting
- e) Parking of vehicles of site operatives and visitors
- f) Estimation of the excavated soil and concrete rubble removed from the site
- g) Loading and unloading of plant and materials
- h) Storage of plant and materials used in constructing the development
- i) Erection and maintenance of security hoarding including decorative displays and facilities for public viewing, where appropriate,
- j) Wheel washing facilities,
- k) Measures to control the emission of dust, dirt and other matters during construction,
- l) A scheme for recycling/disposing of waste resulting from demolition and construction works.

The plan shall be implemented and adhered to in full and retained until the development has been constructed. Any deviation from this Statement shall be first agreed in writing with the local planning authority.

Reason: In the interests of highway safety and convenience and neighbour amenities.

Relevant policy: Core Strategy policies CP3 and CP6.

7. Parking and access details

No development shall commence until additional car parking details have been submitted to and approved in writing by the local planning authority. Unless otherwise agreed in writing by the local planning authority, this should include the following details:

- a) Vehicle turning space within the site (allowing vehicles to turn so that they may enter and leave the site in a forward gear)
- b) Secure and covered bicycle storage
- c) Unimpeded pedestrian access to the cycle storage
- d) Methods to be used to manage the potential for in/out vehicle conflict along the single width driveway and allow for a vehicle waiting area
- e) Defined pedestrian access from the street kerb to the rear of the site, including delineated markings and surfacing on the roadway
- f) An Electric Vehicle (EV) charging strategy, including on-site infrastructure, installation of charging points and future proofing of the site

None of the residential units shall be occupied until the above aspects have been provided in full accordance with the approved details and shall thereafter be so-retained.

Reason: To ensure a satisfactory form of development, to avoid adverse impact on the public highway in the interests of highway safety and to encourage the use of sustainable modes of travel.

Relevant policy: NPPF Section 9 and Core Strategy policies CP1, CP3 and CP6 and Managing Development Delivery Local Plan policy CC07.

8. Car Parking Management Plan

No development shall commence until a Car Park Management Plan (CPMP) has been submitted and approved in writing by the local planning authority. As a minimum, the CPMP shall detail the following:

- a) Provision of unallocated parking and methods of ensuring that the spaces remain unallocated and will remain in force for the life of the development
- b) Arrangements for access to the carpark at the rear of 4 King Street Lane
- c) Details of gate operations
- d) Signage to direct residents to park front into the spaces on the northern side of the site

Reason: To ensure adequate on-site parking provision in the interests of highway and pedestrian safety, convenience and amenity.

Relevant policy: Core Strategy policies CP3 and CP6 and Managing Development Delivery Local Plan policy CC07.

9. Bin storage details

No development shall commence until details of bin storage areas for each dwelling have been submitted to and approved in writing by the local planning authority. The storage area shall be permanently so-retained and used for no purpose other than the temporary storage of refuse and recyclable materials.

Reason: In the interests of visual and neighbouring amenities and functional development.

Relevant policy: Core Strategy CP3 and Managing Development Delivery Local Plan policy CC04.

10. Access details

No development shall commence until a scheme indicating the provision of at least two dwellings being made adaptable for disabled people to gain access has been submitted to and approved in writing by the local planning authority. The Scheme so-approved shall be implemented before the development hereby permitted is brought into use, and shall thereafter be retained.

Reason: To ensure the needs of the general population are met.

Relevant policy: Core Strategy policy CP2.

11. Renewable energy details

No development shall commence until a scheme for generating 10% of the predicted energy requirement of the development from decentralised renewable and/or low carbon sources (as defined in the glossary of Planning Policy Statement: Planning and Climate Change (December 2007) or any subsequent version) has been submitted to and approved in writing by the local planning authority. The approved scheme shall be implemented before the development is first occupied and shall remain operational for the lifetime of the development.

Reason: To ensure developments contribute to sustainable development.

Relevant policy: NPPF Section 14 (Meeting the Challenge of Climate Change, Flooding and Coastal Change), Core Strategy policy CP1, Managing Development Delivery Local Plan policy CC05 & the Sustainable Design and Construction Supplementary Planning Document.

12. Project Community Liaison Group

No development shall take place until a scheme has been submitted to and approved in writing by the Local Planning Authority which specifies the provisions and details to be made to establish a Project Community Liaison Group for the duration of the construction works. The scheme shall include details of dates of meetings and a name and telephone number for residents to contact should any issues arise during the construction period. The scheme shall be implemented as approved.

Reason: In order to minimise disturbance to neighbours during construction works.

13. Contamination details

In the event that contamination is found at any time when carrying out the approved development, it must be reported in writing immediately to the Local Planning Authority. Any subsequent investigation/remedial/protective works deemed necessary by the LPA shall be carried out to agreed timescales and approved in writing by the local planning authority. If no contamination is encountered during the development correspondence confirming this fact shall be submitted to the LPA upon completion of the development

Reason: To ensure that any contamination on the site is remediated to protect the existing/proposed occupants of the application site and adjacent land.

Relevant policy: Relevant policy: NPPF paragraphs 170, 178 and 179.

14. Car parking

None of the residential dwellings hereby permitted shall be occupied until the vehicle parking has been provided in accordance with the approved plans. The parking be retained and maintained in accordance with the approved details and remain available for the parking of vehicles at all times.

Reason: To provide adequate off-street vehicle parking and providing a functional, accessible and safe development and in the interests of amenity.

Relevant policy: Core Strategy policies CP3 & CP6 and Managing Development Delivery Local Plan policy CC07.

15. Bonded surface

None of the residential dwellings hereby permitted shall be occupied until the vehicular access has been surfaced or is retained with a permeable and bonded material across the entire width of the access for a distance of 10 metres measured from the carriageway edge.

Reason: To avoid spillage of loose material onto the highway, in the interests of road safety.

Relevant policy: Core Strategy policy CP6.

16. Permeable paving

None of the residential dwellings hereby permitted shall be occupied until all new hard surfacing is constructed from porous materials or provision made to direct water run-off from the hard surface to a permeable or porous area within the curtilage of the development. The hard surfacing shall thereafter be so-maintained.

Reason: To prevent increased flood risk from surface water run-off.

Relevant policy: NPPF Section 14 and Managing Development Delivery Local Plan policies CC09 and CC10

17. Hours of construction work and deliveries

Unless otherwise agreed in writing by the local planning authority, the hours of work and deliveries and vehicle movements to and from the site for all contractors for the duration of the construction of the development shall be limited to 8am-6pm, Mondays to Fridays and 8am to 1pm on Saturdays. No work shall be carried out on Sundays or Bank Holidays.

Reason: In the interests of the amenity of neighbouring occupiers.

Relevant policy: CP3 of the Core Strategy

18. Retention of trees and shrubs

No trees, shrubs or hedges within the site which are shown as being retained on the approved plans shall be felled, uprooted wilfully damaged or destroyed, cut back in any way or removed without previous written consent of the local planning authority; any trees, shrubs or hedges removed without consent or dying or being severely damaged or becoming seriously diseased within 5 years from the completion of the development hereby permitted shall be replaced with trees, shrubs or hedge plants of similar size and species unless the local planning authority gives written consent to any variation.

Reason: To secure the protection throughout the time that development is being carried out, of trees, shrubs and hedges growing within the site which are of amenity value to the area.

Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21.

Informatives

1. Section 106 Agreement

This permission should be read in conjunction with the legal agreement under Section 106 of the Town and Country Planning Act dated # May 2019, the obligations of which relate to the securing affordable housing contributions for the scheme.

2. Pre Commencement Conditions

The applicant is reminded that this approval is granted subject to conditions which must be complied with prior to the development starting on site. Commencement of the development without complying with the pre-commencement requirements may be outside the terms of this permission and liable to enforcement action. The information required should be formally submitted to the Council for consideration with the relevant fee. Once the details have been approved in writing the development should be carried out only in accordance with those details. If this is not clear please contact the case officer to discuss.

3. Changes to the Approved Plans

The applicant is reminded that should there be any change from the approved drawings during the build of the development this may require a fresh planning application if the changes differ materially from the approved details. Non-material changes may be formalised by way of an application under s.96A Town and Country Planning Act 1990.

4. Protected Species

This permission does not convey or imply any approval or consent required under the Wildlife and Countryside Act 1981 for protected species. The applicant is advised to contact Natural England with regard to any protected species that may be found on the site.

Should any bats or evidence of bats be found prior to or during the development, all works must stop immediately and an ecological consultant or the Council's ecologist contacted for further advice before works can proceed. All contractors working on site should be made aware of the advice and provided with the contact details of a relevant ecological consultant.

5. Thames Water

There are water mains crossing or close to your development. Thames Water do not permit the building over or construction within 3m of water mains. If you are planning significant works near our mains (within 3m) we'll need to check that your development does not reduce capacity, limit repair or maintenance activities during and after construction, or inhibit the services we provide in any other way. Refer to <https://developers.thameswater.co.uk/Developing-a-large-site/Planning-your-development/Working-near-or-diverting-our-pipes>

Thames Water will aim to provide customers with a minimum pressure of 10m head (approx 1 bar) and a flow rate of 9 litres/minute at the point where it leaves Thames Waters pipes. The developer should take account of this minimum pressure in the design of the proposed development.

6. Community Infrastructure Levy

The development hereby permitted is liable to pay the Community Infrastructure Levy. The Liability Notice issued by Wokingham Borough Council will state the current chargeable amount. A revised Liability Notice will be issued if this amount changes. Anyone can formally assume liability to pay, but if no one does so then liability will rest with the landowner. There are certain legal requirements that must

be complied with. For instance, whoever will pay the levy must submit an Assumption of Liability form and a Commencement Notice to Wokingham Borough Council prior to commencement of development. For more information see <http://www.wokingham.gov.uk/planning/developers/cil/cil-processes/>.

7. Proactive and Positive Discussion

The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received. This planning application has been the subject of positive and proactive discussions with the applicant in terms of extending the determination period of the application to allow for consideration at the Planning Committee and amended plans being submitted by the applicant to overcome planning concerns.

PLANNING HISTORY

App No.	Description	Decision/Date
1979/1973	Warehouse, offices and service depot (outline)	Refused 14 January 1974
-	Appeal against refusal of 1979/1973	Dismissed 28 February 1975
03418	CoU of residential to offices	Approved 30 October 1975
14442	Continued used as offices	Approved 26 November 1982
22802	Office extensions and car parking	Approved 7 March 1985
BR 24070	Office extension	Approved 28 January 1985
40591	Two storey extension and eight car parking spaces	Approved 5 March 1993
FP96/5582	Loading bay	Approved 7 August 1996
F/2003/9652	Relocation of access and ground floor infill extension for additional office floorspace, new entrance canopy, fence and gates	Approved 5 September 2003
OFF/2013/2542	Conversion of offices to create five residential units (prior approval)	Approved 13 February 2014
162148	CoU and extension to create 18 flats	Refused 28 April 2017
W/17/3177248	Appeal against refusal of 162148	Dismissed 28 November 2017

SUMMARY INFORMATION

Site Area	1,200m ²
Previous land use	Two storey office building
Proposed units	12 dwellings (nine x 1-bedroom units and three x 2-bedroom units)
Proposed density	100 dwellings per hectare
Number of affordable units	Nil (Reduced commuted sum in lieu of on site provision)
Existing parking spaces	Informal parking for up to 22 spaces
Proposed parking spaces	12 spaces

CONSULTATION RESPONSES

WBC Highways	No objection, subject to conditions requiring additional parking details (Condition 7), turning details (Condition 7) and a Construction Management Plan (Condition 6) and Car Parking Management Plan
--------------	--

	(Condition 8). Refer to 'Highway Access and Parking Provision' for further assessment.	
WBC Trees and Landscaping	The lack of landscaping to the side boundary treatment is unacceptable and there is no space or plans to provide landscaping in the rear garden. <u>Officer comment:</u> Refer to further comments in 'Landscaping and Trees' and Conditions 3 and 4.	
WBC Environmental Health	No objection, subject to conditions relating to contamination and remediation and construction working and delivery hours. See Conditions 13 and 17.	
WBC Affordable Housing	No objection, subject to a commuted sum totalling £140,393.50 in lieu of the provision of 2.4 on site affordable housing units. <u>Officer comment:</u> A viability report was submitted and reviewed, with an agreed affordable housing contribution of £2,504. Refer to 'Housing Affordability' for further assessment.	
WBC Drainage	No objection, subject to additional drainage details being provided as a pre commencement Condition 5.	
WBC Cleaner and Greener	No objection.	
WBC Education		
WBC Growth and Delivery	No comments received.	
WBC Property Services		
WBC Sports Development		
Fire Service		
Fire Service		
SSE Power Distribution		
Southern Gas Networks		
National Grid		
Crime Prevention Officer		
Thames Water		No objection in terms of capacity within the waste and water network.

REPRESENTATIONS	
Winnersh Parish Council	Opposition is raised on the grounds of a lack of parking. <u>Officer comment:</u> The level of parking provision is acceptable, as noted in 'Highway Access and Parking Provision'.
Ward Member	Councillor Prue Bray initially raised concerns with some of the documentation but raises no in-principle objection.
Neighbours	The application was initially consulted to neighbours from 11 April to 7 May 2018 and the revised plans were then reconsulted from 15 February to 1 March 2019. One submission was received in relation to both the original and revised scheme. It raised the following issues:

	<ul style="list-style-type: none"> • Out of character with the area <p><u>Officer comment:</u> The building is acceptable in terms of its relationship with the surrounding built form, as noted in 'Character of the Area'.</p> <ul style="list-style-type: none"> • Overlooking, loss of sunlight and daylight and overbearing form as viewed from 404 Reading Road • Noise, dust and air pollution from car parking <p><u>Officer comment:</u> The level of amenity afforded to the neighbouring property and all other neighbouring properties is acceptable, as noted in 'Neighbour Amenity'.</p>
--	---

APPLICANTS POINTS	
	<ul style="list-style-type: none"> • There have been many flat developments and conversions to this area • All areas modified if required, with ramps low level switches, stair lifts/main lift and main door openings at 1m wide • There are green areas within a 200m radius of the site • An affordable housing report has been submitted with the application

PLANNING POLICY	
National Policy	National Planning Policy Framework (NPPF) National Planning Policy Guidance (NPPG)
Core Strategy 2010 (CS)	Policy CP1 – Sustainable Development Policy CP2 – Inclusive Communities Policy CP3 – General Principles for Development Policy CP5 – Housing Mix, Density and Affordability Policy CP6 – Managing Travel Demand Policy CP7 – Biodiversity Policy CP9 – Scale and Location of Development Proposals Policy CP13 – Town Centres and Shopping Policy CP17 – Housing Delivery
Managing Development Delivery Local Plan 2014	Policy CC01 – Presumption in Favour of Sustainable Development Policy CC03 – Green Infrastructure, Trees and Landscaping Policy CC04 – Sustainable Design and Construction Policy CC06 – Noise Policy CC07 – Parking Policy CC09 – Development and Flood Risk Policy CC10 – Sustainable Drainage Policy TB05 – Housing Mix Policy TB07 – Internal Space Standards Policy TB15 – Major Town, and Small Town/District Centre development Policy TB16 – Development for Town Centre Uses Policy TB21 – Landscape Character Policy TB23 – Biodiversity and Development
Other	Borough Design Guide Supplementary Planning Document Affordable Housing Supplementary Planning Document DCLG – National Internal Space Standards

PLANNING ISSUES

Description of Development

1. The proposal, as amended on 15 February 2019 and 24 April 2019, involves the following works:
 - Change of use of the existing building from offices to residential
 - Removal of the existing roof and addition of a new second floor over part of the existing building
 - Single storey rear extension
 - Provision of nine x 1-bedroom units and three x 2-bedroom units, with five units on the ground floor, four units on the first floor and three units on the second floor
 - New access and parking arrangements, comprising two parking spaces at the front of the site (including one accessible space), use of the existing access to ten parking spaces at the rear of the site and provision for two motorcycle spaces at the front of the site
 - Provision of a communal open space area at the rear of the building and private amenity space for Unit 1 at the front of the site
 - Provision for cycle storage and bin storage and other ancillary site works

2. This application is a resubmission of the refused planning application 162148, which was also dismissed at appeal (APP/0360/W/17/3177248) and incorporates the following amendments:
 - Reduction in the number of units from 18 to 12 (including the loss of two studio units and four x 1-bed units)
 - Reduction in the overall ridge height from 10.9m to 8.6m and the deletion of the third floor in its entirety
 - Deletion of the external roof terraces at the front of the third floor and rear of the second floor
 - Deletion of the rear extension at first floor level and reduction in the length of the new second floor
 - Reduction in the number of car spaces from 18 to 12, including the reduction in the number of accessible spaces from 2 to 1
 - Provision of 90m² of communal open space at the rear of the building
 - Provision of shared pedestrian access to the rear of the site
 - Provision of motorcycle parking

Site Description

3. The site is located on the western side of King Street Lane, just south of the intersection of Reading Road. It is a narrow 1,200m² block with a plot frontage of 15m and depth of 71m. On the site is a two storey office building with hard surfacing across most of the site.

4. The site forms part of the Winnersh District Centre, with Sainsbury's supermarket to the east, an office building and warehouse containing Magnet Kitchens to the south, a car wash to the immediate north and retail buildings lining Reading Road further to the north. Beyond this, residential dwellings predominate the wider area.

Principle of Development

5. The National Planning Policy Framework has an underlying presumption in favour of sustainable development which is carried through to the local Development Plan. Policy CC01 of the MDD Local Plan states that planning applications that accord with the policies in the Development Plan for Wokingham Borough will be approved without delay, unless material considerations indicate otherwise.
6. Policy CC02 of the MDD sets out the development limits for each settlement as defined on the policies map and therefore replaces the proposals map adopted through the Core Strategy, as per the requirement of policy CP9. Policy CP9 sets out that development proposals located within development limits will be acceptable in principle, having regard to the service provisions associated with the major, modest and limited categories. As the site is within a major development location, the proposal is acceptable in principle.

Sustainability

7. Policies CP1, CP6, CP9 of the Core Strategy permit development where it is based on sustainable credentials in terms of access to local facilities and services and the promotion of sustainable transport. The site is well located in terms of access to services and facilities, including a variety of retail shops in the immediate proximity, including a major supermarket. It is also 100m from a bus stop with regular services throughout peak, non-peak and evening hours between Reading and Bracknell, 250m from Winnersh Train Station, which serves Reading to London Waterloo and Gatwick and 1.5km from Winnersh Triangle Business Park. It is within a major development location and is acceptable in terms of the sustainability principles stated in the Core Strategy.

Prior approval fallback position

8. A prior approval was granted for the conversion of the existing office building to create eight residential units (reference OFF/2013/2542). The Design and Access Statement submitted with this planning application suggests that '*the works are being carried out for this conversion at present.*'
9. This contradicts the findings of the Inspector in the appeal decision in paragraph 3, where it states that '*on my internal inspection of the site, I saw little evidence of that other than the clearance out of office furniture*' and more recent correspondence submitted as part of discussions with this application where it was indicated that '*the owners currently have a couple of temporary office tenants in the building*'. Regardless of the above, the residential conversion is required to be completed within three years from the date of the decision, or no later than 13 February 2017. As the works have clearly not been completed, there is currently no fallback option associated with the prior approval.

Loss of office floorspace

10. Policy CP15 of the Core Strategy (in addition to Policy TB21 of the MDD Local Plan) states that any change of use from Class B1/B2/B8 should not lead to an overall net loss of Class B floorspace. The basis behind this is outlined in paragraph 4.70, which refers to an Employment Land Study (2006) which sets out that the supply of floor space for industry and warehousing needs to increase by 51,000m² to meet forecast Class B use growth in the borough over the Development Plan period to 2026. The Council's most recent Employment Land Monitoring Report (ELMR) for April 2013–March 2014 demonstrates that there was a net gain of 3,649m² of Class B use floor space in the borough from 1 April 2006.
11. Paragraph 4.71 also states that the creation of additional floorspace on existing and new sites would allow for the reuse of some existing employment sites for other uses where there is a demand for other uses and/or a lack of demand for business uses without a net loss in employment floorspace.
12. The site is currently B1(a) office space and its location in Winnersh and outside a Core Employment Area offers an alternative location, site and size of employment. The retention of existing sites is an integral part of ensuring borough wide variety. Were such sites allowed to be lost unchecked, without appropriate justification, this would clearly run contrary to the qualitative policy aim to ensure variety in provision across the borough. The proposed loss of this alternative site without appropriate justification is, in the Council's view, contrary to Policy CP15.
13. There is generally a lack of supporting justification advanced by the applicant, whether in the form of marketing and or viability evidence or in any other appropriate form, for a proper consideration to be given to whether a departure from policy was appropriate.
14. Section 8 of the Affordable Housing Variability Report states that there is a general lack of demand for offices of this type and location. There has also been correspondence stating that there are temporary office tenants currently occupy the building but they are not paying rent and only paying ongoing rates. At the request of the planning officer, comment was provided which referred back to the history of the site and the fallback nature of the lawful development certificate. In short, the previously refused and dismissed application was submitted whilst the previous lawful development certificate was extant and the offices were still occupied. The subject application represents a resubmission of this application but it was lodged after the lawful development certificate expired. It was also submitted in March 2018. The site is now unoccupied and has been for the past few months.
15. The previous planning application did not raise any concerns on the grounds of loss of office floorspace and it was not raised at appeal stage. It would not be in the spirit of the planning process to pursue this avenue under this application and for this reason, the issue is not raised as an objection.

Character of the Area

General form

16. Policy CP3 of the Core Strategy states that development must be appropriate in terms of its scale, mass, layout, built form, height and character of the area and

must be of high quality design. R1 of the Borough Design Guide SPD requires that development contribute positively towards and be compatible with the historic or underlying character and quality of the local area.

17. The existing office building is an elongated structure on a narrow plot although it sits amongst a varied mix of building types and uses on an irregular arrangement of plot sizes and proportions. The most prominent of these features include a car wash building on the south western corner of the main intersection, the Sainsbury's supermarket opposite the subject site and a 9.5m high office building to the south and a warehouse on a backland site to the south with a large floorplate of 1340m².
18. Amongst these buildings, the existing building has a volume of 1665m³ and the proposed building will have a volume of 2540m³ – a 52% increase. The additional floor area comprises an infill extension to the south eastern corner or front of the building, a single storey, flat roof extension to the rear of the building and a new second floor level. However, it also includes a net reduction in bulk at the rear, with a lowering in the roof form.
19. The extensions and additions to the building would be visible within the street scene but it would still sit below the height of the office building to the south and well below the height of the Sainsbury's supermarket to the east. Views of the whole depth of the structure would be available between dwellings on Reading Road.
20. However, the majority of the extensions are at or near the rear and the proposed development provides for a more sympathetic building in the streetscene, with the additions comprising an attractive art deco form with a low roof pitch softening the overall bulk. Materials will match existing. The additional bulk and scale of the development is not excessive when taking account of the surrounding built form. At three storeys in height, it is compatible with the character of the area and suits its district centre and major development location.
21. The previous refusal on the site, which comprised an additional floor and depth was considered to be an overdevelopment of the site with a cramped appearance and would have a detrimental impact upon visual amenity and character of the area. The changes made under this application have resolved these concerns.

Density

22. Policy CP5 of the Core Strategy and Policy TB05 of the MDD Local Plan require an appropriate dwelling density and R10 of the Borough Design Guide SPD seeks to ensure that the development achieves an appropriate density in relation to local character.
23. The density measures 100 dwellings per hectare, which is not out of character with the wider area. There are several examples of flat buildings and aged care buildings within a 250m radius, including a 35 unit aged care facility at 405-411 Reading Road to the north, an 18 unit flat building at Hunters Court on Reading Road and a 21 unit Council owned housing development on Churchill Drive to the west. In this case, given the accessible district centre location, sustainable public transport connections and mix of uses in the surrounding area, the density is appropriate and entirely consistent with paragraphs 106 and 127 of the NPPF, which support higher densities in accessible locations.

Building line

24. R7 of the Borough Design Guide SPD requires a consistent building line relative to existing buildings. With the aforementioned mix of building types and plot arrangements, there is no real established building frontage to King Street Lane. Nonetheless, the front building line remains unchanged, which is acceptable in terms of R7. The rear setback measures 22m, which represents 31% of the site depth, which is also acceptable in terms of its relationship with surrounding properties, including the properties to the north fronting Reading Road, which are perpendicular to the subject site.

Height

25. R9 of the Borough Design Guide SPD note that height, bulk and massing should respond to the local context and the prevailing heights in the area.

26. In relation to height, paragraph 5 of the Inspector's decision for the previous scheme stated that '*the difference in height would be sufficiently substantial for the proposed development to appear overbearing and out of keeping in its immediate context. The existence of some larger buildings in the wider area would not negate this harm.*'

27. This application proposes to lower the overall height from 10.9m to 8.5m across its entire length (a 22% reduction), such that it will be 100mm above the height of the existing ridge. The rear third of the building is also proposed to step down to two storeys or 6.35m in height, which is 2.0m lower than the ridge of the existing office building. The proposed extension to the very rear is single storey with a flat roof, with a height of 3.4m. In the middle third of the building, the eaves are increasing in height from 5.6m to 7.4m front of the building but lowered by 200mm within the rear third.

28. Overall, the three storey height of the building is acceptable in terms of being consistent with surrounding development and appropriate for the context of the area. It is 1.0m below the ridge and 300mm above the eaves of the adjoining office building to the south, 500mm below the flat building at 400 Reading Road and significantly below the 10.9m height of the Sainsbury's supermarket to the east. It will not be overly apparent in views from King Street Lane because it is setback behind the side road running parallel to King Street Lane and is acceptable.

Outbuildings

29. R20 of the Borough Design Guide SPD requires consideration of external elements including the bin and cycle storage to avoid proliferation of clutter. The proposal initially proposed separate cycle and bin storage area at the front of the building, which had the potential to detract from the landscape setting of the site and impact upon the root system of the veteran Oak at the front of the site. Revised details have resolved this issue, with the waste storage relocated to the southern side boundary and the cycle storage at the rear. This achieves an appropriate streetscape presence.

Parking

30. P2 of the Borough Design Guide SPD ensures that parking is provided in a manner that is compatible with the local character. The existing site is mostly paved, including at the front of the site. The proposed development is considered to represent a net improvement in the provision of soft landscaping and the parking arrangement, where two spaces are provided at the front of the site and sufficient soft landscaping is provided at the rear, is seen as a suitable outcome for the site and appropriate for the area.

Materials and design

31. R11 of the Borough Design Guide SPD requires that housing ensure a coherent street character, including materials and colour. The materials will match existing where relevant, with the generous use of glazing and contrasting materials below the windows. On this basis, no objection is raised.

32. R6 of the Borough Design Guide SPD requires that building frontages provide surveillance and activation of the street. The proposal includes a front lobby to the front corner of the building and a large expanse of glazing to the front elevation, such that it is acceptable in terms of R6.

33. R19 and R23 of the Borough Design Guide SPD require a coherent and well-designed approach that influences and responds to the original building whilst paragraph 130 of the NPPF states that *'where the design of a development accords with clear expectations in plan policies, design should not be used by the decision-maker as a valid reason to object to development.'*

34. The existing office building has limited architectural relevance and the proposed alterations and additions will achieve an art deco form, with a curved corner frontage, a continuous bay window with a large expanse of glazing and an asymmetrical gable roof form to the front elevation. It is not typical of the immediate area but is nonetheless supported from a design and architectural perspective.

Dwelling Mix

35. Policy CP5 of the Core Strategy requires an appropriate mix of dwelling types, tenures and sizes so that the housing needs of the community are met. Policy TB05 of the MDD Local Plan requires an appropriate housing mix which reflects a balance between the underlying character of the area and both the current and projected needs of households.

36. The Berkshire (including South Bucks) Strategic Housing Market Assessment (February 2016) identified future housing need for the Wokingham Borough. Table 107 (on page 295) identifies the following:

No of beds	1 bed	2 bed	3 bed	4+ bed	Total
Dwellings required	934	3488	5605	2862	12889
	7.2%	27.1%	43.5%	22.2%	100%
Subject application	9	3	0	0	12
	75%	25%	0%	0%	100%

37. The intent of Council's policies are to provide a mix of accommodation to cater for the varied needs of the community and to ensure that it is provided where it is

needed. In this case, there is a significantly higher proportion of smaller units, which is contrary to the need for 3 and 4-bed units. However, paragraph 4.30 of the Core Strategy also states that '*Depending upon the character and needs of the area, it may not be necessary to include a mix of dwellings in every residential scheme, especially smaller ones.*'

38. In this case, it is in close proximity of Winnersh Train Station and bus services on Reading Road as well as being within walking distance of Winnersh Triangle Business Park, which is supportive of commuting workers and the provision of smaller units. The site is also constrained in terms of plot width, which limits the ability to provide larger dwellings and a lack of usable open space is not overly conducive to family units. As such, the provision of nine x 1-bed units and 3 x 2-bedroom units is not unreasonable. Furthermore, the issue of dwelling mix was not raised in the previous refused planning application or appeal proceedings (which included 15 x 1-bed units and 3 x 2-bed units) and it would be unreasonable to raise it now.

Housing Accessibility

39. Policy CP2 of the Core Strategy seeks to ensure that new development contributes to the provision of sustainable and inclusive communities, including for aged persons and the disabled. 10–20% of all dwellings should be to Lifetime Homes standards in accordance with Policy CP5 of the Core Strategy and Policy TB05 of the MDD Local Plan. In this case, it equates to 1-3 units.

40. The Design and Access Statement suggests that the units are adaptable or accessible. The open plan layout of the main living areas and the circulation space within the bathrooms and bedrooms is likely to allow for easy conversion for accessible use and the side pathway along the northern side of the building will allow for level access. Condition 10 requires further details in relation to ensuring that two of the units are easily adaptable for disabled use.

41. There is one disabled car space at the front of the site. Representing 8% of the total number of units within the building, it is an appropriate compromise for the site and is consistent with the 6% requirement specified by government guidance.

Housing Amenity

Internal amenity

42. Policy TB07 of the MDD and R17 of the SPD require adequate internal space to ensure the layout and size achieves good internal amenity. In accordance with the Technical housing standards – nationally described space standard, a minimum standard of 50m² applies for 1-bed/2-person units and 61m² for 2-bed/3-person units.

Unit	Bedrooms	Occupancy	Area	Required	Complies
1	1	2	49.6m ²	50m ²	No
2	1	2	50.0m ²	50m ²	Yes
3	1	2	49.4m ²	50m ²	No
4	2	3	78.1m ²	61m ²	Yes
5	2	3	62.7m ²	61m ²	Yes
6	2	3	78.1m ²	61m ²	Yes

7	1	2	49.4m ²	50m ²	No
8	1	2	50.0m ²	50m ²	Yes
9	1	2	49.6m ²	50m ²	No
10	1	2	49.6m ²	50m ²	No
11	1	2	50.0m ²	50m ²	Yes
12	1	2	49.4m ²	50m ²	No

43. Six of the units achieve compliance with the minimum standard and the remaining six units are near compliant or within a 1% margin of error. In terms of providing for sufficient internal amenity, the proposal is acceptable.

44. A dwelling with more than one bedroom should have a main bedroom, which is to have a minimum area of 11.5m² and minimum width of 2.75m. Secondary or single bedrooms should have a minimum area of 7.5m² and width of 2.15m, Living spaces should have a minimum area of 23-25m² and there should also be provision for storage.

House	Bedrooms	Occupancy	Main bedroom	Secondary bedroom	Living Space	Storage
1	1	2	11.9m ² /2.15m	N/A	24m ²	Yes
2	1	2	11.9m ² /2.8m	N/A	24m ²	Yes
3	1	2	11.9m ² /2.8m	N/A	23m ²	Yes
4	2	3	14.1m ² /2.6m	8.6m ² /2.3m	37m ²	Yes
5	2	3	11.5m ² /3.0m	8.0m ² /3.0m	27m ²	Yes
6	2	3	14.1m ² /2.6m	8.8m ² /2.3m	37m ²	Yes
7	1	2	11.8m ² /2.8m	N/A	23m ²	Yes
8	1	2	11.8m ² /2.8m	N/A	24m ²	Yes
9	1	2	11.9m ² /2.15m	N/A	24m ²	Yes
10	1	2	11.9m ² /2.15m	N/A	24m ²	Yes
11	1	2	11.8m ² /2.8m	N/A	24m ²	Yes
12	1	2	11.8m ² /2.8m	N/A	23m ²	Yes

45. All of the bedrooms meet the minimum area standard but the three units at the front of the building (Units 1, 9 and 10) have narrow bedrooms. On balance, there is no objection raised because of the sufficient length of the rooms and compliant total area and because it relates to a single bed unit with an adequate main living space. Main living spaces vary between 23m² and 37m², which is largely consistent with the minimum requirement.

46. R18 of the SPD requires sufficient sunlight and daylight to new properties, with dwellings afforded a reasonable dual outlook and southern aspect. This is reiterated in Paragraphs 127 and 130 of the NPPF, which seek to promote development that has good architecture and layout with a high standard of amenity for existing and future users and Paragraphs 150 and 153 of the NPPF state that new development to should take account of landform, layout, building orientation, massing and landscaping to minimise energy consumption.

House	Orientation	Elevations	Outlook	Ceiling Height
1	SE	2	Front garden	2.5m
2	SW	1	Driveway	2.5m
3	SW	2	Driveway	2.5m
4	SW	2	Driveway	2.5m
5	SW	3	Rear garden	2.5m
6	SW	1	Elevated	2.3m
7	SW	2	Elevated	2.3m
8	SW	1	Elevated	2.3m

9	SE	2	Front garden	2.3m
10	SE	2	Front garden	2.3m
11	SW	1	Elevated	2.3m
12	SW	2	Rear roof	2.3m

47. The development is constrained in some respects, partly because the proposal aims to retrofit an existing building. However, overall, there is a satisfactory level of amenity.

48. The outlook for Units 2-4 on the ground floor are onto the side driveway. The Inspector addresses this at paragraph 13 of the previous appeal decision:

'The outlook from windows in the proposed development would, on one side, be onto the adjoining office building and its car park. However, that would not result in so unattractive a view as to cause unacceptable living conditions. Ground floor windows looking out in this direction would give views directly onto the proposed access road to the rear car park. However, such views are not uncommon in residential schemes and would not result in unacceptable living conditions.'

49. Paragraph 13 continues by noting that other the units look *'onto the petrol filling station, the proximity of the side elevation to the site boundary has the potential to result in a severely reduced outlook from ground floor flats to a degree which would make them unpleasant live in'*. However, the revised scheme has eliminated all single aspect units along this elevation such that no issue is raised. Indeed, windows to the north eastern elevation are openable above 1.7m, which allows cross ventilation and light from this elevation, which has measurable benefits for these units.

50. Four of the units are single aspect, which is 33% of the total units, which is generally acceptable. Moreover, each of the units are afforded a southern aspect with good sized window openings and there is adequate ceiling height within each unit.

51. On this basis of the above, no objection is raised.

External amenity

52. R16 of the Borough Design Guide SPD stipulates that each unit should have amenity space and it should retain and protect privacy, benefit from sunlight and be able to accommodate 2–4 chairs and a small table. Aside from private amenity space for Unit 1 at the front of the ground, there is no private amenity for the remainder of the development, including any external balcony areas. The proposal instead relies upon a communal garden area at the rear of the site. It has a total area of 90m².

53. The minimum depth for rear gardens is 11m and it should receive direct sunlight and be capable of accommodating play, clothes drying and storage.

54. Paragraph 14 of the previous appeal decision addresses the issue of communal amenity space although in that scheme, it involved a roof garden at the rear of the second floor:

'Whilst I note the Council's concern on the amount of space provided, I have been provided with no clear documentation of any specific standard adopted by the Council and it seems to me that the elevated position of the open space would provide quite an attractive area. In the circumstances it is not been shown that there is an inadequacy of open space provision that should stand against the proposal.'

55. The Inspector is correct in noting that there is no specific standard for communal open space. In the case of the subject application, the provision of 90m² of open space to be shared by 11 units (or 14 bedrooms) is generally acceptable. Being at ground level, it is more accessible and more likely to be utilised than the elevated roof terrace and it is supported by soft landscaping and tree coverage. On balance and when considering the opinion of the Inspector in the previous appeal decision, the amenity space provided is satisfactory and no objection is raised.

Acoustic amenity

56. Policy CC06 and Appendix 1 of the MDD Local Plan requires that development protect noise sensitive receptors from noise impact. The units are well designed in terms of grouping or stacking bedrooms and high noise sources (such as kitchens and living areas) within units. This will minimise the potential for noise disturbance between floors and walls of units and reduce the reliance upon insulation required by the Building Regulations.

57. The bedrooms open onto the side driveway or the front parking area and the site is opposite the intersection of Reading Road and King Street Lane, which is busy throughout the day. However, it is not sufficient to warrant concern on acoustic grounds given the low level of traffic movements within the site and as the building will be subject to acoustic measures within the Building Regulations.

Residential Amenities

Overlooking

58. R15 of the Borough Design Guide SPD requires the retention of reasonable levels of visual privacy to habitable rooms, with separation of 22m to the rear (increasing to 30m on the second floor) and 10m to the street (increasing to 15m on the second floor). R23 notes that the side walls must not contain windows, especially at first floor level.

59. In paragraph 9 of the appeal decision for the previous refusal, the Inspector noted that *'the rear extension of the proposed development would also give rise to 3 bedroom windows facing out onto the garden of No. 400. Given their proximity to the garden this would result in an unacceptable loss of privacy to the occupants of that property.'*

60. The issue of overlooking is now resolved with the provision of 1.7m sill heights to the northern side elevation, the inclusion of only one above ground window to the rear elevation (being a lobby) and a significant reduction in the depth of the second floor extension.

61. There is 40m separation to the carpark to the east, a carwash (with a large expanse of parking) to the north and two non-residential properties to the south, which

include large areas of servicing and parking. The only residential properties are at 400-406 Reading Road to the north and west of the site. The proposal will retain an adequate level of visual privacy to these properties, with separation to the rear boundary of 22m at ground level, 30m at first floor level and 40m at second floor level. To the northern side boundary, ground floor windows will look onto the existing boundary fence and obscure glazing is proposed to all above ground windows.

Sunlight and Daylight

62. R18 of the Borough Design Guide SPD aims to protect sunlight and daylight to existing properties, with no material impact on levels of daylight in the habitable rooms of adjoining properties. Habitable windows of adjoining properties must not be obstructed by a 25 degree angle.
63. In paragraph 10 of the appeal decision for the previous refused application, the Inspector noted that *'there is a potential for an unacceptable loss of light to the garden of that property as a result of the added height to the building. I consider this to be a legitimate concern and the appellant has provided no technical evidence to show that the development would be acceptable in this respect.'*
64. Despite no technical information being provided as part of this application, the issue of loss of light and shadowing is now resolved. The modest increase in ridge height and the lowering of the building at the rear third of the building, where it corresponds with the rear boundary of 400 Reading Road to the north, will ensure that there is no adverse reduction in daylight to the rear elevation of the adjoining flat building and the adequate retention of sunlight through the afternoon. It accords with the requirement to not affect any windows within the 25 degree line and if anything, the lowering of the building at the rear will improve the penetration of light and direct sunlight, including to the rear gardens of 400-404 Reading Road. On this basis, it is acceptable.

Overbearing and Sense of Enclosure

65. R16 of the Borough Design Guide SPD requires separation distances of 1.0m to the side boundary and 11m to the rear boundary.
66. In paragraph 8 of the appeal decision for the previous refused application, the Inspector noted that *'where it abuts the rear garden of flats at No. 400 Reading Road it would for half the width of that garden be 3 stories high to eaves height. In addition there would be a 2-storey flat roof rear extension similarly close to the boundary and extending across the remaining width of the garden at No. 400. This would be taller than the eaves height of the existing building. Development of this height and proximity to the rear garden boundary of No. 400 would appear unacceptably overbearing when seen from that property.'*
67. The issue of dominance is resolved with the deletion of the third floor, the reduction in the depth of the second floor and by limiting the rear extension to single storey only. More generally, a side setback of 1.0m is maintained to the northern side boundary and 5.0m to the southern side boundary, which matches the existing boundary setbacks. A 22m rear setback is also achieved. Despite an increase in the eaves height on the boundary of 1.8m to 7.4m, this occurs alongside the car wash,

with a lower eaves height of 5.2m adjacent to the boundary with 400 Reading Road and 3.4m alongside 400 and 402 Reading Road, which is entirely reasonable

Noise disturbance

68. Policy CC06 and Appendix 1 of the MDD Local Plan requires that development protect noise sensitive receptors from noise impact. There are no adverse noise implications arising from the proposed development, particularly given the high level of background noise from traffic on Reading Road.

Light Spill

69. There are no light spill concerns. There are limited window openings along the northern side of the building and it adjoins non-residential uses on its southern side. Traffic movements along the driveway will not affect any habitable windows within the development or across the side boundary.

Dust and air pollution

70. The area is hard paved such that there are no adverse dust issues. The carpark at the rear of the site is hard against the rear boundary with 404 and 406 Reading Road but in the context of the surrounding highly trafficked areas of Reading Road and King Street Lane (including idling cars at the intersection) as well as the non-residential uses and the expansive Sainsbury's car park, there are no objections on air pollution grounds. More locally, there is at least 12m separation to the nearest dwelling and the level of traffic movements is not sufficient to warrant concern and likely to be sufficiently different from the existing office use. As an additional safeguard, however, Condition 8 is imposed to ensure that signage is installed within the car park to direct residents to park front into the spaces on the northern side of the site.

Access and Movement

Car Parking

71. Policy CC07 and Appendix 2 of the MDD Local Plan stipulates minimum off street parking standards, including provision for charging facilities.

72. The proposal makes provision for 12 car spaces, with two spaces at the front of the site and ten spaces at the rear. The car spaces measure 2.5m x 5.0m which meets the guidelines in the Borough Design Guide.

73. A 12 unit development will generate a requirement for 12 spaces where each space is unallocated. Subject to the spaces being unallocated (with the exception of the one disabled car space) and a car parking management plan being submitted (see Condition 8), there is no objection to the application on parking grounds, particularly given the excellent public transport connections.

74. Charging facilities are outlined in Condition 7.

Cycle Parking

75. Policy CC07 and Appendix 2 of the MDD Local Plan stipulates minimum cycle parking standards and Policies P2 and P3 of the Borough Design Guide SPD ensure that it is conveniently located, secure and undercover and provided where it is compatible in the streetscene.
76. A total of 12 spaces are required and space has been provided alongside the car parking at the rear of the site. This is satisfactory in terms of providing safe and secure storage and whilst it is not the most accessible location for all unit owners, it is the most appropriate location when balancing the constraints of the site. No specific details have been provided and as such, these are required by Condition 7.
77. Two PTW (motorcycle) spaces are proposed adjacent to the main entrance. This exceeds the requirement for one space per 20 car parking spaces as required by Appendix 2 of the MDD Local Plan. However, it is an efficient use of space in an accessible location and complements the compliant parking at the front and rear of the site.

Access

78. The proposal relies upon the existing street entrance, which is acceptable in terms of two way access and visibility. However, the maximum driveway width is 3.6m, which is not sufficient for two cars to pass. Given the length of the single lane is at least 55m long and there is a lack of visibility around the building and because the driveway is shared with 4 King Street Lane, there is the potential for vehicle conflict throughout the day. However, the proposal also includes a net reduction in parking spaces as part of the change of use and works to the rear of the site. As such, there is a reduction in traffic movements along the driveway. On balance, as part of a wider scope of pre commencement information in Condition 7, further details of how this will be managed will be required. This could include the installation of lights and the provision of vehicle waiting areas.
79. Gates are shown on the elevation drawings and it would appear that they are the same as the existing gates on the site. There is sufficient waiting space in front of the gates at the top of the driveway. However, in the absence of any further detail, including whether they will be mechanically operated, Condition 8 requires further information.
80. Pedestrian access is via the northern side of the building but it is somewhat unappealing because of its narrowness. In this respect, there is also shared pedestrian access along the side driveway. There is generally no objection to this arrangement because of there is sufficient width to allow vehicles to pass without impeding pedestrian access. However, further details of how this area will be delineated is required by Condition 7.
81. Elsewhere on the site, there is unimpeded pedestrian access to the bin and cycle storage areas and to the amenity space at the rear, which is satisfactory.

Traffic Generation

82. The Council's Highways Officer notes that it is unlikely that the proposal would have an adverse impact on the highway network. However, due to the location at a busy

intersection and the limitations posed by the existing site, a Construction Management Plan is required by Condition 6.

Landscape and Trees

83. Policy CC03 of the MDD Local Plan aims to protect green infrastructure networks, promote linkages between public open space and the countryside, retain existing trees and establish appropriate landscaping and Policy TB21 requires consideration of the landscape character. R14 of the Borough Design Guide SPD requires well-designed hard and soft landscaping that complements housing.
84. The plot size is narrow and there is a small parking area to the rear, the size of which will be reduced with the extension to the rear. However, there is a net increase in soft landscaping and opportunities for tree planting across the site, including at site boundaries and at the street frontage. There is a communal garden at the rear of the ground floor, which is generally acceptable, subject to additional details relating to planting to complement the space and soften the building bulk.
85. On this basis, and subject to further details in Conditions 3 and 4, it represents a positive outcome for the streetscene and adequate amenity for future occupiers. On this point, in paragraph 8 of the appeal decision for the previous refused application, the Inspector noted that '*although only limited landscaping would be provided within the site fairly substantial roadside verges and planting would result in a reasonably well landscape setting to the development.*'
86. There is some landscaping to the street boundary which will retain some screening and landscape setting to the streetscene. There are some minor groundworks at the front of the site associated with pedestrian access and a veteran Oak on the Council verge remains in a susceptible position immediately alongside the existing vehicular entrance. Condition 4 requires arboricultural information and Condition 7 requires additional access details, which combined will ensure its protection.

Ecology

87. Policy TB23 of the MDD Local Plan requires the incorporation of new biodiversity features, buffers between habitats and species of importance and integration with the wider green infrastructure network. Given the densely populated and high trafficked location and expanse of hard surfacing across the site, there are no ecological issues with the proposal.

Sustainable Design/Construction

88. Policy CC04 of the MDD Local Plan and the Sustainable Design and Construction SPD require sustainable design and conservation and R21 of the Borough Design Guide SPD requires that new development contribute to environmental sustainability and the mitigation of climate change.
89. The proposal involves the retrofitting of an existing building, which is supportive of reuse principles. An Energy Statement was submitted as part of the application. It refers to building fabric thermal details, A rated boilers and energy efficient lighting and appliances, in terms of its compliance with Part L of the Building Regulations. The proposal is considered to be acceptable in terms of Policy CC04 and R21.

90. The Energy Statement also notes the use of a photovoltaic system on the south eastern roof plane, generating 10.5% of the energy requirements for the development. This meets the requirements of Policy CC05 of the MDD Local Plan, which encourages renewable energy and decentralised energy networks, with encouragement of decentralised energy systems and a minimum 10% reduction in carbon emissions. Compliance with this requirement would be applied by Condition 11.

Boundary Treatments

91. R12 of the Borough Design Guide SPD states that boundary treatments contribute positively to the character of the area. The Council's Trees Officer has objected to the application because of a lack of hedgerow to the property boundaries. However, where the site is located within a densely populated area and is surrounded by several non-residential uses and because of the minimal side boundary setback on its northern boundary, there is limited opportunity to provide such landscaping. Where opportunities exist, the site plan shows hedges mostly to the rear of the site and subject to the provision of additional landscaping details, this is a satisfactory outcome and not worthy of refusal of the application.

92. Vehicular gates are shown on the elevation drawings, which appear to be the existing gates. They are setback behind the front building line and are not obtrusive in the streetscene.

Waste Storage

93. Policy CC04 of the MDD Local Plan requires adequate internal and external storage for the segregation of waste and recycling as well as provision for green waste and composting and an appropriate area for ease of collection. Waste storage facilities have been noted at the front of the site to the southern side of the driveway, which is acceptable in terms of providing access for residents and for ease of collection by Council's refuse vehicles. Notwithstanding this, further details will be required to ensure that the area is adequately sized to accommodate general waste, recycling and green waste bins for 12 units and that it fits within the streetscape. This is outlined in Condition 9.

Flooding

94. Policy CC09 of the MDD Local Plan requires consideration of flood risk from historic flooding. The site and access thereto is located within Flood Zone 1 and the proposal represents no additional flood risk or vulnerability. It is therefore acceptable in terms of Policy CC09.

Drainage

95. Policy CC10 of the MDD Local Plan requires sustainable drainage methods and the minimisation of surface water flow. The Council's Drainage Engineer has reviewed the proposal and raises no objection, subject to details of the drainage system via a pre commencement Condition 5. It is therefore acceptable in terms of Policy CC10.

96. R23 of the Borough Design Guide SPD notes that parking spaces in front gardens must be paved with permeable surfaces to avoid any increase in surface water run-off and should include for soft landscaping. This is required by Condition 16.

Housing Affordability

97. Policy CP5 of the Core Strategy, Policy TB05 of the MDD Local Plan and the Affordable Housing SPD specify an affordable housing rate of 20% for any development involving five dwellings or more or where it is located on land with a total area of 0.16 hectares or more.

98. Whilst there is prior approval from 2013 for the conversion of the office building to create eight residential units, this consent has not been enacted and Class O of the GPDO requires that the conversion works need to be completed within three years. This has not occurred and the Inspector's decision from the 2017 appeal noted that there was no evidence of works commencing. As such, the proposal represents a net increase of 12 residential dwellings on a 0.12 hectare site and this equates to a requirement for 2.4 affordable units.

99. The only practical means of delivery for the affordable housing is through a commuted sum. This is because of the nature of the scheme (being an office conversion) and potentially high service charges and management constraints associated with a small affordable housing scheme. Based on the Viability Study undertaken by Levvel Ltd, the Council's approach to calculating commuted sums for affordable housing is based on the difference in the residual development value of a scheme without on-site affordable housing and the same scheme with on-site affordable housing. The commuted sum sought in-lieu of 2.4 units is £140,393.50.

100. The applicant submitted a Viability Assessment dated February 2019 arguing that the scheme was not viable. A review was undertaken by the Council's consultant, where the following discrepancies were noted:

- At £411.37, the gross development value was under-estimated even when accounting for new build uplift. The more appropriate figure used by the Council's consultant was £450
- The build costs were over-estimated at £1,369,386, with a more appropriate figure of £1,236,052 used instead
- At £725,000, the benchmark land value is over estimated (because the quality of the internal fit-out was of poorer quality to the examples cited and there is a lack of demand for office accommodation) and even when accounting for a developer incentive uplift and a more appropriate figure is £578,963
- At £104,317, CIL has been under-estimated, with the Council adopting a figure of £111,436
- Capitalised ground rents (£300-£350 per annum), a build cost contingency (7.5%), professional fees (10%) and sales and legal fees (£57,703) are all reasonable
- The finance rate (7%) was accepted and the timescales (9 month construction and total of 18 months)

101. The accepted developer's profit is acknowledged at 17.5% and when taking account of the above findings, the review undertaken by the Council's consultant concluded that the proposal would bring about a profit of 17.58%, with the excess 0.08% equating to a total affordable housing contribution of £2,504. The appellant is agreeable to making this contribution and this forms part of a legal agreement as noted in Informative 1.

102. It is acknowledged that the applicant chose to accept a commuted sum of £425,440 within the appeal proceedings for the previous refused planning application but that was in relation to a larger 18 unit scheme and where no viability assessment was undertaken. As such, this is of no consequence to this application.

Community Infrastructure Levy

103. The application is liable for CIL payments because it involves additional floor area in excess of 100m². It is payable at £365/m² index linked.

Employment Skills

104. Policy TB12 of the MDD Local Plan requires an employment skills plan (ESP) for this development. ESPs use the Construction Industry Training Board (CITB) benchmark based on the value of construction. The Council's Policy Officer has reviewed the proposal and notes that the cost of works would not trigger a requirement for an ESP.

CONCLUSION

The proposed development represents a measured infill development that reuses an existing brownfield building. It is acceptable in terms of visual and neighbor amenity and conditions have been applied to address parking, access and tree protection issues. The social, economic and environmental benefits that arise from additional housing in a highly sustainable location outweigh the minimal harm posed to the immediate area. The application is therefore recommended for conditional approval.

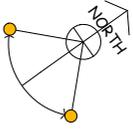
This page is intentionally left blank

PLANNING REF : 180753
PROPERTY ADDRESS : Winnersh Community Centre
: New Road, Sindlesham, Wokingham
: RG41 5DX
SUBMITTED BY : Winnersh Parish Council
DATE SUBMITTED : 07/03/2019

COMMENTS:

There is insufficient parking and no provision for visitor parking.

This page is intentionally left blank



scale:- 1:1250@A4



scale bar:- 1:1250

177

TRIDENT HOUSE, KING STREET LANE.
WINNERSH, WOKINGHAM. RG41 5AS - EXISTING SITE LOCATION PLAN:- TRIDENT/PL01



Architectural Design Services

Mr S Saxena
Suite 462
5, Spur Road, Isleworth,
Middlesex.
TW7 5BD
email:- sundeep.saxena@yahoo.co.uk



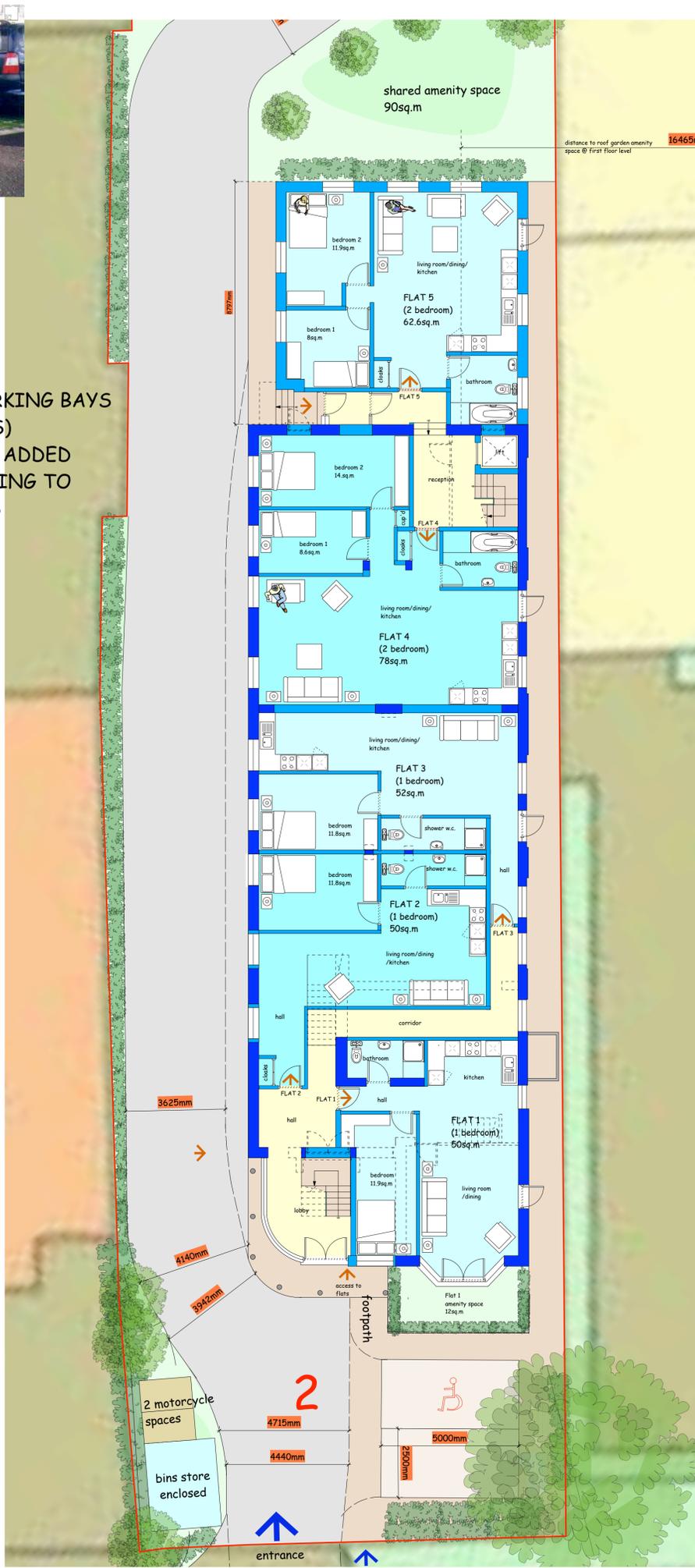
This page is intentionally left blank

NOTE: No dimensions are to be scaled from this drawing. Report all errors and omissions to the Architect. All dimensions to be checked on site before fabrication.

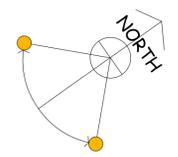


PROPOSED SITE / GROUND PLAN
scale:- 1:200@A1

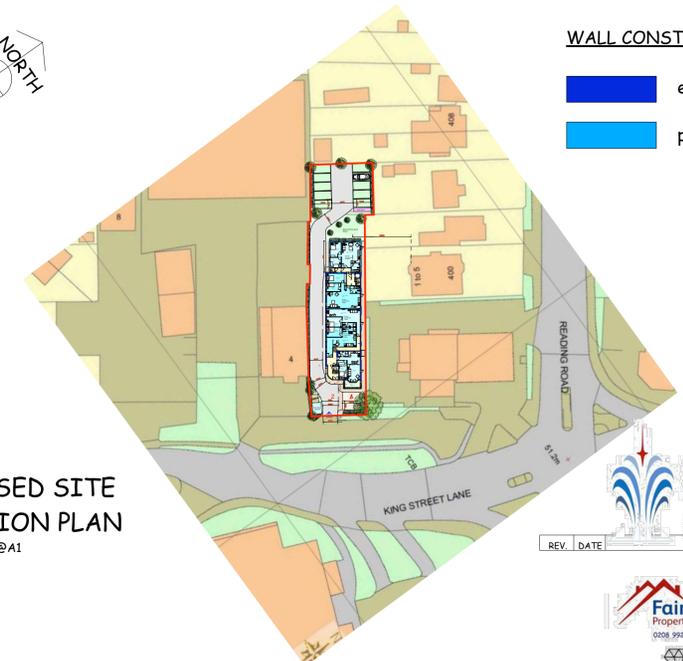
TRIDENT HOUSE, KING STREET LANE. WINNERSH, WOKINGHAM. RG41 5AS
INFORMATION AS PROPOSED 12 FLATS
SAXENA DESIGN - www.sundeepsaxena.com



PROPOSED GROUND FLOOR PLAN
scale:- 1:100@A1

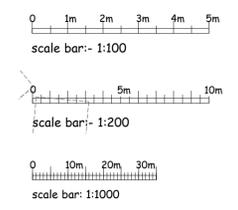


PROPOSED SITE LOCATION PLAN
scale:- 1:1000@A1



WALL CONSTRUCTION

- existing wall
- proposed wall



REV.	DATE	DESCRIPTION

Fairdeal Properties
0208 992 9350

THE 8TH AVENUE
0208 5044 888

Sit saxum firmum

SAXENA DESIGN
INTERNATIONAL ARCHITECTURE & DESIGN CONSULTANTS

Mr. S. Saxena
Suite 402
5 Spire House, Silverthorn,
London, W17 4BP.
www.sundeepsaxena.com

Saxena

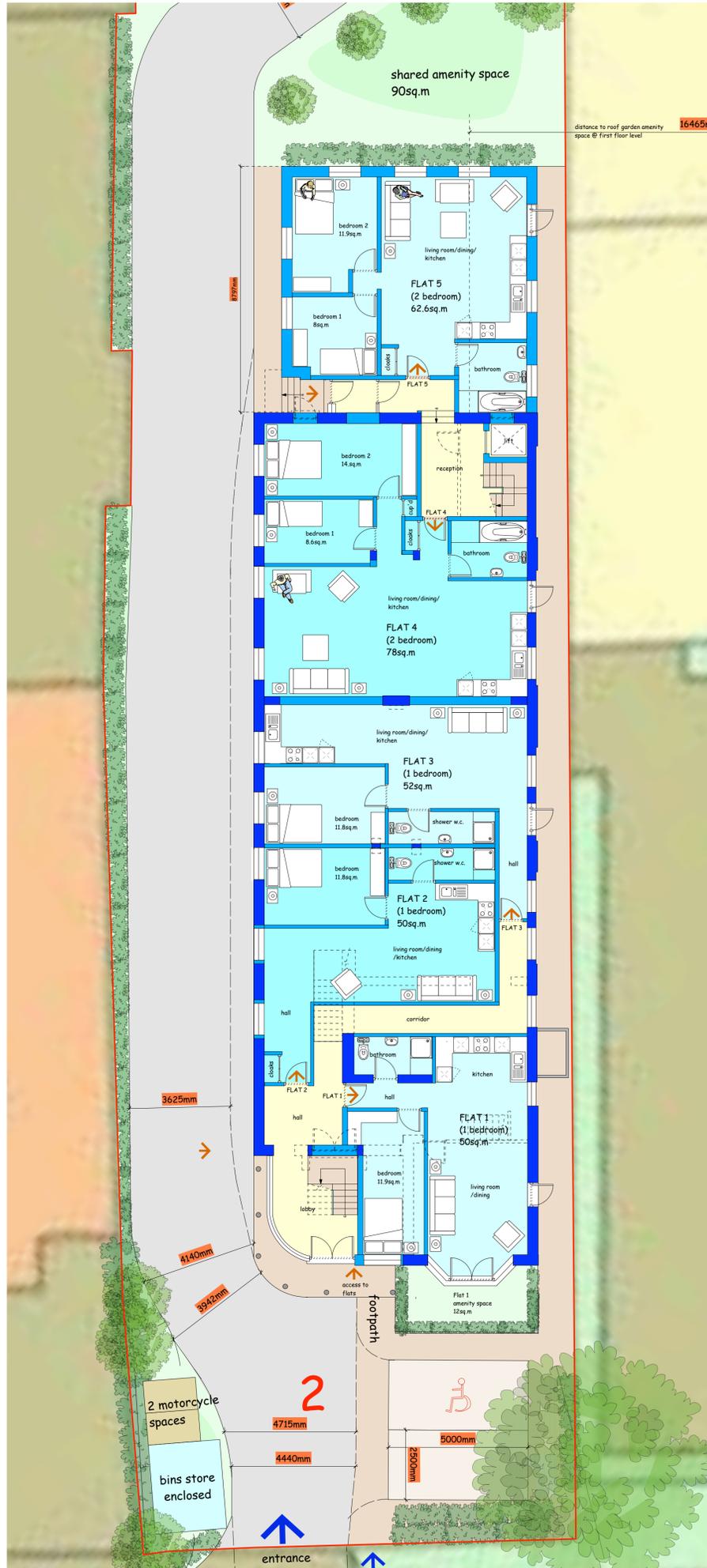
PUNJAB SKIPS
0208 571 4040

TR GROUP
07889 121 518

iDream Developments
0208 045 0533
MASTER BUILDING CONTRACTORS

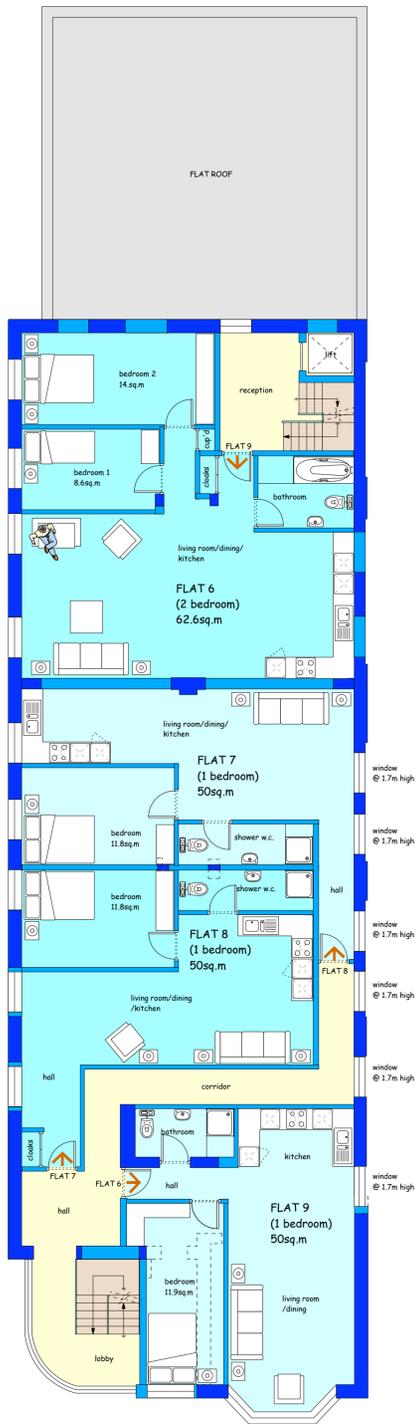
PROJECT TRIDENT HOUSE, KING STREET LANE, WINNERSH, WOKINGHAM, RG41 5AS	
DRAWING PROPOSED FLATS	
CLIENT	DATE
SCALE 1:100/1:200/1:1000 @ A1	APRIL/2019
DRAWING No TRIDENT/PL07L	
© copyright retained	

This page is intentionally left blank

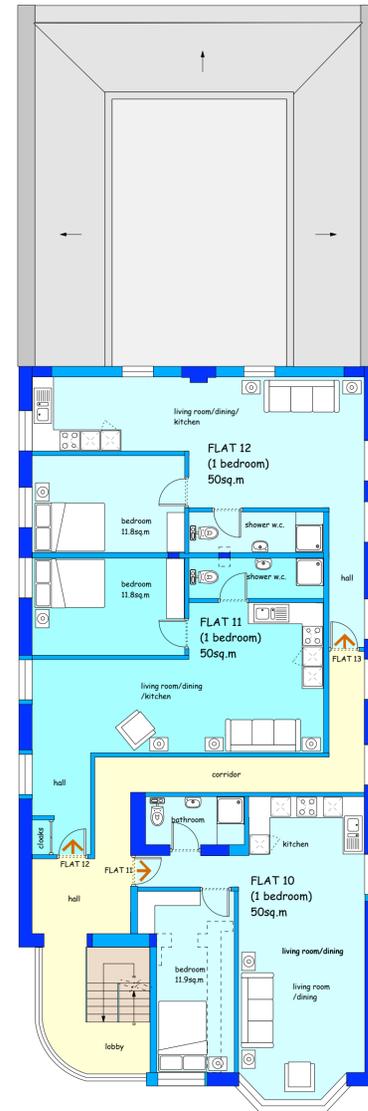


PROPOSED GROUND FLOOR PLAN
scale:- 1:100@A1

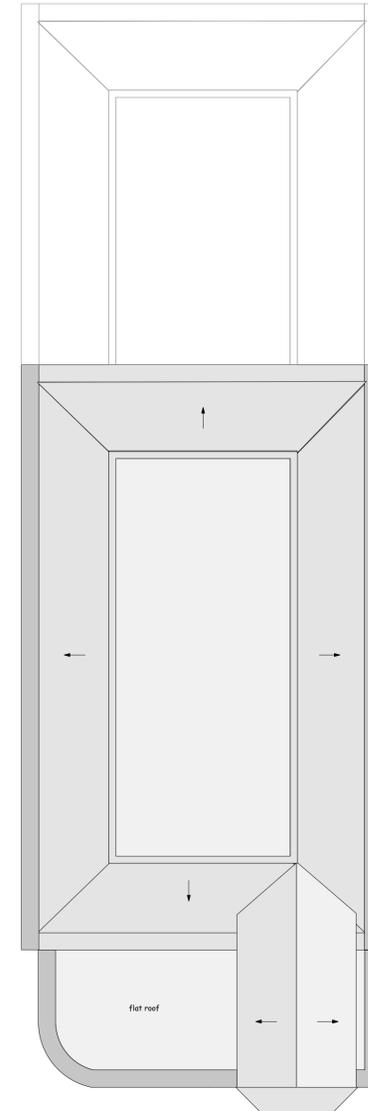
Site Area:- (approx) - 1,124.m/12,099sq.ft 0.277 ACRES



PROPOSED FIRST FLOOR PLAN
scale:- 1:100@A1



PROPOSED SECOND FLOOR PLAN
scale:- 1:100@A1

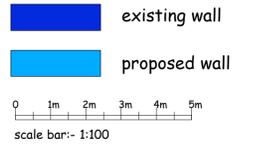


PROPOSED ROOF
scale:- 1:100@A1

FROSTED WINDOWS AND DOORS TO THIS ELEVATION

NOTE: No dimensions are to be scaled from this drawing. Report all errors and omissions to the architect. All dimensions to be checked on site before fabrication.

WALL CONSTRUCTION



Ground Floor

- FLAT 1 - 50sq.m - 1 bedroom
- FLAT 2 - 50sq.m - 1 bedroom
- FLAT 3 - 52sq.m - 1 bedroom
- FLAT 4 - 78sq.m - 2 bedroom
- FLAT 5 - 62.6sq.m - 2 bedroom

First Floor

- FLAT 6 - 62.6sq.m - 2 bedroom
- FLAT 7 - 50sq.m - 1 bedroom
- FLAT 8 - 50sq.m - 1 bedroom
- FLAT 9 - 50sq.m - 1 bedroom

Second Floor

- FLAT 10 - 50sq.m - 1 bedroom
- FLAT 11 - 50sq.m - 1 bedroom
- FLAT 12 - 52sq.m - 1 bedroom

12 FLATS TO SITE :- 657.2sq.m/7,074sq.ft

parking onsite:- 14 bays (unallocated)
1 bay per unit with 2 visitor bays

amenity space onsite:- 122sq.m
(10sq.m per unit)

refuse:- onsite
cycle store:- onsite



REV.	DATE	DESCRIPTION

Fairdeal Properties
0208 992 9100

THE 8TH AVENUE
0208 2044 888

Saxena Design
Mr. S. Saxena
Suite 402
5 Spar Road, Winnersh,
Wokingham, RG41 5AS
saxena@sundeepsaxena.com
www.sundeepsaxena.com

PUNJAB SKIPS
0208 871 4040

TR GROUP
07889 121 518

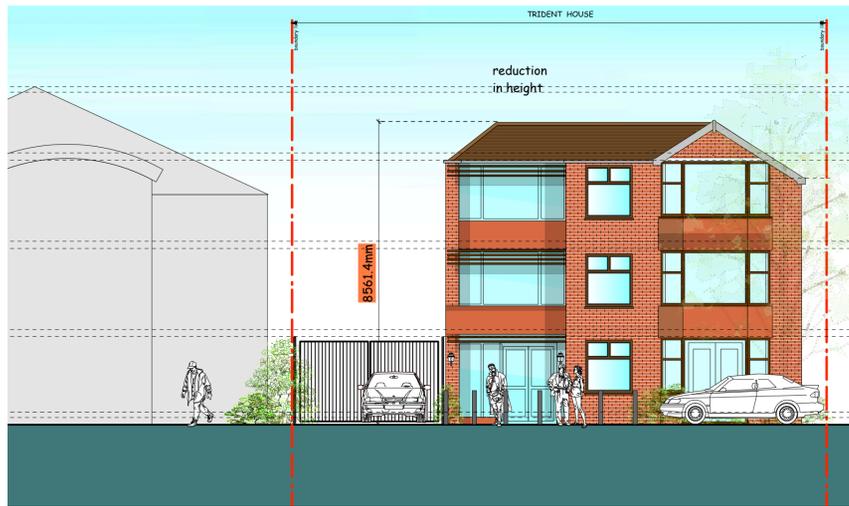
iDream Developments
0208 045 0833
MASTER BUILDING CONTRACTORS

TRIDENT HOUSE, KING STREET LANE. WINNERSH, WOKINGHAM. RG41 5AS
INFORMATION AS PROPOSED 12 FLATS / 14 PARKING BAYS WITH COMMUNAL AMENITY
SAXENA DESIGN - www.sundeepsaxena.com

PROJECT TRIDENT HOUSE, KING STREET LANE. WINNERSH, WOKINGHAM. RG41 5AS	
DRAWING PROPOSED PLANS	
CLIENT	
SCALE 1:100 @ A1	DATE APRIL/2019
DRAWING No TRIDENT/PL20H	
© copyright retained	

This page is intentionally left blank

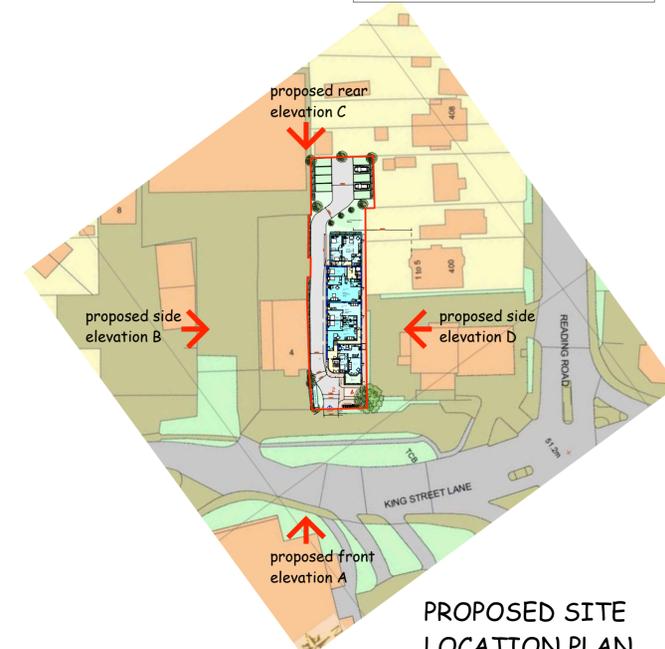
NOTE: No dimensions are to be scaled from this drawing. Report all errors and omissions to the Architect. All dimensions to be checked on site before fabrication.



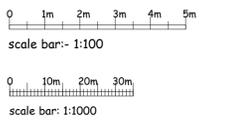
PROPOSED FRONT ELEVATION A
scale:- 1:100@A1



PROPOSED REAR ELEVATION C
scale:- 1:100@A1



PROPOSED SITE LOCATION PLAN
scale:- 1:1000@A1



PROPOSED SIDE ELEVATION B
scale:- 1:100@A1



PROPOSED SIDE ELEVATION D
scale:- 1:100@A1



DATE	DESCRIPTION



THE 8TH AVENUE
International Architecture & Design Contest Finalist 2018 S. Saxena



Saxena



PROJECT TRIDENT HOUSE, KING STREET LANE, WINNERSH, WOKINGHAM, RG41 5AS	
DRAWING PROPOSED ELEVATIONS	
CLIENT	
SCALE 1:100/1:1000 @ A1	DATE APRI./2019
DRAWING No TRIDENT/PL21F	
© copyright retained	

This page is intentionally left blank

Agenda Item 92.

Application Number	Expiry Date	Parish	Ward
190673	9 May 2019	Wokingham Town	Wescott

Applicant	Mr Norman Patterson
Site Address	Luckley House School, Luckley Road, Wokingham RG40 3EU
Proposal	Full application for the proposed construction of a reduced size multi-use synthetic turf sports pitch with a 3m-4.5m high fence and 6no 12m column floodlights
Type	Full
PS Category	18
Officer	Simon Taylor
Reason for determination by committee	Listed by Councillor Cllr Julian Sumner (Ward Member for Westcott)

FOR CONSIDERATION BY	Planning Committee on Wednesday, 8 May 2019
REPORT PREPARED BY	Assistant Director – Place

SUMMARY
<p>Luckley House School is a coeducational independent secondary school with day and boarding pupils from 11-18 years old. The proposal seeks to install a multi-use games area (MUGA) at the southern end of the campus. It will be constructed with a synthetic turf playing surface and dimensions of 80m x 50m and located within the existing turf playing fields and multi-purpose tennis and netball courts. It is to be used as an undersized hockey and soccer field but will also accommodate tennis courts. It will also comprise 3.0m fencing (with a rebounding surface) around the perimeter of the field and six x 12m high floodlights enabling morning and evening use to 9pm (and 8pm on weekends and bank holidays). It is intended to be made available for use by the public.</p> <p>The minimum residential separation is to 51m to properties in Denby Close to the west and approximately 75m to properties in Luckley Wood to the east (although these properties are separated by tree coverage), and these are the two areas of properties where most of the 59 objections against the proposal have been received.</p> <p>The primary issues relate to noise disturbance and light spillage to surrounding residents. Unacceptable impacts upon traffic and harm to existing wildlife and habitat were also noted in numerous submissions.</p> <p>The proposal was accompanied by a Noise Report, Lighting Strategy and Ecology Report and these have been reviewed by the Council's Environment Health Officer, Trees Officer and Ecology Officer. Sport England were also consulted in relation to the provision of sports facilities.</p> <p>On balance, the proposal is acceptable, subject to conditions relating to drainage (Condition 3), a Construction Management Plan (Condition 4), Maintenance Scheme (Condition 5), Noise management scheme (Condition 6), biodiversity measures (Condition 9), limiting the use until 9pm on weekdays (and 8pm on weekends) and the lighting to earlier times (Condition 12), protection of trees (Condition 14) and a Community Use Agreement (Condition 15).</p>

PLANNING STATUS

- Major development location
- Settlement edge (southern boundary)
- Greenway Route (along Luckley Road)
- Tree Preservation Orders 1564-2017 (to southern boundary and woodland on eastern edge of the site)
- 7 x Veteran trees interspersed across the campus
- Priority habitat (Lowland mixed deciduous woodland)
- Bat consultation zone
- Grade II listed building (Luckley House)
- Thames Basin Heaths Special Protection Area (5km zone)
- Nitrate vulnerable zone (surface water)
- Flood zone 1
- South East Water consultation zone
- Farnborough Aerodrome consultation zone
- Wind turbine safeguarding zone
- Minerals consultation zone
- Sand and gravel extraction consultation zone
- Non-classified road

RECOMMENDATION

That the committee authorise the GRANT OF PLANNING PERMISSION subject to the following conditions and informatives:

Conditions

1. Timescale

The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason: In pursuance of s.91 of the Town and Country Planning Act 1990 (as amended by s.51 of the Planning and Compulsory Purchase Act 2004).

2. Approved details

This permission is in respect of the submitted drawings numbered LHS02/01 (dated 7 January 2019), LHS02/03 (dated 9 January 2019), LHS02/04 (dated 8 January 2019) and LHS02/05 (dated 19 January 2019), all received by the local planning authority on 8 March 2019. The development shall be carried out and maintained in accordance with the approved details unless otherwise agreed in writing by the local planning authority.

Reason: For the avoidance of doubt and to ensure that the development is carried out in accordance with the application form and associated details hereby approved.

3. Drainage details

Prior to the commencement of the development, details of the implementation, maintenance and management of the sustainable drainage scheme shall be submitted to and approved in writing by the local planning authority. The scheme

shall be implemented and thereafter managed and maintained in accordance with the approved details. Those details shall include:

- a) A timetable for its implementation, and
- b) A management and maintenance plan for the lifetime of the development which shall include the arrangements for adoption by any public body or statutory undertaker, or any other arrangements to secure the operation of the sustainable drainage scheme throughout its lifetime.

Reason: To prevent increased flood risk from surface water run-off.

Relevant policy: NPPF Section 14, Core Strategy policy CP1 and Managing Development Delivery Local Plan policies CC09 and CC10.

4. Construction Management Plan

No development shall take place, including any works of demolition, until a Construction Method Statement has been submitted to, and approved in writing by, the local planning authority. The approved Statement shall be adhered to throughout the construction period and shall provide for:

- a) The parking of vehicles of site operatives and visitors,
- b) Loading and unloading of plant and materials,
- c) Storage of plant and materials used in constructing the development
- d) The erection and maintenance of security hoarding including decorative displays and facilities for public viewing, where appropriate,
- e) Wheel washing facilities,
- f) Measures to control the emission of dust and dirt during construction,
- g) A scheme for recycling/disposing of waste resulting from demolition and construction works
- h) Measures for ensuring that there is no adverse interruption to existing school activities and operations, including the movement and learning of students

Reason: In the interests of highway safety and convenience and neighbour and school amenities.

Relevant policy: Core Strategy policies CP3 and CP6.

5. Management and Maintenance Scheme

Before the first use of the MUGA hereby approved, a Management and Maintenance Scheme for management responsibilities, maintenance schedule and a mechanism for review shall be submitted to and approved in writing by the local planning authority after consultation with Sport England. The scheme is to include measures to ensure the replacement of the Artificial Grass Pitch within the manufactures' specified period. The measures set out in the approved scheme shall be complied with in full, with effect from commencement of use of the MUGA.

Reason: To ensure that a new artificial grass pitch is capable of being managed and maintained to deliver an artificial grass pitch which is fit for purpose, sustainable and to ensure sufficient benefit of the development to sport.

Relevant policy: NPPF Paragraphs 92, 96 and 97, Core Strategy Policy CP1 and Managing Development Delivery Local Plan Policy TB08

6. Noise management scheme

Prior to the first use of the MUGA hereby approved, a noise management scheme shall be submitted to and approved in writing by the local planning authority. The scheme must specify provisions for the control and mitigation of noise emanating from the use of the MUGA and include the following:

- a) A noise monitoring supervisor
- b) A code of conduct for users of the pitch
- c) A mechanism to report and investigate noise complaints
- d) A mechanism for reviewing and updating noise mitigation measures following receipt of substantiated complaints
- e) Liaison with stakeholders and interested parties

The pitch shall only be used in accordance with the approved noise management scheme.

Reason: To safeguard the residential amenities of nearby residents and the area generally from noise and disturbance.

Relevant policy: NPPF Section 15 (Conserving and Enhancing the Natural Environment), Core Strategy policies CP1 and CP3 and Managing Development Delivery Local Plan policy CC06.

7. Noise mitigation

Prior to the first use of the MUGA hereby approved, the noise mitigation measures in paragraphs 12.4 and 12.5 of the Noise Impact Assessment (numbered TH2111182NR, dated 15 January 2019 and prepared by Peak Consultants) shall be installed and maintained throughout the life of the facility.

Reason: To safeguard the residential amenities of nearby residents and the area generally from noise and disturbance.

Relevant policy: NPPF Section 15 (Conserving and Enhancing the Natural Environment), Core Strategy policies CP1 and CP3 and Managing Development Delivery Local Plan policy CC06.

8. Construction of Multi Use Games Area

The development hereby permitted shall not be brought into use until the area shaded in green and edged by a red outline on drawing LHS02/01 has been cleared and laid out in accordance with drawings LHS02/01 and LHS02/03 so that it is available for use as an artificial grass pitch. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) (England) Order 2015 (as amended) (or any order amending, revoking or re enacting that order) the MUGA shall not thereafter be used for any purpose other than as an artificial grass pitch.

Reason: To secure the provision and use of the MUGA.

Relevant Policy: NPPF Paragraph 97, Core Strategy Policy CP1 and CP3, Managing Development Delivery Local Plan Policy TB08.

9. Biodiversity measures

The lighting, hereby permitted, shall be used until the biodiversity measures outlined in Table 8 of the Bat Survey prepared by Arbtech Consulting Ltd have been installed. A letter/report confirming that the boxes, bricks or tiles have been installed, including a plan showing their location and photographs of the boxes, bricks or tiles in situ, is to be submitted to and approved in writing by the Council prior to use of the lighting.

Reason: To ensure that the ecological value of the site is enhanced post development in line with paragraphs 170 and 175 of the NPPF.

10. Hours of construction work and deliveries

No work or deliveries relating to the development hereby approved, including works of demolition or preparation prior to building operations, shall take place other than between the hours of 8am and 6pm Monday to Friday and 8am and 1pm Saturdays and at no time on Sundays or Bank or National Holidays.

Reason: To protect the occupiers of neighbouring properties from noise and disturbance outside the permitted hours during the construction period.

Relevant policy: Core Strategy policies CP1 and CP3 and Managing Development Delivery Local Plan policy CC06.

11. External Lighting

The proposed artificial pitch shall be artificially illuminated only in accordance with the lighting specification set out within the Lighting Statement by Paul Hawkins Development dated February 2019. The artificial light spill associated with the development shall not exceed that as set out in the approved plans.

Reason: In the interests of neighbour amenity.

Relevant policy: Core Strategy policies CP1 and CP3.

12. Hours of use of MUGA pitch and lighting

The use of the MUGA pitch, hereby permitted, shall not operate other than between the hours of 8am and 9pm Monday to Friday and between the hours of 8am and 8pm on Saturdays, Sundays and Bank Holidays.

Notwithstanding the above, the use of the floodlighting, hereby permitted, is to be turned off no later than the following:

- 6:30pm GMT/7:30pm BST in March
- 8:30pm BST in April
- 9pm BST in June and July
- 8:45pm BST in August
- 7:45pm BST in September
- 6:45pm BST/5:15pm GMT in October

Lighting is to be operated by a secure/lockable, automatic, timer controlled switch and is not to be operated beyond the specified hours.

Reason: To safeguard residential amenities and to protect foraging routes for bats in the nearby woodland.

Relevant policy: Core Strategy policies CP1, CP3 and CP7 and Managing Development Delivery Local Plan policies CC06 and TB23.

13. Amplified music

No external sound amplifying equipment shall be installed within the confines of the MUGA.

Reason: To safeguard the residential amenities of nearby residents and the area generally from noise and disturbance.

Relevant policy: Core Strategy policies CP1 and CP3.

14. Protection of trees

- a) The development shall take place in complete accordance with the Arboricultural Method Statement and Tree Protection Plan prepared by Arbtech Consulting Ltd, which provides for the retention and protection of trees, shrubs and hedges growing on or adjacent to the site in accordance with BS5837: 2012.
- b) No operations shall commence on site in connection with development hereby approved (including any tree felling, tree pruning, demolition works, soil moving, temporary access construction and or widening or any other operation involving use of motorised vehicles or construction machinery) until the tree protection works required by the Approved Scheme are in place on site.
- c) No excavations for services, storage of materials or machinery, parking of vehicles, deposit or excavation of soil or rubble, lighting of fires or disposal of liquids shall take place within an area designated as being fenced off or otherwise protected in the Approved Scheme.
- d) The fencing or other works which are part of the Approved Scheme shall not be moved or removed, temporarily or otherwise, until all works including external works have been completed and all equipment, machinery and surplus materials removed from the site, unless the prior approval in writing of the local planning authority has first been sought and obtained.

Reason: To secure the protection throughout the time that the development is being carried out of trees shrubs or hedges growing within or adjacent to the site which are of amenity value to the area, and to allow for verification by the local planning authority that the necessary measures are in place before development and other works commence.

Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21

15. Community Use Agreement

Within 12 months of the date of this permission and prior to first non-school related use, whether or not it is part of a commercial arrangement, a community use agreement prepared in consultation with Sport England is to be submitted to and

approved in writing by the local planning authority. A copy of the completed approved agreement is to be provided to the local planning authority.

The agreement shall refer to car parking details, changing details, pricing policy, access by non-educational establishment users, hours of use of non-educational users, management responsibilities (including operation of lighting) and a mechanism for review. The development shall not be used otherwise than in strict compliance with the approved agreement.

Reason: To secure well managed safe community access to the MUGA to ensure sufficient benefit to the development of sport.

Relevant policy: Core Strategy Policy CP1 and Managing Development Delivery Local Plan Policy TB08.

Informatives

1. Changes to the Approved Plans

The applicant is reminded that should there be any change from the approved drawings during the build of the development this may require a fresh planning application if the changes differ materially from the approved details. Non-material changes may be formalised by way of an application under s.96A Town and Country Planning Act 1990.

2. Pre occupation conditions

Where this permission requires further details to be submitted for approval, the information must formally be submitted to the Council for consideration with the relevant fee. If this is not clear please contact the case officer to discuss.

3. Protected species

This permission does not convey or imply any approval or consent required under the Wildlife and Countryside Act 1981 for protected species. The applicant is advised to contact Natural England with regard to any protected species that may be found on the site.

Should any bats or evidence of bats be found prior to or during the development, all works must stop immediately and an ecological consultant or the Council's ecologist contacted for further advice before works can proceed. All contractors working on site should be made aware of the advice and provided with the contact details of a relevant ecological consultant.

4. Tree Preservation Order

This permission does not convey or imply any approval or consent required under the Wildlife and Countryside Act 1981 for protected species. The applicant is advised to contact Natural England with regard to any protected species that may be found on the site.

5. Community Use Agreements

Guidance on preparing Community Use Agreements is available from the Sport England website at <http://www.sportengland.org/planningapplications/>. For artificial grass pitches, it is recommended that the applicant seek guidance from the England Hockey on pitch construction when determining the community use hours the artificial pitch can accommodate.

6. Discussion

This permission does not convey or imply any approval or consent required under the Wildlife and Countryside Act 1981 for protected species. The applicant is advised to contact Natural England with regard to any protected species that may be found on the site.

RECENT PLANNING HISTORY

There is an extensive planning history relating to the Luckley Oakfield School dating back to 1959. The more recent planning history is provided.

App Number	Proposal	Decision
F/1997/66008	Single storey conservatory extension	Approved 22 August 1997
LA/1998/67563 F/1998/67330	Upgrading of assembly hall	Approved 24 July 1998
F/2001/4282	Two storey extension to science block	Approved 15 August 2001
F/2001/5482	Two storey library building with classrooms	Approved 20 February 2002
F/2004/3037	Single storey rear extension to art room	Approved 18 November 2004
F/20070963	Two storey music room	Withdrawn 4 June 2007
F/2008/0439		Approved 8 August 2008
F/2008/1144	Maintenance building	Approved 18 July 2008
F/2011/1038	New entrance and improvements to roads and parking	Approved 9 September 2011
VAR/2011/2448	Variation of F/2011/1038 to allow changes to car parking	Approved 13 January 2012
VAR/2012/1235	Variation of F/2011/1038 to allow road, parking and footpath changes	Approved 7 June 2017
182774	MUGA sports pitch (larger pitch but in similar location)	Withdrawn 13 December 2018

SUMMARY INFORMATION

Site Area	School campus measures 5.7 hectares with sports fields making up approximately 1.3 hectares
Previous land use(s)	Class D1 school campus with playing fields and car parking.
Number of jobs created/lost	Nil
Existing parking spaces	Two separate car parks with 89 spaces.
Proposed parking spaces	No change

CONSULTATION RESPONSES

WBC Environmental Health	No objections are raised in relation to the light spillage or noise disturbance to neighbouring properties, subject to conditions that are imposed to protect neighbour amenity and require adherence to the
--------------------------------	--

	Lighting Strategy, the preparation of a Noise Management Scheme and a limitation on the hours of use (see Conditions 11, 6 and 12 respectively).
WBC Trees and Landscaping	No objection, subject to the protection of existing trees (including TPO protected trees), as specified in Condition 14.
WBC Ecology	No objection, subject to Condition 9 relating to biodiversity measures and Condition 12 restricting the use of the lighting in summer months when the bats will be foraging in the nearby woodland.
WBC Highways	No objection.
Sport England	No objection, subject to conditions relating to the construction of the facility (Condition 8) and the submission of additional details in the form of a Management and Maintenance Scheme (Condition 5) and a Community Use Agreement (Condition 15).

REPRESENTATIONS	
Wokingham Town Council	<p>No objection is raised if it were limited to 8pm, with lights being turned off by 8:30pm.</p> <p><u>Officer comment:</u> Refer to 'Neighbour Amenity' for further consideration of the operating hours.</p>
Local Members	<p>Councillor Julian Sumner listed the application for the Planning Committee on the following grounds:</p> <ul style="list-style-type: none"> • Noise pollution • Light pollution <p><u>Officer comment:</u> On balance, the proposal is likely to achieve a satisfactory outcome on neighbour amenity grounds, as discussed in further detail in 'Neighbour Amenity'.</p> <ul style="list-style-type: none"> • Traffic congestion (including no assessment of traffic impacts) <p><u>Officer comment:</u> There are no concerns on traffic and highway access grounds, as noted in 'Highway Access and Parking Provision'.</p>
Neighbours	<p>The application was consulted to neighbours from 25 March 2019 to 22 April 2019, with submissions received from the following properties:</p> <ol style="list-style-type: none"> 1) 3 Luckley Wood, Wokingham RG41 2EW 2) 5 Luckley Wood, Wokingham RG41 2EW 3) 22 Luckley Wood, Wokingham RG41 2EW 4) 24 Luckley Wood, Wokingham RG41 2EW 5) 30 Luckley Wood, Wokingham RG41 2EW 6) 30 Luckley Wood, Wokingham RG41 2EW 7) 31 Luckley Wood, Wokingham RG41 2EW 8) 31 Luckley Wood, Wokingham RG41 2EW 9) 32 Luckley Wood, Wokingham RG41 2EW 10) 32 Luckley Wood, Wokingham RG41 2EW 11) 35 Luckley Wood, Wokingham RG41 2EW 12) 37 Luckley Wood, Wokingham RG41 2EW 13) 39 Luckley Wood, Wokingham RG41 2EW 14) 42 Luckley Wood, Wokingham RG41 2EW 15) 45 Luckley Wood, Wokingham RG41 2EW

- 16) 44 Luckley Road, Wokingham RG41 2EN
- 17) 44 Luckley Road, Wokingham RG41 2EN
- 18) 44 Luckley Road, Wokingham RG41 2EN
- 19) 48 Luckley Road, Wokingham RG41 2EN

- 20) 1 Denby Close, Wokingham RG41 2AL
- 21) 2 Denby Close, Wokingham RG41 2AL
- 22) 3 Denby Close, Wokingham RG41 2AL
- 23) 4 Denby Close, Wokingham RG41 2AL
- 24) 6 Denby Close, Wokingham RG41 2AL
- 25) 6 Denby Close, Wokingham RG41 2AL
- 26) 8 Denby Close, Wokingham RG40 3EY
- 27) 8 Denby Close, Wokingham RG40 3EY
- 28) 9 Denby Close, Wokingham RG41 2AL
- 29) 9 Denby Close, Wokingham RG41 2AL
- 30) 9 Denby Close, Wokingham RG41 2AL

- 31) 6 Hart Dyke Close, Wokingham RG41 2HQ
- 32) 6 Hart Dyke Close, Wokingham RG41 2HQ
- 33) 7 Hart Dyke Close, Wokingham RG41 2HQ
- 34) 7 Hart Dyke Close, Wokingham RG41 2HQ
- 35) 8 Hart Dyke Close, Wokingham RG41 2HQ
- 36) 9 Hart Dyke Close, Wokingham RG41 2HQ
- 37) 10 Hart Dyke Close, Wokingham RG41 2HQ
- 38) 15 Hart Dyke Close, Wokingham RG41 2HQ
- 39) 17 Hart Dyke Close, Wokingham RG41 2HQ
- 40) 18 Hart Dyke Close, Wokingham RG41 2HQ

- 41) 185 Finchampstead Road, Wokingham RG40 3HD
- 42) 185a Finchampstead Road, Wokingham RG40 3HD
- 43) 203b Finchampstead Road, Wokingham RG40 3HS
- 44) 204 Finchampstead Road, Wokingham RG403HB
- 45) Finchampstead Road, Wokingham RG40 3HS (no number specified)

- 46) 3 Bradley Drive, Wokingham RG40 3HZ
- 47) 83 Evendons Lane, Wokingham RG41 4AD
- 48) 12 Gorrick Square, Wokingham RG41 2PB
- 49) Buchandale, Sandhurst Road, Wokingham RG40 3JQ
- 50) 3 St. Helier Close, Wokingham RG41 2HA
- 51) 3 St. Helier Close, Wokingham RG41 2HA
- 52) 41 Tattersall Close, Wokingham RG40 2LP
- 53) 23 Kiln Ride, Finchampstead RG40 3PN
- 54) Lyndhurst, The Village, Finchampstead RG40 4JR
- 55) Hawthorns, Longwater Lane, Finchampstead RG40 4NX
- 56) Hawthorns, Longwater Lane, Finchampstead RG40 4NX

- 57) White Cottage, Milley Road, Waltham St Lawrence RG10 0JP
- 58) White Cottage, Milley Road, Waltham St Lawrence RG10 0JP
- 59) No address supplied

The submissions raised the following issues:

Noise

- Noise pollution
- No noise assessment has been undertaken, including for the tennis courts
- Noise consideration does not account for 22 football players
- Noise assessment only refers to hockey use
- Noise assessment does not refer to voice
- WHO and Sport England noise levels will be breached
- Computer modelling has been used
- The predicted noise levels are inaccurate
- Conclusion that the existing noise levels are the same is incorrect
- Spectators are anticipated in the Planning Statement but not expected in the Noise Statement
- Does not accord with Policy CC06
- Unacceptable continuous noise levels

Officer comment: The Noise Assessment accompanying the planning application includes consideration of football and netball use. It does not consider 22 football players because it is not a full sized football pitch. It includes consideration of hockey balls hitting backpads, human voice and referee whistles. It also includes measurements obtained over a three day period at two residential noise receptors at the nearest residential properties, and is supplemented with modelling, which is standard acoustic practice.

It makes comments in relation to the existing sports facilities but prefaces that there are different variables about intensity and time of use. It also assumes that there will be lower crowd participation than would be anticipated at larger sporting events, which is a reasonable assumption.

It is acceptable for the purposes of this assessment. It has been reviewed by the Council's Environmental Health Officer and the methodology is considered to be sound and the outcomes are considered to be reasonable. Further comment can be found at 'Neighbour Amenity'.

Light pollution

- Light pollution
- Lighting should be removed

Officer comment: The proposal was accompanied by a Lighting Strategy and Plan and the impacts of light spill are acceptable. Refer to 'Neighbour Amenity' for further comment.

- Loss of view/outlook
- Loss of darkness

Officer comment: The proposal will result in some glare to an existing unlit area of the school and against the backdrop of the woodland trees. However, the extent of the impact is not unreasonable in the context of the major development location.

The fencing and light columns are minor elements and sufficiently removed from neighbouring residential properties such that no objection is raised on the grounds of harm to any outlook.

- The lighting levels (and impact upon habitats) have reduced from the previous report

Officer comment: Because of a reduced pitch size, the total power has been reduced from 43kW to 30kW

Traffic and parking

- Traffic congestion, including from large coaches

Officer comment: Refer to 'Highway Access and Parking Provision' for further comment.

- Lack of parking

Officer comment: The MUGA will be used during school hours (where there is no additional impact on traffic) or after school hours, when on-site parking is available. Refer to 'Highway Access and Parking Provision' for further comment.

Ecology

- Light spill and noise will affect wildlife, including bats
- Loss of woodland habitat
- Loss of bats

Officer comment: 'Refer to Ecology' and Condition 12.

- Treelines will be illuminated, contrary to legislation

Officer comment: This is confirmed as an issue in 'Ecology' and has necessitated a restriction in the hours of use of the lighting in Condition 12.

- There are no bat records
- Ecology report is inadequate as it only considers bat roosts

Officer comment: The Bat Survey submitted with this application is satisfactory for the purposes of the application.

- The impacts are contrary to the findings of the Ecology Report for the music building

Officer comment: The Ecology Report for the music building (F/2008/0439) stated that lighting should be directed away from the retained woodland edge in order to maintain darkened flight paths for the bats who commonly use woodland edges to commute through. The lighting associated with this application will cause some illumination of the woodland and Condition 12 requires additional restrictions on the lighting.

- Lighting will affect the landscaping and the SANG

Officer comment: Subject to Condition 12, the proposed lighting will not have an adverse impact upon the landscape setting of the woodland and there is no impact upon the proposed SANG to the south, although it is not yet constructed. Refer to 'Landscaping and Trees'.

Demand

- No requirement for such facilities
- There is no need

Officer comment: The Planning Statement states that the school wishes to provide an all-weather hockey pitch for competition use. There are no existing facilities that allow this. It would also be inappropriate for the Council to question the future desired facilities of the school.

It is worthwhile noting that if it is indeed correct that there is no need for such a facility, the level of use of the MUGA would be reduced.

- Loss of track and field facilities

Officer comment: The proposal will encroach into the existing playing fields, which show use for track and field (evidenced by aerial mapping). Sport England have reviewed the proposal and raise no objection. This is noted in 'Open Space'.

- Represents a change of use to commercial operation

Officer comment: Whilst the intention is for the MUGA to be used by the public, it remains ancillary to the main use of the facility by the school during school hours. It is not considered to represent a change of use to a commercial use and it is not unreasonable arrangement for the school.

Character

- Loss of settlement gap
- Encroaches onto the countryside and will allow housing to be constructed
- Lights will contribute to a loss of settlement gap
- Harm to the character of the area

- Fencing will be intrusive
- Will be visible from neighbouring properties

Officer comment: The MUGA will be positioned inside the settlement boundary and does not encroach into the countryside. There will be some negligible light spill across the southern boundary into the designated countryside and the lighting will be visible from areas in the countryside to the south. However, the pitch, lighting and fencing is not unreasonable in the context of the surrounding built form is not out of character with the edge of settlement location. This is discussed further in 'Character of the Area'.

Landscaping

- Loss of hedgerow
- Loss of TPO protected trees
- Leaf matter and overshadowing will affect the courts
- Canopies may need to be pruned

Officer comment: The MUGA, lighting, fencing and associated groundworks are sufficiently removed from existing hedgerow and trees, including TPO protected trees, such that there is no perceived landscaping issue and the Council's Trees Officer raises no objection.

Drainage

- Flooding caused by additional runoff
- Inadequate drainage details

Officer comment: The MUGA will be positioned in the area of the existing hardcourt tennis courts and also comprises SuDS drainage. Subject to additional details as required by Condition 3, the net runoff from the site is likely to be minimal and no objection is raised.

Other/general

- Unacceptable continuous noise levels
- No break in impact due to use seven days a week
- Hours and days of use are excessive

Officer comment: On the basis that the noise and lighting impacts are reasonable, there is no objection to the use of the MUGA being 365 days per year although in reality, however, continued after hours use will not be the case as it is dependent upon demand, seasonal variations and weather conditions.

- No consultation

Officer comment: Given the scale of the proposal, there is no obligation of the school to undertake consultation.

- Has already been rejected
- Only slightly changed from the previous withdrawn application

Officer comment: Planning application 182774 for a larger MUGA pitch was withdrawn and not rejected because of the Council's concerns with the loss of trees. This issue has been resolved, as noted in 'Landscaping and Trees' and the proposal is now acceptable.

- May be linked to housing proposed under application 190286

Officer comment: There is no connection with planning application 190286 for 216 houses on the site to the south.

- Plans are out of date and invalid

Officer comment: The plans are sufficient for the purposes of the assessment of the subject application, including consideration of the surrounding context.

- Community benefits are vague

Officer comment:

- No Management Plan has been submitted

Officer comment: Condition 15 requires the submission of a Community Use Agreement to address any issues associated with after-hours use by members of the general public.

- Security risk, including for boarders
- Increased risk of robbery

Officer comment: The school is currently used after hours by Wokingham Netball Club. It is also a boarding school, allowing for after-hours activation and surveillance. It is not perceived that the additional use of the MUGA would result in any additional security risk, whether in terms of the safety of boarders or for prospective users.

- Roadway will lead to antisocial behaviour

Officer comment: Aside from an access path, the proposal does not include any new access through the site. There is no perceived link with the use of the MUGA and access thereto and antisocial behaviour.

- Loss of acoustic and visual privacy from spectators towards residents

Officer comment: There is separation of at least 50m to the nearest residential property. This is sufficient for the retention of acoustic and visual privacy for existing residents imposed by proposed users of the MUGA.

	<ul style="list-style-type: none"> Does not satisfy the environmental principles in the Core Strategy and NPPF <p><u>Officer comment:</u> On balance, the proposal achieves a satisfactory environmental outcome and is recommended for approval.</p> <ul style="list-style-type: none"> No dimensions are shown <p><u>Officer comment:</u> The site layout plan is scaled, which is sufficient for the purposes of assessment.</p>
--	---

APPLICANTS POINTS
<ul style="list-style-type: none"> The fencing has been designed in powder-coated black to be as invisible as possible against the backdrop of the area surrounding it The pitch has been positioned to minimise light spill and noise onto surrounding areas and roads, facilitate easy access, whilst eliminating the need for any tree removal, and avoiding damage or impediment to the existing trees to the east and south of the proposed development All tree protection during construction will be in accordance with the Arboricultural Impact Assessment report submitted with this application The entire structure, consisting of carpet, shockpad, base, sub-base and perimeter will be fully permeable. During heavy rainfall, the stone base acts as a water storage medium with a slow release rate. The drainage characteristics are, therefore, similar to the existing natural turf There is no illumination of the tree line to the west of the pitch, a maximum of 20 lux to the tree group south of the pitch and a maximum of 10 lux to the tree and hedgeline to the north-east of the pitch The lights are “state of the art”, and have been designed specifically to illuminate the playing area efficiently whilst minimising light spill both into neighbouring areas and upwards from the luminaire All recommendations of the noise report will be actioned Floodlighting will not impact tree T1; the only tree with bat roosting features. Furthermore, bat foraging and commuting behaviour will not be impacted by the increase in lighting on the tree lines.

PLANNING POLICY		
National Policy	NPPF	National Planning Policy Framework
Adopted Core Strategy DPD 2010	CP1	Sustainable Development
	CP2	Inclusive Communities
	CP3	General Principles for Development
	CP6	Managing Travel Demand
	CP7	Biodiversity
	CP9	Scale and Location of Development Proposals
Adopted Managing Development Delivery Local Plan 2014	CC01	Presumption in Favour of Sustainable Development
	CC02	Development Limits
	CC03	Green Infrastructure, Trees and Landscaping
	CC04	Sustainable Design and Construction
	CC06	Noise
	CC07	Parking
	CC09	Development and Flood Risk
	CC10	Sustainable Drainage

	TB08	Open Space, Sport and Recreational Facilities Standards
	TB21	Landscape Character
	TB23	Biodiversity and Development
Other	BDG	Borough Design Guide
	SDC SPD	Sustainable Design and Construction Supplementary Planning Document
	OSRS	Wokingham Open Space, Sports and Recreation Strategy 2013

PLANNING ISSUES

Description of Development

1. The proposal involves the removal of four existing outdoor tennis/netball courts at the southern end of the school campus and the construction of an uncovered multi use playing area (MUGA) measuring 80m x 50m and constructed with a synthetic turf surface on a bitmac base.
2. It will be enclosed by 3.0m high fencing around the perimeter of the court (4.5m at the goal ends) with rebounding walls and a total of six x 12m high floodlights positioned equidistantly along both sides.
3. Minor groundworks are proposed along the western side, with a 350mm high batter along the western edge. An access path leads from the existing music building and connects with the western side of the pitch.
4. The MUGA is intended for hockey and football as well as tennis and general play and is proposed for use by the school during school hours and after hours use by the general public.
5. The supporting documentation refers to various operating hours of up to 10pm. For the purposes of this application, the hours specified in the application form are used, which are 8:30am-9pm weekdays and 8:30am-8pm on all other days, including bank holidays.

Site Description

6. Luckley House School sits within a 5.7 hectare campus on the southern side of Luckley Road on the southern outskirts of Wokingham. On the site are a collection of two storey buildings, including the Grade II listed Luckley House in the centre of the site forming the nucleus of the main classroom and administration buildings and two storey buildings towards the southern end of the campus. There are two access roads from Luckley Road and two main carparks with 89 parking spaces. There are 1.3 hectares of sports fields at the southern end of the site, protected woodland to the eastern end of the site and TPO protected trees lining the southern boundary.

Principle of Development

7. The National Planning Policy Framework has an underlying presumption in favour of sustainable development which is carried through to the local Development Plan. The Managing Development Delivery Local Plan (MDD) Policy CC01 states that planning applications that accord with the policies in the Development Plan for

Wokingham Borough will be approved without delay, unless material considerations indicate otherwise.

8. The MUGA forms part of the wider school campus and replaces existing sports facilities. It does not involve an increase in staff or student numbers, is within a major development location and meets the sustainability intent of the NPPF and the Core Strategy.
9. The MUGA will be available for use by the general public as a private hire (pay and play) outside of normal school hours. It was initially proposed as being available for up to 10pm but based on the conclusions and recommendations of the applicant's own acoustic report, it is limited to 9pm on weekdays and 8pm on weekends and bank holidays.
10. Given the acceptable nature of the proposal on acoustic and light pollution grounds (when subject to various conditions), there are no planning objections to this arrangement. It accords with the *Open Space, Sports and Recreation Strategy 2013*, which permits the '*dual use of both primary and secondary school sports facilities...providing that these facilities include a higher proportion of artificial turf pitches or hard courts than might otherwise be expected.*'
11. Submissions against the application argued that there were other similar facilities in Wokingham and that the subject MUGA is not required. However, this ignores the fact that the initial intentions of the school are to provide additional facilities for students. Even so, the existence of other pitches in the area is of no real consequence and if it is underutilised, any amenity impacts upon surrounding residents would be lessened by less frequent use.

Character of the Area

12. Policy CP3 of the Core Strategy states that development must be appropriate in terms of its scale, mass, layout, built form, height and character of the area and must be of high quality design. Policy NR8 requires high quality and simple materials and components.
13. The MUGA pitch will be located within the major development location but the settlement edge lies immediately to the south. In this respect, Policy CC02 of the MDD Local Plan states that development at the edge of settlements is acceptable where it is demonstrated that it is within development limits and respects the transition between the built up area and the open countryside by taking account of the character of the adjacent countryside and landscape.
14. The proposal includes the removal of four hardcourt tennis courts (approximately 2,200m² of hard paving) and its replacement with a 4,030m² synthetic pitch. It will be coloured green, which will complement the turf of the playing fields. It will be constructed with a level playing surface, which will necessitate a 350mm raising of the ground on its western edge, which is not unreasonable. It will be located at the southern edge of the campus and will be framed by protected trees along its western and southern edge. This gives a detached appearance to the pitch but given it is within the settlement boundary and well removed from neighbours, the openness of the area will remain intact and there is no perceived issue.

15. The proposal also includes the installation of 3.0-4.5m high fencing around the playing surface and 8 x 15.5m high floodlights along the western and eastern (or long) sides of the court. The fencing will be open in nature and will allow sightlines through the site. It is not out of character with other facilities within the school campus and would arguably be less obtrusive than the combined effects of the existing tennis court fencing. At a predominant height of 3.2m, it is not of excessive height and no objections are raised.
16. The floodlights are lightweight in nature with a width of 440mm at the base. They are also retractable back onto the playing surface. At a height of 12m, they will be the equivalent height of the adjacent sports hall. However, the height is still not excessive given it is framed by woodland trees and can be viewed amongst the backdrop of the existing school buildings.
17. The spillage from the light columns is well contained to the immediate area of the pitch but the submitted plan shows some spillage across the settlement boundary to the south (when not accounting for existing trees). The light spill will also be visible as an urban element, reducing the level of darkness in this area of the school campus. However, on balance, it is an acceptable element within the development limits and when viewed from the rural landscape to the south.
18. Accordingly, no objection is raised in relation to the aforementioned Policies CP3, CC02 and NR8.

Heritage and Conservation

19. Section 66 of the Planning (Listed Buildings and Conservation Areas) Act 1990 states that development must ensure the preservation of any nearby listed building, including its setting, Paragraphs 192-196 of the NPPF requires consideration of the harm to the significance of a designated heritage asset and Policy TB24 of the MDD Local Plan requires the conservation and enhancement of Listed Buildings, including their views and setting.
20. Luckley House is a Grade II listed building that is the focal building within the school campus. It is 50m to the north of the proposed MUGA as the crow flies but is largely screened by existing vegetation and the sports hall. Furthermore, there are numerous non-listed buildings closer to the listed building that interrupt the interpretation of the building. Moreover, the extent of the proposed works (pitch, fencing and light columns) and the impression of lighting from the columns is reasonable and does not pose an adverse outcome for the significance of the building.

Residential Amenities

21. Policy CP3 of the Core Strategy aims to protect neighbouring amenity and Policy CC06 and Appendix 1 of the MDD Local Plan requires that development protect noise sensitive receptors from noise impact.
22. As measured from the fencing along the western and eastern sides of the pitch, it is some 75m from the rear boundary of the nearest residential property in Denby Close to the west and 51m from the rear boundary of the nearest residential property in Luckley Wood to the east. A total of 18 residential properties adjoin the

school grounds on both boundaries but the visibility of the pitch and its associated elements, including the lighting when it is in use, would vary because of available sightlines across open countryside to the south and the heavily wooded land to the east of the pitch.

Sunlight and Daylight, Dominance and Enclosure

23. The MUGA pitch is enclosed by a 3.0-4.5m high fence, which itself is unimposing. It is surrounded by other taller school buildings and is at least 75m from the nearest residential dwelling. There is no perception of dominance or loss of light and as a result the proposal is acceptable.

Overlooking

24. The MUGA will be located within the existing school grounds. Whilst it will feasibly lead to increased numbers of participants and for longer periods of the day, there is adequate separation to neighbouring properties such that there is no unreasonable overlooking.

Noise pollution

25. The existing sporting facilities in this area of the school comprise four tennis courts and playing fields, which have a dual use as an undersized football pitch, turf hockey pitch, track and field and rounders. The replacement with a synthetic, all weather pitch for hockey, football and tennis, which will allow increased usage of the facilities, including during evening periods and during poorer weather. The noise levels associated with this use are likely to increase and this has the potential to harm neighbouring residents.

26. A Noise Assessment was provided with the planning application. It notes that for most sports, the most dominant sound is generally from human voice, but in the case of hockey, the ball hitting the backboards is most pronounced. The assessment also refers to human voice (including shouting (in this case a male voice is referred to)), footballs hitting fences, clashing of sticks, some spectator participation and the referees whistle. Given the less intensive nature of a game of tennis, fewer participants and existing courts, noise arising from this activity is irrelevant to the assessment.

27. Furthermore, as the facility is for a school, it is not anticipated that there would be large numbers of vocal supporters and this conclusion is reasonable.

28. Amongst the other existing activities of the school, including use of the outdoor sporting facilities, the indoor sports hall and general lunchtime play, the use of the MUGA during normal school hours is unlikely to result in adverse or unacceptable noise transmission above that expected and tolerated of a secondary school. In this regard, it is acceptable.

29. Its use after school hours by school students would result in some additional noise levels but would remain acceptable in the context of existing school extracurricular and sporting activities that would typically occur or would be expected on a day to day basis at the school. Similarly, where the pitch would now allow for competition

marches (hockey and tennis only), it would not be unreasonable to expect some noise from the use of the facility for school purposes.

30. The use of the pitch by the general public up until 9pm on weekdays and 8pm on weekends will result in additional noise outside of normal school hours, including during holidays.
31. The comments from the Environmental Health Officer conclude that the proposal is satisfactory, subject to additional mitigation for rubber backboards, fabric football goals and resilient fencing as recommended in the Noise Assessment. Paragraphs 32-37 outline, in detail, these comments.
32. The Planning and Design and Access Statement states that the proposed artificial pitch provides an opportunity for additional use of the school's facilities by the community in the evenings after school activities have ceased as well as during the day and evenings during holiday periods. This will result in intensified use of the site compared to the existing grass sports field especially during winter weather conditions and the introduction of the artificial flood light system. There are existing sensitive residential properties located in close proximity to the proposal site that could be adversely affected by noise from this intensified use.
33. A Noise Impact Assessment (NIA) has been submitted with the application dated 15 January 2019 by Peak Consultants. This includes an assessment of the existing residual noise levels at the site and comparison of these against criteria outlined in 'Artificial Grass Pitch (AGP) Acoustics – Planning Implications' (Sport England, 2015) which specifies typical noise levels of sports pitches and their associated noise impacts.
34. The noise level of the MUGA over a representative period of one hour has been calculated at the nearest sensitive receptors (NSRs) and is displayed in Table 3. These results indicate that noise generated by the proposed MUGA would be below the existing ambient levels in terms of the level averaged over a sports game until 9pm on weekdays and 8pm on weekends. After these times the residual noise levels are lower and noise from the MUGA would be more prominent at the NSRs. It is understood the applicant was originally seeking use of the development until 10pm. The NIA states that this would be possible with mitigation in place such as acoustic fencing but the applicant has advised the development will now not be used beyond 9pm on weekdays and 8pm on weekends in view of the assessment results.
35. Maximum individual noise events have also been calculated and are shown in Table 4. The results indicate that impulsive noise events arising from hockey balls impacting on goal back boards and a referee's whistle would exceed the existing noise maxima by approximately 3-6db and so may be audible at the NSRs. It is stated that noise from hockey balls hitting the goal back boards could be mitigated by lining the backboards with a resilient rubber layer of the type supplied by sports surfacing manufacturers. Other more generic mitigation measures are also proposed in Section 12.5 to minimise noise impact in the design of the pitch and selection of materials and products in accordance with the Sport England Guidance.
36. This leaves only impulsive noise from a referee's whistle exceeding the existing noise maxima by approximately 3-6 db. IEMA's guidelines for environmental noise

impact assessment state that a change of 3 dB(A) is the minimum perceptible under normal conditions. Residents at the NSRs are therefore likely to notice the impulsive noise from a referee's whistle but I do not consider this to be at a level that would cause an unreasonable impact.

37. In view of the above, I have no grounds for objection and so would recommend approval subject to the conditions to ensure the proposed hours of use and noise mitigation outlined in the report are adhered to. I would also recommend a condition requiring provision of a noise management plan for the facility in view of the proximity of the NSRs. The facility is being opened up to the community and some users may act in an unreasonable manner resulting in higher noise levels and the Sport England Guidance states that a management/monitoring plan could effectively respond to specific incidents. Similarly, the restriction of use to authorised groups could be appropriate in many situations.
38. The above conditions form Conditions 5, 6, 7, 12 and 15. It would also be prudent to prohibit any external speaker or public address system, as Condition 13 does.
39. Expanding on this, it is worth noting that the pitch does not conform to full standards that would allow 11 on 11 play in hockey or football or full competition play. As such, the numbers of players will generally be lower than would otherwise be expected of such an activity. Where it is proposed to be used for tennis, it would, as an absolute maximum, result in a similar number of players if all courts were being used.
40. The construction of the proposal is likely to generate noise but it is not unreasonable in the residential context. Hours of construction are conditioned at Condition 10.

Light pollution

41. The MUGA pitch will have six x 12m high lighting columns spaced equally along the long sides of the pitch. The lighting level is 350 lux. The supporting documentation refers to the height of the columns and the lighting levels are the minimum standard to meet England Hockey and The Federation International de Hockey.
42. The lighting is proposed to be used until 9pm on weekdays and 8pm on weekends, which will result in light glare from surrounding residential properties, mostly in winter months. The application was supported by a Lighting Strategy and Lighting Plan (dated February 2019 by Paul Hawkins Development) and this was reviewed by the Council's Environmental Health Officer. Comments follow in paragraphs 43 and 44.
43. The proposed lighting system utilises 20 x Philips MVP 507 Optivision luminaires with 2Kw lamps. These are reportedly 'state of the art' and have been designed specifically to illuminate the playing area efficiently whilst minimising light spill both into neighbouring areas and upwards from the luminaire. The light spill levels away from the pitch at a height of 2m are shown in accompanying calculations and on drawing LHS02/04. These indicate that the illumination is largely confined to the playing area and falls off rapidly away from the pitch. There is no measurable illumination either onto (or beyond) the tree lines to the west and south west of the pitch, any nearby homes or roads.

44. The proposed lighting installation complies with the Institution of Lighting Engineers Guidance notes for the reduction of obtrusive light (2005) and is the minimum required by England Hockey and The Federation international de Hockey (FIH). It is accepted, subject to Condition 11 requiring installation to be in accordance with the submitted details.
45. Expanding on this, it is also worth noting that if the light columns were lowered below 12m, it would lead to increased upwards light spillage because of the need to project the light outwards rather than downwards. It would also result in glare for players,
46. Whilst the lighting will be visible in the backdrop of the school and its edge of settlement location, the existing vegetation provides some screening and even then, the existence of the lights in the wider domain is not unreasonable. Furthermore, the visual impacts will generally only be confined to the winter months. During daylight saving, the lighting will be used for only a limited period in the evening, if at all at the height of summer.
47. Accordingly, subject to Condition 12, there are no objections on light spillage grounds and there is no specific opposition to the lights being in use until 9pm. The condition is specific in that it requires that the lights be controlled by a timer, that access be secure so that it cannot be tampered with and that they are off by 9pm, which means that play will cease prior to 9pm.

Dust pollution

48. The construction of the proposal may give rise to some dust generation but there is a relatively large separation distance to neighbouring properties. Condition 4 requires the submission of a Construction Management Plan to address any potential issues, including with school pupils.

Access and Movement

49. Policy CC07 and Appendix 2 of the MDD Local Plan stipulates minimum off street parking standards. In this case, the MUGA pitch supplements the existing school facilities and the parking generation rate is applied per number of staff. Alternatively, with its availability for community use, the generation rate could also be applied at a rate of 20 spaces per pitch.
50. Use of the MUGA during school hours is unlikely to generate any perceptible increase in traffic movement during peak hour periods. It is feasible that additional coaches will access the site during competition but the impact is minimal and not unreasonable.
51. It will allow for increased after school usage by students, This would have the benefit of reducing peak traffic movements during the afternoon pick up time as more students will remain on the campus after hours and be picked up later in the afternoon. There would also be more parking spaces available in the existing car park.
52. The facility is also intended for use for private hire by members of the public up to 9pm. With its undersized dimensions not allowing for a full game of hockey or

football, there is likely to be no more than 25-35 additional people attending the school, when accounting for players, officials and spectators and allowing for overlap between games. Any perceived increase in traffic movements from the use of the facility by coaches and players will be comfortably accommodated within the existing 89 car spaces and the movements will be outside of school hours and almost entirely outside of peak hour when traffic flows are lower. Cycling facilities are available, which are likely to be used given the limited need for the equipment required for football activities.

53. The existing parking facilities are sufficient to account for the increase in movements and it is unlikely that on-street parking will be affected, particularly given the walking distances to the pitch at the southern end of the school. In this regard, no objection is raised, particularly as Council's Highways Officer has reviewed the proposal and raises no objection.

Open Space

54. Policy TB08 of the MDD Local Plan states that there should be no loss of open space, sporting or recreational facilities and that new facilities should be provided on site. Paragraph 97 of the NPPF states that existing open space, sports and recreational buildings and land, including playing fields, should not be built on unless

- a) *an assessment has been undertaken which has clearly shown the open space, buildings or land to be surplus to requirements; or*
- b) *the loss resulting from the proposed development would be replaced by equivalent or better provision in terms of quantity and quality in a suitable location; or*
- c) *the development is for alternative sports and recreational provision, the benefits of which clearly outweigh the loss of the current or former use.*

55. The existing playing field is used for undersized hockey pitches and summer sports like athletics and rounders. Four hard tennis courts are located in the south east corner. The proposed fenced synthetic sport pitch with sports lighting will be designed primarily for hockey training and play. Other sports may be possible to be delivered on in within the curriculum.

56. The proposal includes six tennis courts within the MUGA, which is a net increase of two courts. However, the enlarged MUGA within the playing field prejudices the use, or leads to the loss of use, of a playing field in the last five years, as defined in The Town and Country Planning (Development Management Procedure) (England) Order 2015 (Statutory Instrument 2015 No. 595). On this basis, Sport England were consulted.

57. The application relates to the provision of a new indoor/outdoor sports facility or facilities on the existing playing field and is to be considered against exception 5 of the above policy, which states:

'The proposed development is for an indoor or outdoor facility for sport, the provision of which would be of sufficient benefit to the development of sport as to outweigh the detriment caused by the loss, or prejudice to the use, of the area of playing field.'

58. Sport England note that the non-conforming size of the pitch is disappointing particularly given it does not address a shortfall of pitches in the area. However, it is not opposed on these grounds. To address this, Condition 15 requires a Community Use Agreement, which will allow for more broader use of the pitch as a community facility and to provide training facilities for teams.
16. Sport England, along with the Football Association and England Hockey raise no objection as it is considered to meet exception 5 (E5) of the above policy. This is subject to Condition 5 (Management and Maintenance Scheme) and Condition 15 (Community Use Agreement).

Accessibility

59. Policy CP2 of the Core Strategy seeks to ensure that new development contributes to the provision of sustainable and inclusive communities, including for aged persons, children and the disabled. The pitch will be installed at existing ground level, a DDA complaint path will connect with the school and there are blue badge spaces in the existing car park. This ensures that the proposal achieves full accessibility.

Flooding and Drainage

60. Policy CC09 of the MDD Local Plan requires consideration of flood risk from historic flooding and Policy CC10 requires sustainable drainage methods and the minimisation of surface water flow.
61. The site and access thereto is located within Flood Zone 1 and there is no increase in the vulnerability of the use. As it is replacing a four existing hard court tennis courts, the loss of soft landscaping is less than 2000m². It will also be fully permeable and comprise inbuilt SuDS drainage as part of the new playing surface, thereby implying negligible impacts in terms of runoff. Subject to additional details in Condition 3, there is no objection in terms of Policies CC09 and CC10.

Landscaping and Trees

62. Policy CC03 of the MDD Local Plan aims to protect green infrastructure networks, promote linkages between public open space and the countryside, retain existing trees and establish appropriate landscaping and Policy TB21 requires consideration of the landscape character.
63. The site is located within the settlement boundary but adjacent to the countryside which is south of the site. The southern boundary of the school with the countryside is defined by a TPO woodland and tree group (1564/2017) which are significant landscape features.
64. The Council's Trees Officer has reviewed the proposal and raises no objection. The pitch is oriented within the site so as not to impact on the adjacent TPO trees, whether in terms of existing roots or canopy. This is confirmed by the information provided within the Arboricultural Method Statement dated 5 March 2019 and associated Tree Protection Plan (Arbtech TTP 02).

Ecology

65. Policy TB23 of the MDD Local Plan requires the incorporation of new biodiversity features, buffers between habitats and species of importance and integration with the wider green infrastructure network.
66. Council's Ecology Officer has reviewed the proposal in terms of any perceived impact upon protected species from light spill and noise disturbance. They note that the site comprises an amenity grassland sports pitch, bound by hedgerows/lines of trees and a block of broadleaved woodland along the north-eastern and western boundary. The woodland is a priority habitat as per the NPPF as are the hedgerows.
67. A bat survey report (prepared by Arbtech Ltd and dated 09 January 19) has been submitted with the application. The report details the results of a day time inspection and a survey of nearby trees for their potential to host roosting bats. The report states that *'The tree lines on site provide excellent foraging and commuting habitat for bats. Tree T1 has suitable roosting features'* and that *'the new floodlighting will have no impact on T1. The proposed lighting will not impact bat commuting and foraging as this will only be used during the winter months when bats are hibernating.'*
68. The assessment that the "tree lines on site provide excellent foraging and commuting habitat for bats" is likely to be correct., and, if the proposed lighting is only to be used during the winter then the statement that "The proposed lighting will not impact bat commuting and foraging as this will only be used during the winter months when bats are hibernating" would also be correct.
69. However, the Design and Access Statement does not rule this out, noting that the floodlights will not be used after 10:00pm. It notes further that *'there is no illumination of the tree line to the west of the pitch, a maximum of 20 lux to the tree group south of the pitch and a maximum of 10 lux to the tree and hedgeline to the north-east of the pitch.'* This appears to differ from the Illuminance Plan (plan LHS02/04) which shows lux levels of up to 368 Lux along the north eastern boundary and up to 73 along the south.
70. These are very high light levels and are likely to stop bats commuting along these routes. This is likely to occur even if light levels were the lower figures quoted in the Design and Access Statement. Bats emerge from their roosts and start foraging at dusk (approximately 30 minutes after sunset). Impacts on bats can therefore be reduced by restricting the times at which lights can be switched on. During winter (November to March) bats are usually hibernating, so are unlikely to be affected by lighting.
71. As such, unless the lights are not used during the summer months, there is a risk that the proposals would affect commuting and foraging bats. It is therefore recommended that a condition is set to ensure that the lights are turned off by the following times and that the pitches are not used before dawn:
- March: 18.30 GMT/19.30 BST
 - April: 20.30 BST
 - May: 21.15 BST

- June: 21.45 BST
- July: 21.30 BST
- August: 20.45 BST
- September 19.45BST
- October 18.45 BST/ 17:15 GMT

72. The Council's Ecology Officer notes that the proposal is unlikely to affect other protected species such as badgers, reptiles or amphibians as the habitat to be affected (amenity grassland) are unlikely to be used by these species. On this basis, and when subject to Condition 12 controlling the time of the lighting, there are no objections on ecology grounds.

Thames Basin Heaths Special Protection Area

73. Policy CP8 of the Core Strategy states that where development is likely to have an effect on the Thames Basin Heaths Special Protection Area (TBH SPA), it is required to demonstrate that adequate measures to avoid and mitigate any potential adverse effects are delivered. There are no SPA related implications associated with the proposal.

Waste

74. The development complements existing school facilities and does not represent any adverse increase in waste generation. Any refuse can be disposed of using existing facilities. As such, no objection is raised.

Contamination

75. There is no known contamination of the site.

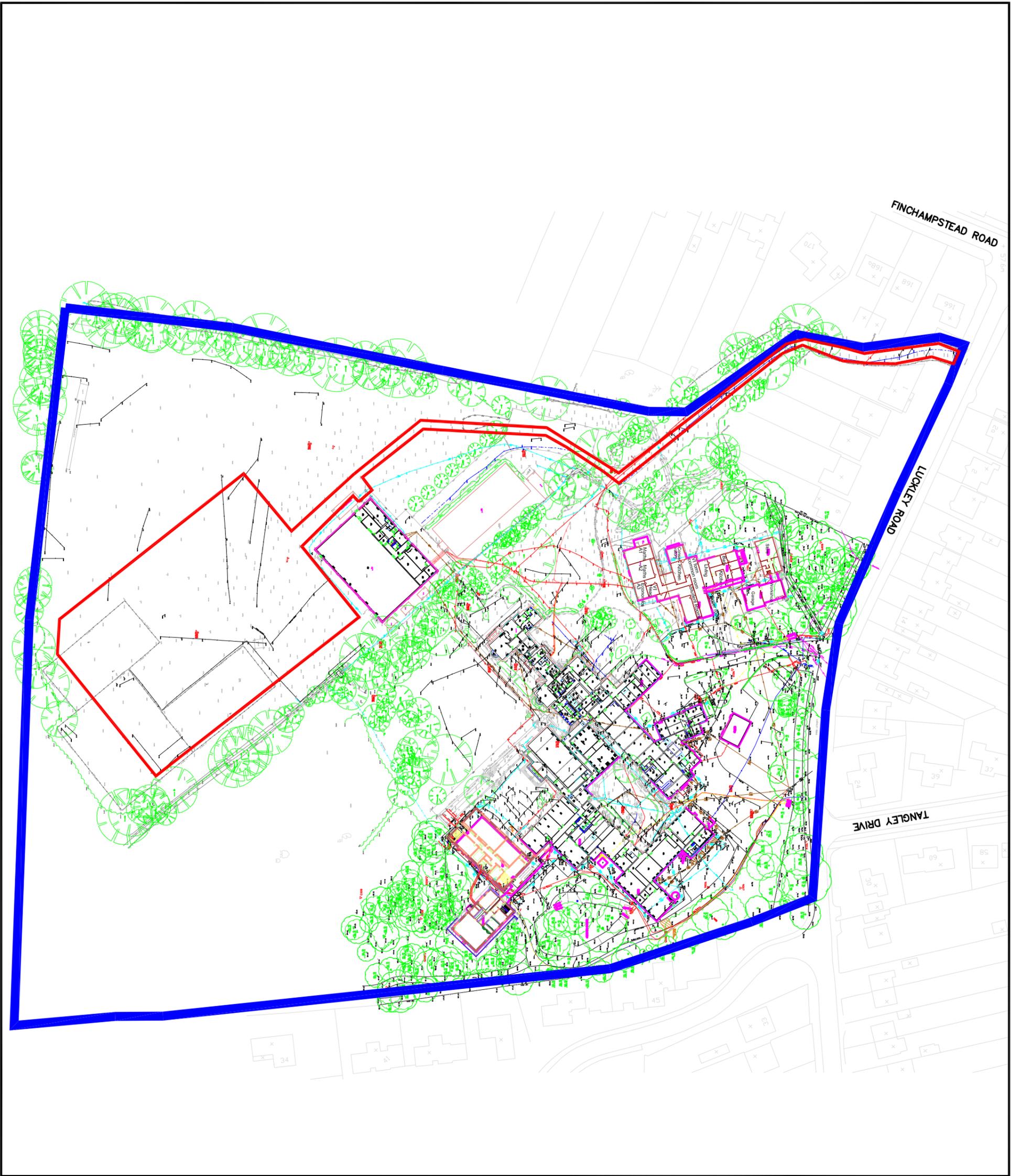
Community Infrastructure Levy

76. The application is not liable for CIL payments.

CONCLUSION

The proposal is considered to be acceptable, including in terms of its community use during the evening period (and the associated impacts of light and noise pollution upon surrounding residents. Approval is recommended although it remains subject to conditions relating to the hours of use of the MUGA and of the lighting and ongoing management and use.

This page is intentionally left blank



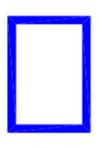
FINCHAMPSTEAD ROAD

LUCKLEY ROAD

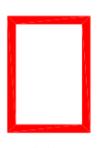
TANGLEY DRIVE



REVISION HISTORY



LAND OWNED BY
LUCKLEY HOUSE SCHOOL



SITE BOUNDARY
(0.62 Ha)

PHD
Paul Hawkins Development
365 Uppingham Road, Leicester LE5 4DP

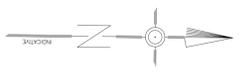
LHS02/05
Luckley House School
New Hockey Pitch
Location Plan

09/01/19
Scale: 1:1250(A3)
© PAUL HAWKINS DEVELOPMENT 2019

This page is intentionally left blank

This page is intentionally left blank

This page is intentionally left blank



REVISION HISTORY

PHD Paul Hawkins Development
 Ludlow House School
 New Adventure sports path
 Illumination levels (lux)

08/01/19
 © PAUL HAWKINS DEVELOPMENT 2019
 1:150000

This page is intentionally left blank

PLANNING REF : 190673
PROPERTY ADDRESS : Town Hall
: Market Place, Wokingham, Berkshire
: RG40 1AS
SUBMITTED BY : Wokingham Town Council
DATE SUBMITTED : 17/04/2019

COMMENTS:

The Wokingham Town Council P&T Committee would approve the application if change was made so that activities were to cease on site at 8pm and the Floodlights be switched off by 8:30pm.

This page is intentionally left blank

Agenda Item 93.

Application Number	Expiry Date	Parish	Ward
190233	13 th May 2019	Winnersh	Winnersh

Applicant	Kevin Harris, Royal Masonic Benevolent Institution
Site Address	Lord Harris Court, Mole Road, Sindlesham, RG41 5EA
Proposal	Full planning application for demolition of Lord Harris Court Centre care home (88 bedroom) and erection of a new 45 bedroom care home and 60 assisted living apartments (C2 use Class), together with associated car parking, landscaping and amenity space provision.
Type	Full
PS Category	1
Officer	Andrew Chugg
Reason for determination by committee	Major application

FOR CONSIDERATION BY	Planning Committee on Wednesday, 8 May 2019
REPORT PREPARED BY	Assistant Director – Place

SUMMARY
<p>The proposal overcomes all of the reasons for refusal issued on the previous application (180194) and would provide adequate living accommodation for both a new care home and the assisted living units while protecting the amenities of nearby residential properties. The bulk and height of the scheme (in particularly the care home) has been significantly reduced to improve the relationship of the built form with the street frontage along Mole Road and the adjacent Countryside and Conservation Area. Important trees and protected species would be suitably protected and the applicant has agreed to enter into a s106 legal agreement to secure necessary planning obligations that are directly related to the development and fairly and reasonably related in scale and kind.</p> <p>The proposal is acceptable in highway safety terms and the applicant has agreed to provide an additional 7 car parking spaces (the details of which would be secured via condition requiring an amended plan to be submitted) which would demonstrate adequate parking provision would be provided for the proposed development on site.</p> <p>Therefore, overall the proposal is policy compliant subject to adherence to the s106 obligations and conditions outlined below.</p>

PLANNING CONSTRAINTS
<ul style="list-style-type: none"> • Limited development location • Sand and gravel extraction • Minerals consultation zone • Great Crested Newt Consultation Zone • Tree Preservation Order • Adjacent to Grade II Listed Building • Adjacent to Bearwood Recreation Conservation Area

RECOMMENDATION

That the committee authorise the **GRANT OF PLANNING PERMISSION** subject to the following:

A. Completion of a legal agreement to secure:

- i. **An Employment Skills Plan and/or financial contribution in lieu of any ESP targets that are not met; and**
- ii. **That the proposed assisted living apartments (C2 Use Class) fall within the council's extra care housing model and are maintained as such in perpetuity.**

B. Conditions and informatives:

Conditions:

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason: In pursuance of s.91 of the Town and Country Planning Act 1990 (as amended by s.51 of the Planning and Compulsory Purchase Act 2004).

2. This permission is in respect of the submitted application plans and drawings numbered: Site Location Plan - A-731 01 Rev A, Masterplan - Site Plan - A-731 55 Rev C, Site Plan – Dimensions to Neighbouring Properties - A-731 56 Rev C, Masterplan – Care Home Site Plan - A-731 57 Rev C, Masterplan – Assisted Living - A-731 58 Rev C, Care Home Floor Plans - A-731 60 Rev A, Care Home Floor Plans - A-731 61 Rev A, Assisted Living Floor Plans - A-731 62 Rev A, Assisted Living Floor Plans - A-731 63 Rev A, Assisted Living Floor Plans - A-731 64 Rev A, Services/Refuse Store Floor Plan - A-731 65 Rev A, Assisted Living Site Sections - A-731 70 Rev B, Assisted Living Site Sections - A-731 71 Rev B, Care Home Site Sections - A-731 72 Rev A, Care Home Elevations - A-731 75 Rev A, Care Home Elevations - A-731 76 Rev A, Assisted Living Elevations - A-731 80 Rev A, Assisted Living Elevations - A-731 81 Rev A, Assisted Living Elevations - A-731 82 Rev A, Assisted Living Elevations - A-731 83 Rev A, Assisted Living Elevations - A-731 84 Rev A and Assisted Living Elevations - A-731 85 Rev A as received by the local planning authority on 25th January 2019. The development shall be carried out in accordance with the approved details unless other minor variations are agreed in writing after the date of this permission and before implementation with the Local Planning Authority.

Reason: For the avoidance of doubt and to ensure that the development is carried out in accordance with the application form and associated details hereby approved.

3. Prior to the commencement of any development and notwithstanding the details shown on the plans hereby approved (refer to Condition 2 above), an amended parking layout plan to demonstrate the provision of 7 additional parking spaces for staff parking shall be submitted to and approved in writing by the local planning authority.

Reason: To ensure adequate on-site parking provision in the interests of highway safety, convenience and amenity. Relevant policy: Core Strategy policies CP3 & CP6 and Managing Development Delivery Local Plan policy CC07.

4. Prior to the commencement of development a strategy for the two phases of the development hereby approved shall be submitted to and approved in writing by the Local Planning Authority. The Phasing Strategy will define the development to be delivered within each phase and the anticipated timescales for the completion of each phase. All development shall be carried out in accordance with the approved Phasing Strategy.

Reason: to ensure the comprehensive planning of the site, highway safety and to protect the amenity of the area in accordance with Wokingham Borough Core Strategy Policies CP1, CP3 and CP6.

5. Before any phase of the development hereby permitted is commenced beyond slab level, samples and details of the materials to be used in the construction of the external surfaces of the building/s on that phase shall have first been submitted to and approved in writing by the local planning authority. Development shall not be carried out other than in accordance with the approved details.

Reason: To ensure that the external appearance of the building is satisfactory.
Relevant policy: Core Strategy policies CP1 and CP3

6. No development of either phase shall take place until a measured survey of that phase of the site and a plan prepared to scale of not less than 1:500 showing details of existing and proposed finished ground levels (in relation to a fixed datum point) and finished roof levels for that phase shall be submitted to and approved in writing by the local planning authority, and the approved scheme shall be fully implemented prior to the occupation of the building(s).

Reason: In order to ensure a satisfactory form of development relative to surrounding buildings and landscape. Relevant policy: Core Strategy policies CP1 and CP3 and Managing Development Delivery Local Plan policy TB21.

7. No phase of the development hereby approved shall be occupied until either; i) confirmation from Thames Water that all waste water network upgrades required to accommodate the additional flows from the respective phase of the development have been completed has been submitted to and approved by the local planning authority; or ii) an accommodation and infrastructure phasing plan agreed between the developer and Thames Water in order to allow additional properties to be occupied has been submitted to and approved by the local planning authority. Where an accommodation and infrastructure phasing plan is agreed no occupation shall take place other than in accordance with the agreed plan.

Reason - The development may lead to sewage flooding and network reinforcement works are anticipated to be necessary to ensure that sufficient capacity is made available to accommodate additional flows anticipated from the new development. Relevant policy: Core Strategy policies CP1 and CP3 and Managing Development Delivery Local Plan CC04 and CC10.

8. No part of any phase hereby permitted shall be occupied or used until the vehicle parking spaces for that phase has been provided in accordance with the approved plans. The vehicle parking space shall be permanently maintained and remain available for the parking of vehicles at all times.

Reason: To ensure adequate on-site parking provision in the interests of highway safety, convenience and amenity. Relevant policy: Core Strategy policies CP3 & CP6 and Managing Development Delivery Local Plan policy CC07.

9. Prior to the commencement of the development of either phase, details of vehicle turning space within the site (allowing vehicles to turn so that they may enter and leave the site in a forward gear) for that phase shall be submitted to and approved in writing by the local planning authority. Neither phase of development shall be occupied until the turning space for the respective phase has been provided in full accordance with the approved details. The turning space shall thereafter be so-retained and shall be used for no other purpose.

Reason: To ensure a satisfactory form of development and to avoid adverse impact on the public highway in the interests of highway safety. Relevant policy: Core Strategy policies CP3 & CP6.

10. Prior to first occupation of either phase of the development hereby permitted details of secure and covered bicycle storage/ parking facilities for the occupants of [and visitors to] that phase of the development shall be submitted to and approved in writing by the local planning authority. The cycle storage/parking shall be implemented in accordance with such details as may be approved before occupation of each phase of the development hereby permitted, and shall be permanently retained in the approved form for the parking of bicycles and used for no other purpose.

Reason: In order to ensure that secure weather-proof bicycle parking facilities are provided so as to encourage the use of sustainable modes of travel. Relevant policy: NPPF Section 9 (Sustainable Transport) and Core Strategy policies CP1, CP3 & CP6 and Managing Development Delivery Local Plan policy CC07.

11. Prior to the first occupation or use of either phase, a Travel Plan for that phase shall be submitted to and approved in writing by the local planning authority. The travel plan shall include a programme of implementation and proposals to promote alternative forms of transport to and from the site, other than by the private car and provide for periodic review. The travel plan shall be fully implemented, maintained and reviewed as so-approved.

Reason: To encourage the use of all travel modes. Relevant policy: NPPF Section 9 (Sustainable Transport) and Core Strategy policy CP6.

12. No development shall take place, including any works of demolition, for either phase of the development hereby permitted until a Construction Method Statement for that phase has been submitted to, and approved in writing by, the local planning authority. The approved Statement shall be adhered to throughout the construction period. The Statement shall provide for:

- i) the parking of vehicles of site operatives and visitors,
- ii) loading and unloading of plant and materials,
- iii) storage of plant and materials used in constructing the development,
- iv) the erection and maintenance of security hoarding including decorative displays and facilities for public viewing, where appropriate,
- v) wheel washing facilities,
- vi) measures to control the emission of dust and dirt during construction,
- vii) a scheme for recycling/disposing of waste resulting from demolition and construction works.

In the interests of highway safety & convenience and neighbour amenities.

Relevant policy: Core Strategy policies CP3 & CP6.

13. No development of either phase shall take place until there has been submitted to and approved in writing by the Local Planning Authority, a Communications Plan for that phase. Each Communications Plan shall specify methods for communicating with local residents, including the creation of a liaison group to meet in accordance with an agreed schedule. The Plans shall be carried out as approved until the final completion of the development In order to minimise disturbance to neighbours during construction works.

Reason: In order to minimise disturbance to neighbours during construction works.

Relevant policy: Core Strategy policy CP3.

14. No work relating to the development hereby approved, including works of demolition or preparation prior to building operations, shall take place other than between the hours of 08:00 and 18:00 Monday to Friday and 08:00 to 13:00 Saturdays and at no time on Sundays or Bank or National Holidays.

Reason: To protect the occupiers of neighbouring properties from noise and disturbance outside the permitted hours during the construction period. Relevant policy: Core Strategy policies CP1 and CP3 and Managing Development Delivery Local Plan policy CC06.

15. Before any phase of the development hereby permitted is occupied details of all boundary treatments for that phase, including gaps to allow the free movement of small mammals, shall first be submitted to and approved in writing by the local planning authority. The approved scheme shall be implemented prior to the first occupation of the development or phased as agreed in writing by the local planning authority. The scheme shall be maintained in the approved form for so long as the development remains on the site.

Reason: In the interests of amenity and highway safety. Relevant policy: Core Strategy policies CP1, CP3 and CP6

16. Prior to the commencement of the development of each phase, full details of both hard and soft landscape proposals for that phase shall be submitted to and approved in writing by the local planning authority. These details shall include, as appropriate, proposed finished floor levels or contours, means of enclosure, car parking layouts, other vehicle and pedestrian access and circulation areas, hard surfacing materials and minor artefacts and structure (e.g. furniture, play equipment, refuse or other storage units, signs, lighting, external services, etc). Soft landscaping details shall include planting plan, specification (including cultivation and other operations associated with plant and grass establishment), schedules of plants, noting species, planting sizes and proposed numbers/densities where appropriate, and implementation timetable.

All hard and soft landscape works for each approved phase shall be carried out in accordance with the approved details prior to the occupation of the respective phase of the development or in accordance with a timetable approved in writing by the local planning authority. Any trees or plants which, within a period of five years after planting, are removed, die or become seriously damaged or defective, shall be replaced in the next planting season with others of species, size and number as originally approved and permanently retained.

Reason: In the interests of visual amenity. Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21.

17. No trees, shrubs or hedges within the site which are shown as being retained on the approved plans shall be felled, uprooted wilfully damaged or destroyed, cut back in any way or removed without previous written consent of the local planning authority; any trees, shrubs or hedges removed without consent or dying or being severely damaged or becoming seriously diseased within 5 years from the completion of the development hereby permitted shall be replaced with trees, shrubs or hedge plants of similar size and species unless the local planning authority gives written consent to any variation.

Reason: To secure the protection throughout the time that development is being carried out, of trees, shrubs and hedges growing within the site which are of amenity value to the area. Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21

18. a) No development or other operation shall commence on either phase of the site until a scheme which provides for the retention and protection of trees, shrubs and hedges growing on or adjacent to that phase of the site in accordance with BS5837: 2012 has been submitted to and approved in writing by the local planning authority (the Approved Scheme); the tree protection measures approved shall be implemented in complete accordance with the Approved Scheme for the duration of the development (including, unless otherwise provided by the Approved Scheme) demolition, all site preparation work, tree felling, tree pruning, demolition works, soil moving, temporary access construction and or widening or any other operation involving use of motorised vehicles or construction machinery.
- b) No development on either phase of the site (including any tree felling, tree pruning, demolition works, soil moving, temporary access construction and or widening or any other operation involving use of motorised vehicles or construction machinery for that phase) shall commence until the local planning authority has been provided (by way of a written notice) with a period of no less than 7 working days to inspect the implementation of the measures identified in the Approved Scheme on-site.
- c) No excavations for services, storage of materials or machinery, parking of vehicles, deposit or excavation of soil or rubble, lighting of fires or disposal of liquids shall take place within an area designated as being fenced off or otherwise protected in the Approved Scheme.
- d) The fencing or other works which are part of the Approved Scheme shall not be moved or removed, temporarily or otherwise, until all works including external works have been completed and all equipment, machinery and surplus materials removed from the site, unless the prior approval of the local planning authority has first been sought and obtained.

Reason: To secure the protection throughout the time that the development is being carried out of trees shrubs or hedges growing within or adjacent to the site which are of amenity value to the area, and to allow for verification by the local planning authority that the necessary measures are in place before development and other works commence Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21

19. No phase of the development shall take place until full details of the drainage system for that phase of the site has been submitted to and approved in writing by the local planning authority. The details shall include:

a) Calculations demonstrating that there will be no flooding of pipes for events up to and including the 1 in 100 year flood event with a 40% allowance for climate change or how such flooding is managed on site.

b) A drainage strategy plan indicating the location and sizing of all SuDS features, with the base of any SuDS features located at least 1m above the seasonal high water table level or details of the SuDS feature showing lining layer to ensure that storage capacity will not be jeopardised by groundwater inflow.

c) Details of the maintenance schedule for the SuDS throughout the lifespan of the development and who will be responsible for private maintenance.

Reason: This is to prevent increased flood risk from surface water run-off. Relevant policy: NPPF Section 10 (Meeting the Challenge of Climate Change, Flooding and Coastal Change), Core Strategy policy CP1 and Managing Development Delivery Local Plan policies CC09 and CC10.

20. No deliveries shall be taken in or dispatched from the site outside the hours of 6:00AM to 11:00PM.

Reason: To safeguard residential amenities. Relevant policy: Core Strategy policies CP1 and CP3 and Managing Development Delivery Local Plan policies CC06 and TB20.

21. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) (England) Order 2015 (or any Order revoking and re-enacting that Order with or without modification), no external lighting shall be installed on the site or affixed to any buildings on the site unless otherwise agreed, in writing, by the local planning authority prior to installation on the site or affixed to any buildings on the site.

Reason: In the interests of amenity and highway safety. Relevant policy: NPPF Section 15 (Conserving and Enhancing the Natural Environment), Core Strategy policies CP1, CP3 [and CP6 / CP11 and Managing Development Delivery Local Plan policy TB21]

22. No development shall take place on either phase until a scheme to deal with any potential contamination on that phase of the site has been submitted to and approved in writing by the local planning authority. The scheme(s) shall include an investigation and assessment to identify the extent of contamination and the measures to be taken to avoid risk when the site is developed. Development shall not commence on each phase until the measures approved in the scheme for that phase have been implemented.

Reason: To ensure that any contamination of the site is identified at the outset to allow remediation to protect existing/proposed occupants of property on the site and/or adjacent land. Relevant policy: NPPF Section 15 (Conserving and Enhancing the Natural Environment) and Core Strategy policies CP1 & CP3.

23. The three first floors windows in the north-eastern elevation of the care home building hereby permitted shall be fitted with obscured glass (as annotated on the approved drawing no. A-731 76 Rev A) and shall be permanently so-retained. The windows shall be non-opening unless the parts of the window which can be

opened are more than 1.7 metres above the finished floor level of the room in which the window is installed and shall be permanently so-retained.

Reason: To safeguard the residential amenities of neighbouring properties.

Relevant policy: Core Strategy Policy CP3.

24. Prior to the commencement of development a scheme for generating 10% of the predicted energy requirement of the development from decentralised renewable and/or low carbon sources (as defined in the glossary of Planning Policy Statement: Planning and Climate Change (December 2007) or any subsequent version) shall be submitted to and approved in writing by the local planning authority. The approved scheme shall be implemented before the development is first occupied and shall remain operational for the lifetime of the development.

Reason: To ensure developments contribute to sustainable development. Relevant policy: NPPF Section 14 (Meeting the Challenge of Climate Change, Flooding and Coastal Change), Core Strategy policy CP1, Managing Development Delivery Local Plan policy CC05 & the Sustainable Design and Construction Supplementary Planning Document.

Informatives:

1. The development hereby permitted is liable to pay the Community Infrastructure Levy. This is a matter for the developer. The Liability Notice issued by Wokingham Borough Council will state the current chargeable amount. Anyone can formally assume liability to pay, but if no one does so then liability will rest with the landowner. There are certain legal requirements that must be complied with. For instance, whoever will pay the levy must submit an Assumption of Liability form and a Commencement Notice to Wokingham Borough Council prior to commencement of development. For more information see - <http://www.wokingham.gov.uk/planning/developers/cil/cil-processes/>.
2. This permission should be read in conjunction with the legal agreement under section 106 of the Town and Country Planning Act dated [INSERT], the obligations in which relate to this development.
3. The requisite Travel plan would need to comply with the latest national and local guidance:
 - 1) NPPF Section 9 (Sustainable Transport)
 - 2) The Essential Guide to Travel Planning (DfT, March 2008)
 - 3) Delivering Travel Plans Through the Planning Process (DfT, April 2009)
 - 4) A Guide on Travel Plans for Developers (DfT)
 - 5) Making Residential Travel Plans Work (DfT, June 2007) All accessible at: <http://www.dft.gov.uk/pgr/sustainable/travelplans/>
<https://www.gov.uk/government/policies/improving-local-transport>

Also:

WBC Transport Plan 3 and Active Travel Plan 2011 – 2026

WBC Workplace Travel Plan Guidance and Residential Travel Plan Guidance

Documents, covering workplace travel plans and residential travel plans provide local guidance and are available on the Borough's website.

4. The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including

planning policies and any representations that may have been received. This planning application has been the subject of positive and proactive discussions with the applicant in terms of:

- a full pre-application process was undertaken by the applicant;
- addressing concerns relating to proposed parking provision on site;
- agreeing Heads of Terms on a s106 legal agreement to secure necessary and relevant obligations to make the development acceptable in planning terms.

The decision to grant planning permission in accordance with the presumption in favour of sustainable development as set out in the NPPF is considered to be a positive outcome of these discussions.

PLANNING HISTORY		
Application Number	Proposal	Decision
F/1996/63222	Single Storey Extension To Nursing Home To Form Kitchen.	Approved 26/02/1996
F/2003/0670	Proposed second floor extension and new lift access to nursing home.	Approved 14/07/2004.
F/2009/1168	Proposed erection of 16 bed Dementia Care Support Unit.	Withdrawn 04/09/2009.
F/2009/1364	Proposed erection of a single storey flat roof extension to existing dining room plus new access ramps.	Approved 26/08/2009.
F/2015/0525	Proposed demolition of existing Class C2 nursing care wing and erection of new Class C2 wing for specialist dementia care alterations to site layout to provide on-site car parking plus landscaping works.	Approved 19/08/2016.
O/2015/0566	Outline application for the erection of 9 dwellings. (Access to be considered).	Approved 28/10/2016.
180194	Full application for proposed demolition of existing care home and redevelopment of the site to provide a replacement care home (72 bedroom) and 60 assisted living apartments, together with associated car parking, landscaping and amenity space.	Refused 18/06/2018

NB: Application 180194 was refused for the following reasons:

1. The proposed development, by reason of the design, size, scale, bulk, height, massing and close proximity to the site boundaries, would result in an incongruous overdevelopment of the site and would be an over-dominant and intrusive form of development out of keeping with the character and appearance of the area. The proposal would be contrary to policies CP1 and CP3 of the Core Strategy, CC01, CC02, TB21 and TB24 of the MDD Local Plan, the Borough Design Guide SPD and section 7 and the Core Planning Principles of the NPPF.

2. The proposed care home building, by reason of its design, size, height, scale, massing and close proximity to neighbouring residential dwellings, would be a dominant & unduly intrusive block of development that would result in an overbearing impact and a loss of privacy detrimentally impacting the amenity of the neighbouring occupiers to the east of the site. The proposed development is contrary to Core Strategy policy CP1 and CP3; MDD Local Plan Policy CC01; the Borough Design Guide SPD and section 7 and the core planning principles of the NPPF.
3. The proposed development will have detrimental impact on trees which are protected and have significant amenity value, due to the close proximity of the proposed care home building. The close proximity of the development to the mature trees will also result in pressure to fell or heavily prune trees comprising their presence and wellbeing. The proposed development is therefore contrary to Core Strategy policy CP1 and CP3, MDD Local Plan policy CC01, CC02, CC03 and TB21, The Borough Design Guide SPD, The British Standard 5837:2012, The Core Planning Principles and sections 7 and 11 of the NPPF and section 197 of the Town and Country Planning Act.
4. In the absence of satisfactory ecology information, the proposed development fails to demonstrate an acceptable impact on wildlife (bats and Great Crested Newts), contrary to policy CP1, CP3 and CP7 of the Core Strategy, CC01 and TB23 of the MDD Local Plan and the core planning principles and section 11 of the NPPF.
5. In the absence of a completed legal agreement, the proposal fails to secure opportunities for training, apprenticeships and other vocational initiatives to develop local employability skills contrary to Core Strategy Policies CP1 and CP4 and MDD policy TB12.

SUMMARY INFORMATION
Existing floorspace: 5,002 sqm Proposed floorspace: 9,865 sqm Net floorspace gain: 4,863 sqm
Existing care rooms lost: 88 Total rooms proposed: 105 Net gain of rooms: 17
Existing employees: 44FT and 32PT Proposed employees: 60FT and 40PT
The sites area is 1.97 ha
Existing parking spaces: 10 spaces
Proposed parking spaces: <ul style="list-style-type: none"> - Care home 30 spaces (+ 7 additional spaces to be secured via conditioned) - Assisted living 40 spaces

- Care home 30 spaces (+ 7 additional spaces to be secured via conditioned)
- Assisted living 40 spaces

CONSULTATION RESPONSES	
WBC Biodiversity	No objection subject to conditions
WBC Specialist Housing Group and Adult Social Care	No objection subject to a legal agreement to ensure that the proposed assisted living apartments (C2 use Class) fall within the council's extra care housing model.
WBC Drainage	No objection subject to conditions
WBC Environmental Health	No comments received
WBC Highways	No objection subject to an amended plan to demonstrate 7 additional parking spaces
WBC Tree & Landscape	No objection subject to conditions
WBC Cleaner & Greener (Waste Services)	No objection
Natural England	No comment
Crime Prevention Design Officer	No comments received
National Grid	No objection
Royal Berkshire Fire and Rescue	No comments received
Southern Gas Networks	No objection
SEE Power Distribution	No objection
Thames Water	No objection subject to conditions

REPRESENTATIONS
<p>Winersh Parish Council: Objects on the following grounds:</p> <ul style="list-style-type: none"> - Overlooking to residential properties at Mayfield Mews and Mill Lane. - Overshadowing and overbearing impacts on residential properties at Mayfield Mews. - Proposed trees to be removed currently provide a screen for residential properties in Mayfields. - Impact of additional motorised traffic especially during the morning changeover period at 8am. - It is understood that the current agreement with overflow parking in the Sindlesham Court area only relates to the current layout not the planned. If this is correct and overflow parking was not permitted in the Sindlesham Court area, then this additional parking would impact on nearby Mayfields St Marys and Winnersh Parish Council car park. - The proposed smoking shelter should be relocated away from residential dwellings. <p>Local Members: No comments received</p> <p>Neighbours: 8 objections received on the following grounds:</p> <ul style="list-style-type: none"> - The proposed building is still of excessive size, scale, bulk, height and massing within too close a proximity to the site boundaries and out of keeping with the looser and finer grain of development of the surrounding residential areas. - A further increase in mass and density is now proposed on the site of the former bowling green with little or no amenity for the residents. - Insufficient distances to boundaries (often less than 10m) are proposed. This would be overbearing and overlooking (especially from the first floor balconies proposed) on neighbouring houses. - Insufficient soft landscaping is proposed between property boundaries to protect amenity. - 24/7 operational requirements would impact on neighbours via increased noise.

- Oak Trees T33 and T34 should be retained as they add significant amenity value.
- Trees to the rear of 3 Mayfields should be retained within the scheme to protect from overlooking and light pollution.
- The site is not served by adequate bus stops.
- Existing pavements adjacent the care home and inadequate in terms of width.
- How will occupants of the existing 90 room care home be housed in the first proposed phase of just 45 rooms.
- Without a legally binding agreement to deliver the two phases in a tight time-bounded contract, the proposal represents two separate developments; i.e. a 45-bed care home and a 60 unit extra care housing scheme.
- Inadequate parking would be provided unless both phases are delivered within a short timeframe. Overspill parking would result in surrounding roads.
- Insufficient screening for the development between Bearwood Recreation Conservation Area.
- Pictures within the Photomontages Methodology are misleading as it suggests a smaller building than proposed.
- The proposal does not recognise the existing poor traffic situation on surrounding roads.
- Light pollution created by security lights on site would impact on residential amenity especially as the premises would be 24/7.
- Lack of public consultation by the applicants.
- A Bat survey should be provided.
- Paths for residents' gardens would give access to the rear property boundary of existing dwellings such as those in Mayfield Mews; this would also allow access to visitors which is a security risk. The permeability of this area should be reduced to protect from trespassers. The proposal makes no reference to Secure By Design.
- Proposed Elevation D (which is three storey in parts) would be much closer to the properties in Mayfield Mews. Balconies from Elevation D should be removed to protect privacy.
- The proposal represents a massive overdevelopment of the site.

APPLICANTS POINTS

- The buildings have been limited to 2 storeys in height, with façades broken up, stepped roofs, different elevational treatments; resulting in buildings which are sympathetic to the character of the area.
- Separation distances range between approx. 20-60m to neighbouring dwellings. The distances are sufficient to protect the privacy of neighbouring dwellings. The proposed assisted living units would be approx. 5m further away from the boundary than the previous application. Separation distances have been marked-up on the submitted plan ref. A- 731 56C.
- The parking and servicing areas are largely proposed to be positioned away from neighbouring dwellings.
- The loss of T33 and T34 has been justified within the submitted tree impact assessment report, in the interests of the retention of T35 which is of much greater value.
- The submitted townscape and visual impact assessment demonstrates no adverse impacts upon the site's wider character and setting.

- In terms of parking, the proposals would see a significant improvement in parking provision on site compared to existing arrangements.
- The proposals would be expected to result in one additional vehicle movement every 10 minutes during peak hours. This will not have a material or demonstrably adverse impact on existing traffic and safety conditions.
- Railings and gates would limit access to the garden areas providing security to residents and neighbours. Access to the site is overlooked by the entrances to the care home and assisted living building thereby providing additional natural surveillance.
- Footways are provided along Mole Road which continue along the B3030 to the Reading Road and Winnersh centre.
- Bat emergence surveys have been undertaken within existing buildings and found to have low potential to support roosting bats.
- During the preparation of the preceding and current applications extensive engagement has been had with both the Council and local community. This included a public exhibition.
- Elevation D (the eastern elevation of the Assisted Living building) is 2 storey in height. The proposed balconies would be approximately 20m from the end wall of properties on Mayfield Mews.
- In terms of the reduced number of care home rooms, the existing occupancy level will undergo a managed reduction over time and prior to its demolition.

PLANNING POLICY		
National Policy	NPPF	National Planning Policy Framework
Adopted Core Strategy DPD 2010	CP1	Sustainable Development
	CP2	Inclusive Communities
	CP3	General Principles for Development
	CP4	Infrastructure Requirements
	CP5	Housing mix, density and affordability
	CP6	Managing Travel Demand
	CP7	Biodiversity
	CP8	Thames Basin Heaths Special Protection Area
	CP9	Scale and Location of Development Proposals
Adopted Managing Development Delivery Local Plan 2014	CC01	Presumption in Favour of Sustainable Development
	CC02	Development Limits

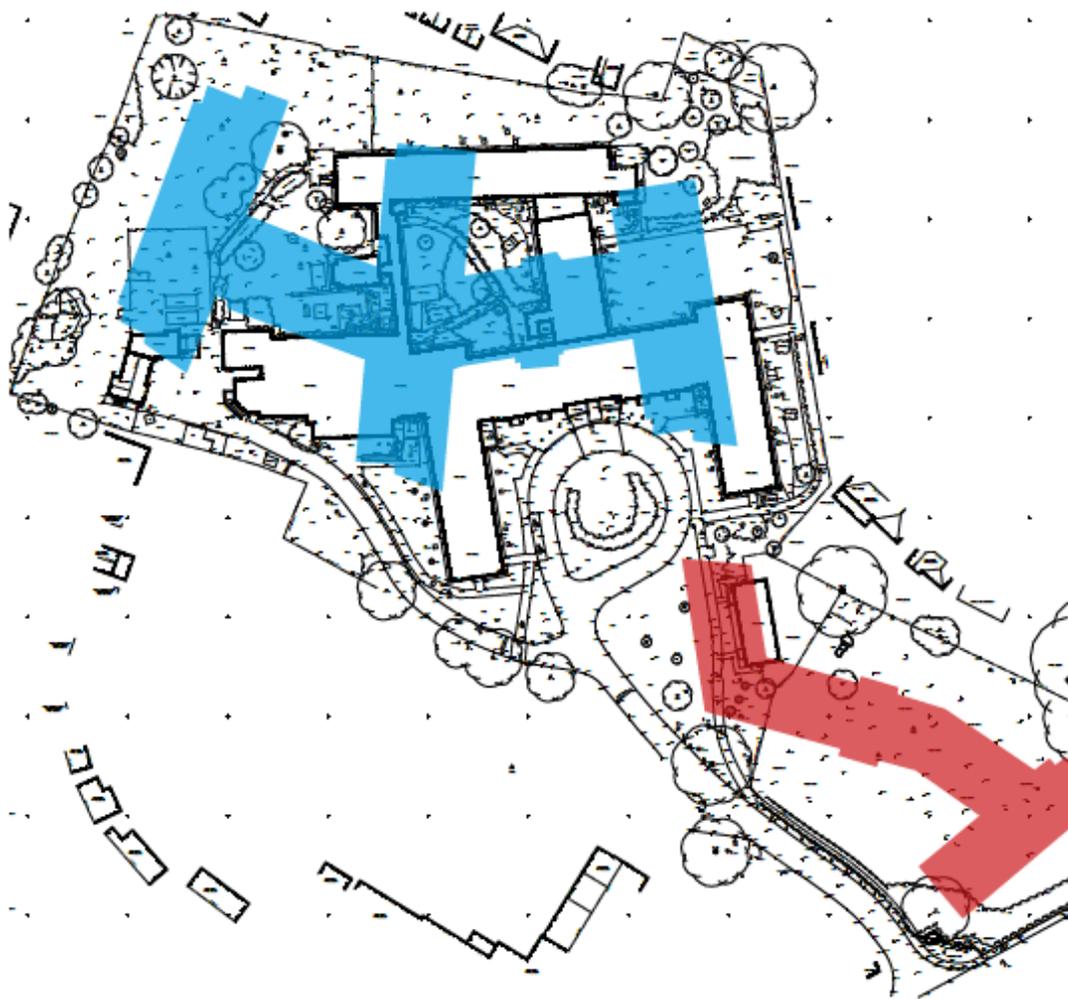
	CC03	Green Infrastructure, Trees and Landscaping
	CC04	Sustainable Design and Construction
	CC05	Renewable energy and decentralised energy networks
	CC06	Noise
	CC07	Parking
	CC09	Development and Flood Risk (from all sources)
	CC10	Sustainable Drainage
	TB05	Housing Mix
	TB12	Employment Skills Plan
	TB21	Landscape Character
	TB23	Biodiversity and Development
	TB24	Designated Heritage Assets
Supplementary Planning Documents (SPD)	BDG	Borough Design Guide Sustainable Design and Construction Older People Strategy
		DCLG – National Internal Space Standards

PLANNING ISSUES

Description of Development:

1. The application comprises of the demolition of the existing Lord Harris Court care home and construction of two separate buildings to provide a new 45 bedroom care home (Phase 1) and 60 assisted living apartments (Phase 2). Both buildings would utilise the existing vehicular access onto Mole Road. The proposal intends this phased approach to development in order to maintain the existing care home operations on site.
2. Phase 1 would include developing land on the existing bowling green area in the south-eastern corner of the site to build the new care home. The existing care home would then be demolished and existing residents relocated to the new care home. Phase 2 would involve the construction of a new assisted living building on the northern part of the site as indicated below.

Care Home (Phase 1) in red and Assisted Living Apartments (Phase 2) in blue



3. The proposal varies from the previously refused application (180194) in that the care home would now have 27 fewer rooms thereby resulting in two buildings with considerably reduced height, bulk and mass. The following comparison table outlines some of the differences between the two schemes in more detail.

	Care home building (Phase 1)	Assisted living building (Phase 2)
Refused application (180194)	Footprint: 1,580sqm Max ridge height: 11.9m 2 / 3 storey building	Footprint: 4,080sqm Max ridge height: 10m and 11.1m 2 storey building
Current application (190233)	Footprint: 1,220sqm (29% reduction) Max ridge height: 8.95m (2.95m reduction) 2 storey building	Footprint: 3,750sqm (9% reduction) Max ridge height: 9.55m and 9.98m (0.5m and 1.1m reduction) 2 storey building

4. The application site is located adjacent to, but outside of, the Sindlesham Conservation Area. To the north and east are residential dwellings that are typically suburban in character. To the south west of the site is Sindlesham Court which comprises of a large function hall and car park (refer to the appended Location Plan). The settlement edge is located to the west of the site and this area has a rural character and appearance.

Principle of Development:

5. The National Planning Policy Framework has an underlying presumption in favour of sustainable development which is carried through to the local Development Plan. The Managing Development Delivery Local Plan (MDD) Policy CC01 states that planning applications that accord with the policies in the Development Plan for Wokingham Borough will be approved without delay, unless material considerations indicate otherwise.
6. The proposed development is for the erection of a replacement care home and assisted living apartments. The proposed 'assisted living' units are considered to be the same 'extra care housing' as defined within the development plan – refer to TB09 below.
7. Core Strategy policy CP2 states that to *'ensure new development contributes to the provision of sustainable and inclusive communities (including the provision of community facilities) to meet long term needs, planning permission will be granted for proposals that address the requirements of... an ageing population, particularly in terms of housing, health and wellbeing; and people with special needs, including those with a physical, sensory or learning disability or problems accessing services.'*
8. Policy TB09 of the MDD Local Plan states that *'the Council will in principle support proposals which provide for the following types of accommodation to provide for peoples' needs over a lifetime:*
 - a) *Extra care home*
 - b) *Dementia extra care units*
 - c) *Enhanced sheltered schemes*
 - d) *Proposals that allow the elderly and those with disabilities to remain in their own homes or purpose built accommodation.'*
9. Paragraph 2.41 of the Core Strategy states *'over the next 10 years, an additional 400 units of extra care housing of mixed tenure will be required'...* *'We will encourage existing providers of residential and nursing homes to diversify and modernise so they are able to offer options in line with our strategic aspirations'.*
10. Extra care units provide an enhanced level of care and there is increasing national evidence of a more positive outcome for older people in extra care. As such, traditional care homes are less likely to be supported as they do not deliver the type of care that is promoted in policy TB09 of the MDD Local Plan.
11. The proposal seeks to demolish the existing 88 bed care home and erect a new care home with 45 bed spaces along with an additional 60 assisted living units. Whilst care homes do not provide the type of care which is favoured by the Council, the proposal will be replacing an existing care home facility and will result in 43 fewer bed spaces in favour of providing extra care units. This

reduction of care home places and the creation of extra care units is considered to accord with the development plan and the Council's Older People Strategy. The applicant has agreed to enter into a legal agreement to ensure the assisted living apartments falls (C2 use Class) within the council's extra care housing model.

12. Consideration needs to be given as to whether the assisted living accommodation provided falls within the council's extra care housing model. Wokingham Council's Older People's Housing Strategy 2014-19 defines Extra Care as the following:

'Self-contained apartments, each one with a bedroom, a living room, bathroom and kitchen either for rent or to purchase. A care team is onsite 24 hours a day and personal care and support for individuals is tailored to each persons assessed needs. The care service will be registered with the Care Quality Commission for 'Extra Care Housing Services'.

Extra care housing offers a community with facilities for people to socialise and pursue their hobbies and interests together with catering facilities on site providing meals to purchase. Extra care housing gives people control over their income, care and support and assistance in emergencies.'

13. The proposed model of care is described in paragraph 7.13 of the submitted Planning Statement. This has been considered by the Housing Team who have advised that the proposed care model does meet the definition of extra care housing referred to above. The development therefore accords with policy TB09 in this regard.
14. The application site is in the defined settlement boundary of Sindlesham which is classed as a limited development location in the development plan. Limited development locations are defined in the Core Strategy as containing a basic range of services and facilities and are physically and socially cohesive. Within the development limits of these settlements, limited development would be acceptable subject to other material considerations and compliance with the other development plan policies.
15. Policy CC02 of the MDD sets out the development limits for each settlement as defined on the policies map and therefore replaces the proposals map adopted through the Core Strategy, as per the requirement of policy CP9. Policy CP9 sets out that development proposals located within development limits will be acceptable in principle, having regard to the service provisions associated with the major, modest and limited categories. As the site is within a limited development location, the proposal is acceptable in principle.

Character of the Area:

16. Policy CP1 of the Core Strategy states that planning permission will be granted for development proposals that 'maintain or enhance the high quality of the environment'. Policy CP3 of the Core Strategy states planning permission will be granted if development is 'of an appropriate scale of activity, mass, layout, built form, height, materials and character to the area together with a high quality of design' and contributes 'to a sense of place in the buildings and spaces themselves and in the way they integrate with their surroundings (especially

existing dwellings) including the use of appropriate landscaping'. The supporting text to policy CP3 also sets out that development should be of a high standard of design that can integrate with the character of the area as this is important to achieving sustainable development.

17. Section 7 of the NPPF states that 'permission should be refused for development of poor design that fails to take the opportunities available for improving the character and quality of an area and the way it functions' and 'the Government attaches great importance to the design of the built environment. Good design is a key aspect of sustainable development, is indivisible from good planning, and should contribute positively to making places better for people'.

18. The site is surrounded by residential development to the north and east. To the south west of the site is Sindlesham Court. The settlement edge lies immediately to the west of the site and this area has a more rural character and appearance. Mole Road to the south also defines the settlement edge and the beginning of the countryside. The existing main building was erected in the 1970's and comprises of yellow brick and a mixture of flat and mansard roof forms. The building is composed of a central core with a series of projecting wings. The existing building is single and two storeys in height with some sections incorporating additional accommodation in the mansard roof void with dormer windows. The south east corner of the site is undeveloped.

- Care home block (Phase 1)

19. The proposed care home building would be located in the south-eastern corner of the site. This part of the site is predominantly undeveloped and was used a bowling green prior to it becoming overgrown. Compared to the refused application (180194), the footprint of the proposed care block building has been reduced by 29% - mainly by virtue of the having its entire second floor removed from the design, thereby reducing the maximum ridge height by approximately 3m to 8.95 metres, and by entirely removing the previous 2-storey section that extended towards the middle of the site; comparison extracts from elevations and 3-dimensional images from both schemes are provided below. The proposal would utilise modern vernacular and will include a dual pitch roof similar to the previous proposal.

Refused application (180194): SE elevation to care home facing Mole Road



ELEVATION E

Current application (190233): SE elevation to care home facing Mole Road



ELEVATION F WITHOUT TREES ON MOLE ROAD

3 dimensional view of Refused application (180194)



3 dimensional view of current application (190233)



20. Given its significantly reduced height and bulk, the proposed care home building would be in keeping with the Mole Road streetscene, especially when considering the existing mature soft landscaping along this site frontage that would help soften the appearance of the development when viewed from the Countryside and Conservation Area to the east.
21. The proposed care home building, while obviously still having a much larger footprint than residential properties in the vicinity; e.g. those in Mayfields (including three new properties to the south of Mayfields) and Barkham Place, would be considerably more commensurate with the height and 2-storey appearance of these neighbouring dwellings than that of the refused scheme (180194). Moreover, the reduced scale, bulk and height of the proposed care home building would help it sit appropriately within the site itself, having a suitable amount of space around the building to provide for an appropriate setting thereby avoiding a cramped appearance in urban design terms.
22. Moreover, it should be noted that outline permission ref: O/2015/0566 granted 9 properties in this location and also represents that the principle of two-storey development is acceptable on this site and remains the applicants 'fall back' position for this part of the site.
- Assisted living block (Phase 2)
23. The proposed assisted living block would be located in the northern part of the site where the existing care home is situated. The tallest part of the existing building is the central core with a mansard roof and this measures approximately 9.5 metres high. The two storey wings off this central core have a height of approximately 7 metres. The north and west wing of the buildings are single storey in height.
24. As indicated above, the proposed building has been reduced in scale from the previous proposal so that the maximum ridge height has been reduced by up to 1.1m to between 9.55m and 9.98m. This is much more commensurate with the overall height of the existing building on site. Moreover, the overall footprint of the proposed assisted living block has been reduced by 9% compared to the previous application (180194).
25. The previous proposals relationship with Mayfield Mews to the east was of particular concern. The current east elevation of the building does not span across the gap in between the properties in Mayfield Mews, however part of the central core located further into the site is visible from this viewpoint. At this point, the proposed building would still have an east elevation approximately 3 metres higher than the existing structure, measuring 10 metres to ridge height.
26. However, this latest proposal has resulted in the assisted living block being set back a further 3.5m than the previous scheme (180194) and 10m further back than the existing building. The eastern elevation of the assisted living block would also be broken up by the use of vertical design elements to avoid any continued use of brickwork at upper floor level; see illustrative CGI below.

Extract from DAS: Illustrative CGI from Mayfield Mews looking towards LHC



27. While the proposed east elevation would span across this gap when viewed from Mayfield Mews this is considered to be an acceptable design solution given that it effectively provides a focal point for the vista looking westwards into the site; and also when considering that the proposed building would now be provided with a greater setting away from this boundary than both the current building and previously refused scheme (180194). As such, the overall height and bulk of the proposed assisted living block is considered acceptable when viewed from Mayfield Mews.
28. To the west of the application site is an access track serving other residential properties. This track defines the edge of the settlement boundary and there is a more rural character and appearance in this location although the tranquillity of this area is somewhat compromised by noise from the M4. Compared to the refused scheme (180194), the western elevation of the proposed assisted living block has been pulled a further approximately 2.5m away from this boundary which allows for a total of 15m between the site boundary and the proposed development.
29. While the existing care home is positioned significantly further away from this western edge and the proposed development would no doubt create a more prominent structure when viewed from outside of the site, significant longer distance views to this part of the site do not exist given the proximity of the nearby M4. As such, it is not considered that the proposed development would create an unacceptably hard urbanising edge to settlement which would conflict with the rural character of the countryside to the west in this particular instance; as viewed shared by the Council's Trees and Landscape Officer (refer to the Landscape and Trees section of this report below).

30. Therefore, while the footprint of the proposed assisted living building would be significantly larger than the existing care home, it is considered that this would not constitute a substantive reason for refusal in its own right and complies with the relevant planning policies in urban design terms.

31. Overall, while both the proposed care home and assisted living block would be significantly larger than the surrounding dwellings, the finer grain of these existing residential properties should not entirely dictate how this site is redeveloped in terms of bulk, massing and the height of any new buildings. The proposal is considerably reduced in terms its bulk, massing and height compared to the previously refused scheme (180194) and would provide a suitable design solution for redevelopment of this site which would suitably integrate with its surroundings including its relationship which adjacent land located with the countryside and the Bearwood Recreation Conservation Area.

Heritage Assets

32. Policy TB24 of the MDD Local Plan requires the conservation and enhancement of Listed Buildings and Conservation Areas, including their views and setting.

33. The application site lies outside but adjacent to the Sindlesham Conservation Area, which contains a park/recreational ground and extends further to the east and south to include residential dwellings and Bearwood College. As set out above, it is considered that the proposed development (in particular the care home aspect of the proposal) has been suitably reduced in bulk, mass and height compared to the previous application (180194) so that it would not have a detrimental impact on the character and appearance of the locality including views experienced from and to Mole Road which lies within the Conservation Area. Therefore, the proposed development would suitably preserve the character or appearance of the Conservation Area as required by policy TB24 of the MDD Local Plan.

34. The Masonic Centre to the south west of the site is also known as Sindlesham Court and this building is Grade II Listed. The listed section of the building specifically excludes the large 20th century extensions to the north elevation. The front elevation of the listed section of the building faces to the west, away from the application site. There is a large car park to the north and an access road to the east and this subdivides the Masonic Centre from the site. Due to the 20th century development surrounding the listed building and its distances and orientation away from the application site, the proposal is not considered to result in any negative impact on this heritage asset.

Residential Amenities:

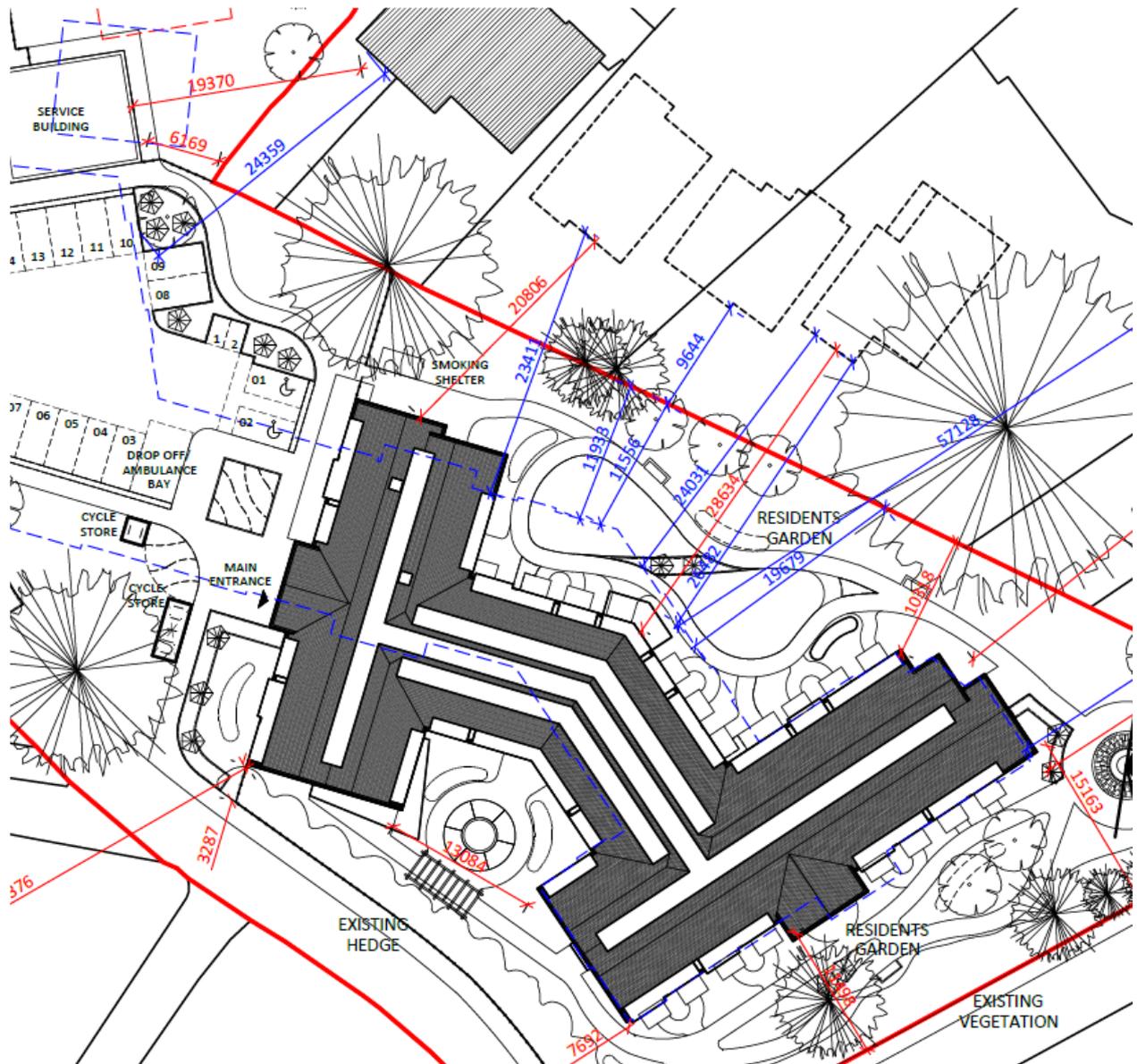
35. Policy CP3 of the Core Strategy states that development should be of 'an appropriate scale of activity, mass, layout, built form, height, materials and character to the area together with a high quality of design without detriment to the amenities of adjoining land users including open spaces or occupiers and their quality of life'.

- **Care home block (Phase 1) – Impact on May Fields / Barkham Mews**

36. The proposed care home building would be positioned in the south east corner of the site. Located to the north-east of the proposed care home building are

residential dwellings fronting onto May Fields. Recent residential 'infill' development has also been constructed to the rear of these properties which include four terraced properties located in Barkham Mews and three detached dwellings which are accessed via May Fields. These neighbouring dwellings are shown on the extract of the proposed Site Plan below (the three new detached dwellings under construction being annotated by hatched outlines) with the outline of the previous refused building and back-to-back distances in blue. The back-to-back distances annotated in red indicate those now proposed.

Extract of Proposed Site Plan (care home) showing distances from neighbours



37. As previously mentioned, the proposed care home has been significantly reduced in bulk and height and would now be limited to an entirely 2-storey building with the majority of it pulled further away from the nearest neighbouring properties to the north-east, as indicated in the above extract. This would result in the proposed back-to-back distances being significantly extended to approximately 28m in many instances. Even where private balconies are proposed in the north-east elevations (refer to extract of proposed Elevation H

below), this is considered a sufficient distance to avoid any significant loss of privacy to these neighbouring properties. Moreover, additional soft landscaping (full details of which are recommended to be secured via condition) along this boundary would further help to both protect privacy and soften the appearance of the building.

38. The north-westerly arm of the proposed care building would result in a shorter back-to-back distance of approximately 21m. While this is marginally under the normal 22m required for back-to-back relationships, as advocated with the Council's Borough Design Guide, it should be noted that the first floor of this part of the building relates to a stairwell and maintenance store and not habitable space. The windows in this elevation are also proposed to be obscure glazed with restricted opening (refer to extract of proposed Elevation H below) and a condition is recommended to secure these windows are maintained in this manner. As such, both existing and proposed residences would be suitably protected from any excessive overlooking and therefore no significant loss of privacy is considered to occur as a result of the proposed care home building.

Extract of Proposed north-east elevation of care home building



ELEVATION H

39. The Borough Design Guide SPD recommends that where new development faces the elevation of another dwelling with a window to a habitable room, draw an imaginary 25 degree vertical angle from the centre line of the window and ensure this remains unobstructed by development. The above north-westerly arm of the proposed care home would have a gable feature measuring 9m in height. When drawing a 25 degree line from the ground floor windows of the property opposite this feature, the proposed gable would not obstruct this line. While some loss of light may be lost from the rear of these neighbouring houses towards the end of daylight hours, this would not constitute any significant overshadowing or loss of daylight that would warrant refusal of the application in its own right.

40. This separation distance is also considered to be sufficient in order to avoid the care home building appearing as an overly oppressive or overbearing feature when viewed from the rear of neighbouring dwellings or their private rear gardens. The design of the roof has been lowered in order to minimise the bulk of the building while the elevational treatment is suitably broken up to define individual units rather it appearing solely as one large unbroken mass.

41. In light of the above positive changes to the design of the new care home building, this proposal would adequately protect the amenities of existing neighbouring residents in terms of privacy and overlooking, overshadowing and potential loss of daylight and an perceived overbearing physical presence of the building. In this respect, the proposal overcomes reason for refusal no. 2 of application 180194.

- **Assisted living block (Phase 2) – Impact on Mayfield Mews and properties to the west and north of the site**

42. Mayfield Mews comprises of two blocks of 4 and 5 terrace houses facing onto a central parking area. The western flank elevation of these houses runs parallel to the boundary with the application site. The existing care home building has a single-storey section that extends across part of this boundary.

43. The proposed assisted living building would be larger than the existing care home when viewed from the east. It would extend across the length of Mayfield Mews, effectively bridging the visual gap from the east between these houses. At this point, the assisted living block (refer to Elevation D - East Elevation on the appended Assisted Living Elevations – drawing no. 85 Rev A) would be 10 metres in height, approximately 3 metres higher than the existing part of the care home building in this location. However, the proposed assisted living block would be positioned approximately 22 metres from the nearest flank elevation with the properties in Mayfield Mews. This back-to-side separation distance significantly exceeds the minimum guidance of 12 metres advocated within the Council's Borough Design Guide.

44. The assisted car home block would have balconies that will allow a degree of overlooking into the properties in Mayfield Mews. However, given the proposed separation distances, and the location of the existing care home, it is considered that there would not be a detrimental impact on amenity of these neighbouring residents.

45. To the east and north of the proposed assisted living building are a series of dwellings that are accessed by a track off Mill Lane (refer to extract of the Assisted Living plan below). There are a pair of semi-detached dwellings on the opposite side of this track called Larkrise and Fridale. These properties will face the front elevation of the western wing of the extra care housing. The building would be set approximately 26 metres from the front elevation of these houses which significantly exceeds the minimum guidance of 10 metres. There is scrub planting that forms a piecemeal hedgerow along the track and this would partially screen outlook from development. Whilst the building will be seen by these neighbouring residents, it is not considered that there would be a detrimental impact on amenity.

Extract of Proposed Site Plan (Assisted Living) showing distances from neighbours



46. There are two detached properties to the north which back onto to the site. The nearest section of the existing building is a single structure. Permission has been granted in 2015 for a 2 storey extension to the rear of the care home which is yet to be implemented. The nearest section of the new building will be the flank elevations of projecting wings and these will have a ridge height of approximately 10 metres. At its nearest point, the proposed building will be approximately 24 metres away from the rear of these houses and this meets the minimum recommended side to rear separation distance in the SPD of 12 metres. Whilst the structure is very large, other parts of the building are set back from this boundary. Overall, it is considered that the development will not have an unacceptable impact on the amenity of these residents.
47. There are two properties immediately to the north and south of the western wing of the proposed assisted living building. This wing would broadly align with these houses and will be set away from the side boundaries. It is considered that the proposal will have an acceptable impact to the amenity of these residents.
48. There are several dwellings to the south west that front onto Mill Lane and Summer Close. The development will be located satisfactorily away from these houses and will not detrimentally impact the amenity of these occupiers. The residents to the properties to the north of May Fields will not be detrimentally impact by the proposed development as they are located at least 30 metres from the boundary of the site.
49. The assisted living block of the previous scheme (180194) was considered acceptable in terms of its potential impact on neighbour's amenity and this latest

proposal further improves relationships with neighbouring property boundaries and is therefore also acceptable in this respect.

Access and Movement:

50. Policy CC07 and Appendix 2 of the MDD Local Plan stipulates minimum off street parking standards, including provision for charging facilities.

51. P3 of the Borough Design Guide SPD notes that parking spaces should be safe and convenient, close to the dwelling and sited to minimise impact upon safety. Visibility splays and swept path analysis are also required to be specified on the plans.

- Traffic Impact

52. The Council's Highways team has assessed this application and advises that it is unlikely that this proposed development would have an adverse impact on the highway network.

- Parking

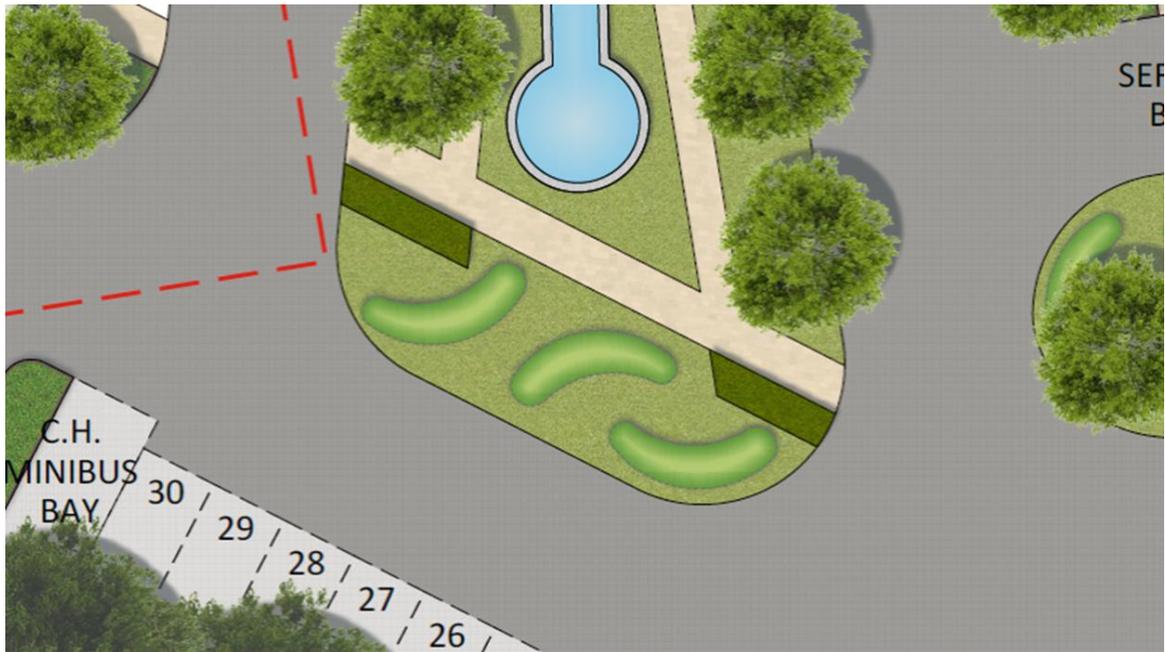
53. The Borough's adopted MDD sets out the parking standards for a residential care home. There are no standards for assisted living and normally the Borough would recommend using those for residential flats.

54. The applicant intends 30 car parking spaces for the proposed 45 bed care home. For these spaces to be in line with the Borough standards, the Council's Highways team would need to know the likely level of FTE employees and their shift pattern. For the assisted living apartments, WBC Highways also required information on the type of person likely to accommodate these apartments. Otherwise, the Council's Highways team would treat these as normal apartments for parking standards.

55. Hence, the applicant has provided additional information has from similar assisted living sites. Based on the sites contained within the Transport Statement submitted to support this application, there would need to be 31 spaces. It is proposed that there would be 40 spaces. This is considered acceptable. However, there is still the requirement for the care home to meet parking standards. As such, an additional 7 spaces need to be provided over and above that currently proposed. Once provided this would make the parking acceptable for this type of development. The applicant has agreed in principle to provide these additional 7 spaces which are recommended to be detailed on an amended plan to be secured via condition.

56. There appears to be sufficient space on the central island opposite the main parking area to provide these additional parking spaces; as highlighted in the Site Plan extract below. This space currently contains no significant tree planting or landscaping and its use for additional parking would therefore not be detrimental to the visual amenity of the surrounding area.

Site Plan extract showing potential area for additional parking spaces



57. The number of cycle spaces is in line with Borough standards. The location of these spaces is also acceptable.

- Sustainability

58. A sustainability assessment has been carried out to support this application. There are few local services close to the site, however there are good bus services and local services on Reading Road a kilometre away. Due to the nature of the site, the Council's Highways team would normally have expected an assessment of the pedestrian route from Reading Road and the site to identify whether or not improvements would be required. However, the Highways have looked at the walking environment to Reading Road and the pavements are considered acceptable and would not discourage pedestrian access particularly for staff.

- Highway safety

59. A swept path analysis has been submitted for a refuse lorry. The manoeuvring of this vehicle would be tight but as it rounds the garden area to approach the loading bay but would still be compliant in highway safety terms.

60. The visibility splays of the existing access are within standards.

61. Due to the size and location of this proposed development a framework construction method statement would need to be submitted which is recommended to be secured via condition.

Flooding and Drainage:

62. The application site is within Flood Zone 1. The development will involve an increase in built form on the land and the loss of previous undeveloped parts of the site. A Flood Risk Assessment (FRA) has been submitted with the application upon which the Council's Drainage officer has provided the following comments:

“The submitted FRA describes the existing drainage of the site as split into two parts with a combined discharge rate of 31.97l/s to an existing ditch to the north west of the site. The proposal is for a two phased development with combined discharge limited at 22.3l/s (a 30% betterment to the existing scenario) with attenuation provided to cater for the 1 in 100 year flood event with a 40% allowance for climate change.

Phase 1 proposes maintaining the existing care home as well as well as building a smaller care home to the east of the access road with discharge controlled at 5l/s to the existing water course to the north west. This is a an improvement to the existing discharge rate of 5.63l/s

Phase 2 proposes demolition of the existing care home and construction of a larger care home with discharged controlled at 17.3l/s into an existing pipe which discharges into the watercourse to the west of the site.

Development proposes 415m³ of attenuation crate storage as represented on the engineering overall layout drawing. This is as a result of the infiltration drainage not being favourable for this site as demonstrated geotechnical assessment. The FRA also mentions that all SuDS will be privately maintained”.

63. In light of the above, the Council’s Drainage officer raises no objection to this application subject to pre-commencement conditions to secure the following:
64. Calculations demonstrating that there will be no flooding of pipes for events up to and including the 1 in 100 year flood event with a 40% allowance for climate change or how such flooding is managed on site.
65. A drainage strategy plan indicating the location and sizing of all SuDS features, with the base of any SuDS features located at least 1m above the seasonal high water table level or details of the SuDS feature showing lining layer to ensure that storage capacity will not jeopardised by groundwater inflow.
66. Details of the maintenance schedule for the SuDS throughout the lifespan of the development and who will be responsible for private maintenance.
67. Foul drainage is proposed to be pumped into the existing foul network and this needs to be agreed with the utilities provider. Thames Water has identified an current inability of the existing foul water network infrastructure to accommodate the needs of the development so they have requested a pre-occupation condition to ensure that either: all wastewater network upgrades required to accommodate the additional flows from the development have been completed; or, an infrastructure phasing plan is agreed with Thames Water to allow the additional accommodation to be occupied.
68. Subject to adherence with the above conditions, the proposal is considered acceptable in drainage and flooding terms and is policy compliant in this respect.

Landscape and Trees:

69. Section 197 of the Town and Country Planning Act sets out the importance of trees within the planning process: ‘Planning permission to include appropriate provision for preservation and planting of trees. It shall be the duty of the local

planning authority... to ensure, whenever it is appropriate, that in granting planning permission for any development adequate provision is made, by the imposition of conditions, for the preservation or planting of trees.'

70. Policy CP3 of the Core Strategy states that planning permission will be granted for proposals that 'maintain or enhance the ability of the site to support fauna and flora including protected species'. Policy CC03 of the MDD Local Plan states that development proposals should demonstrate how they have considered and achieved to 'protect and retain existing trees, hedges and other landscape features'.
71. Section 4.2 of the Borough Design Guide states 'new housing should respond to its context, in particular taking opportunities presented by the physical characteristics of the site and surroundings, including incorporating existing features of value into the proposals, such as existing buildings, structures, landscape, rivers, ponds and wetlands, trees and hedgerows'. Section 4.6 states 'where there are existing landscape features of value, such as hedgerows or trees, these should be retained and incorporated into landscape proposals for a development. Existing trees should be protected in accordance with BS5837 both in terms of the layout of new development and during construction.'
72. The site is located in the settlement of Sindlesham adjacent to the Bearwood Recreation Ground which lies within the countryside and the Sindlesham Conservation Area. There are three TPO oak trees on or close to the eastern boundary of the site. A large oak adjacent to the site access road is also protected by a TPO.
73. The Tree and Landscape Officer has considered the application and provided the following comments:

"The revised layout and design of the care home and the assisted living building is a substantial improvement to the previous application in terms of the reduction in size, massing and extent of the buildings within the site. I also consider the reduction of the originally proposed three storey care home to two storeys adjacent to Mole Road is more in keeping with the existing built form of the surrounding settlement and Conservation Area.

A Townscape and Visual Assessment has been submitted with the application along with a set of Photomontages for the proposal to support the development. I have no concerns over the findings or conclusion of the assessment in which there is little adverse impacts of the development on the adjacent Conservation Area and Landscape Character Areas. The visual effects are also minimised due to the design and location of the buildings within the site and the retention of the existing mature vegetation surrounding the site including the TPO trees.

An Arboricultural Survey and Planning Integration Report has been provided and discusses the proposed relationship of the care home with the existing category A TPO trees within and adjacent to the site. None of the five category A Oak trees will be removed... I have considered the proposed removal of trees T33 & T34 which is discussed in the Arboricultural Report and conclude I have no objection to this. T33 & T34 currently compromise the growth of T35, which is considered to be a high quality tree of national importance.

The revised location and orientation of the care home in relation to T3 is a considerable improvement and the Arboricultural Report also discusses how ground conditions can be improved for this tree, some of which I believe have already been implemented.

In my opinion the proposed revised location and positioning of the care home within the site will allow for the successful retention of the most important trees within and adjacent to the site as well as other vegetation on the site boundaries. If the application is approved we will require the submission of an Arboricultural Method Statement in accordance with BS5837:2012 which will need to take account of the following:

- Recommendations raised in paragraph 11.1 of Arboricultural Report including the location of the proposed utilities, underground cellular storage for the care home and French drain on the eastern boundary of the assisted living units.

- Provision of more detailed information on the above ground surfacing especially in relation to T3.

- Possible use of new planting bed around T3 as a rain garden – my suggestion as part of the SuDS Strategy.

- Additional tree protection fencing shown on the Tree Protection Plan for the RPA of T35 within the site, existing vegetation to be retained fronting onto Mole Road and retention of the Beech hedgerow along the site access”.

74. The Council's Tree and Landscape Officer has also recommended the following conditions should the application be approved:

- CC1 – Boundary Treatments
- CL5 – Landscaping (large scale)
- CL6 – Retention of existing trees/shrubs/hedges
- CL8 – Protection of Trees (major)

75. Therefore, subject to adherence with the above conditions, the proposal is acceptable in planning policy terms in respect of tree and landscape issues as it overcomes the previous concerns identified in reasons for refusal no. 3 of planning application 180194.

Environmental Health issues:

76. Policy CC06 and Appendix 1 of the MDD Local Plan requires that development protect noise sensitive receptors from noise impact. Policy CP3 of the Core Strategy aims to protect neighbouring amenity.

77. The proposed development would result in an increase of elderly residents on the site and will result in building on previously undeveloped land. No comments have been received from the Council's Environmental Health Team but in respect of the previous application (180194) they raised no objection to the redevelopment of this site but recommended a condition to ensure that contamination, if found, should be remediated safely.

78. The site is surrounded by neighbouring dwellings and the proposed development would be set a sufficient distance away from these dwellings in order to avoid any unacceptable amenity concerns as addressed earlier in this report. Moreover, conditions are recommended to restrict delivery times outside of antisocial hours and to minimise noise and disturbance during construction works in order to protect residential amenity during the construction phases.

79. Objections have been received in respect of noise associated with the proposed use and its 24-7 operation. However, the current use already provides 24 hour on-site nursing care and the overall net increase in activities is not considered to result in any significant additional noise concerns because much of the same operations already exist; e.g. general deliveries, laundry collection and bin removals. Moreover, it is in the interest of the applicant to manage their site responsibly in order to avoid any complaints from neighbouring residents. If any 'statutory noise nuisance' complaints were received (whether in respect of the current or proposed use) by the Council's Environmental Health team these could be dealt with outside of planning legislation; e.g. under the Environmental Protection Act (1990).

80. Objections in respect of both noise and odour were also raised on the previous application (180194) and specifically about the location of the proposed refuse area/service building. The proposed service building (11m x 9.5m) would serve both the care home and assisted living units which is the reason the applicant proposes it to be located centrally within the site. However, compared to the previous scheme, the current proposal moves this service building a further 4m away from the rear of neighbouring properties at Barkham Place and Mayfield Mews (refer to extract of the proposed Masterplan below).

Fig. x: Extract of Masterplan showing location of proposed service building



81. In light of this additional set off distance and the fact that the service building would be brick built with a flat roof, the extent to which both noise and odour

would impact on neighbouring residential amenities would be limited provided the aforementioned recommended conditions are adhered to. Therefore, it is considered that the proposal accords with the relevant planning policies in this respect.

Amenity Space for future occupiers and internal space standards:

82. The proposed assisted living apartments would offer future residents varying levels of care. The flexibility of these apartments to cater for the individual needs of an occupant are reliant on the level of space and amenity the accommodation provides. The application was considered by the Council's specialist housing group who raised no objection from the group was raised on this point.

83. The proposed assisted living building would provide 21 no. 1-bedroom and 39 no. 2-bedroom units. The one bedroom properties would have floor areas of between 60-74sqm and the 2 bedroom units would range between 70 and 77sqm. Although the units are not open market dwellings, internal floor space exceeds the minimum unit sizes set out the Nationally Described Space Standards.

84. The assisted living apartments would be provided with communal living spaces, dining areas and outdoor amenity space around the building. The borough has no specific requirements regarding outdoor amenity for flats or apartments. Whilst an objection is raised with the regard to the overdevelopment of the site and the cramped appearance, it is considered that sufficient on site amenity space would be provided to the future residents. It is also note that there is a large park in very close proximity to the site which will be accessible to residents.

85. In terms of the objection raised regard to security risks associated with the proposed residents' gardens, it should be noted that the permeability of these areas will be controlled via the use of suitable boundary treatments; e.g. railings, gates, fencing etc. in order to restrict access to residents and visitors. Moreover, these areas would be overlooked by the entrances to the care home and assisted living buildings thereby providing additional natural surveillance.

Ecology:

86. Policy CP7 of the Core Strategy states that development proposals that may harm habitats or species of principle importance or features of the landscape that are of major importance for wild flora and fauna (including wildlife and river corridors), will only be permitted if it has been clearly demonstrated that the need for the proposal outweighs the need to safeguard the nature conservation importance; that no alternative sites that would result in less or no harm is available which will meet the need, and:

mitigation measures can be put in place to prevent damaging impacts; or

ii) Appropriate compensation measures to offset the scale and kind of losses are provided.

Policy TB23 of the MDD Local Plan further states that development proposals should demonstrate how they:

a) Provide opportunities, including through design, layout and landscaping to incorporate new biodiversity features or enhance existing.

b) Provide appropriate buffer zones between development proposals and designated sites as well as habitats and species of principle importance for nature conservation.

c) Ensure that all existing and new developments are ecologically permeable through the protection of existing and the provision of new continuous wildlife corridors, which shall be integrated and linked to the wider green infrastructure network.

87. The Council's Ecologist has considered the proposal in light of the updated ecological report submitted to support this application. It was concluded that sufficient information has now been provided to confirm that protected species; i.e. Great Crested Newts and Bats would not be affected by the development. As such, the proposal accords with the above relevant development plan policies and therefore overcomes reasons for refusal no. 4 of planning application 180194.

88. In addition, a revised boundary treatment plan to show 'hedgehog-friendly' fencing was requested by the Council's Ecologist. The applicant has confirmed that this will be provided before your meeting, the details of which will be provided in an update report.

Sustainable Design/Construction:

89. Policy CC05 of the MDD Local Plan encourages renewable energy and decentralised energy networks, with encouragement of decentralised energy systems and a minimum 10% reduction in carbon emissions for developments or in excess of 1000m².

90. The application is supported by an Energy and Sustainability Statement which analyses the potential energy sources for the proposed development and the ability to first reduce energy requirements, followed by improving energy efficiency and implementing renewable technologies if required.

91. The report concludes that the development could result in a 16% reduction for both the care home and the assisted living building with a combination of the above measures. Hence, the principle of meeting the requirements of Policy CC05 has been demonstrated and a condition is recommended in order to secure a minimum reduction of 10% in carbon emissions on the final scheme.

Community Infrastructure Levy (CIL):

92. *Community Infrastructure Levy:* The Council formally adopted a charging schedule in accordance with the Community Infrastructure Level on 06.04.2015. The development would be liable to pay the Community Infrastructure Levy.

Employment Skills:

93. Policy TB12 of the WBC MDD, requires that planning applications for all major development (both commercial and residential) in Wokingham Borough to submit an employment skills plan (ESP) with a supporting method statement.

94. The applicant has submitted a draft statement which outlines their intended approach to ESP's and supporting apprenticeships as part of this proposed development. They have provided their agreement to the principle of providing either a detailed ESP and/or financial contribution in lieu of any ESP targets that are not met. These obligations are recommended to be secured via a s106 legal agreement if Planning Committee are minded to grant planning permission to this scheme.

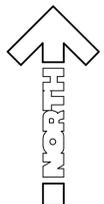
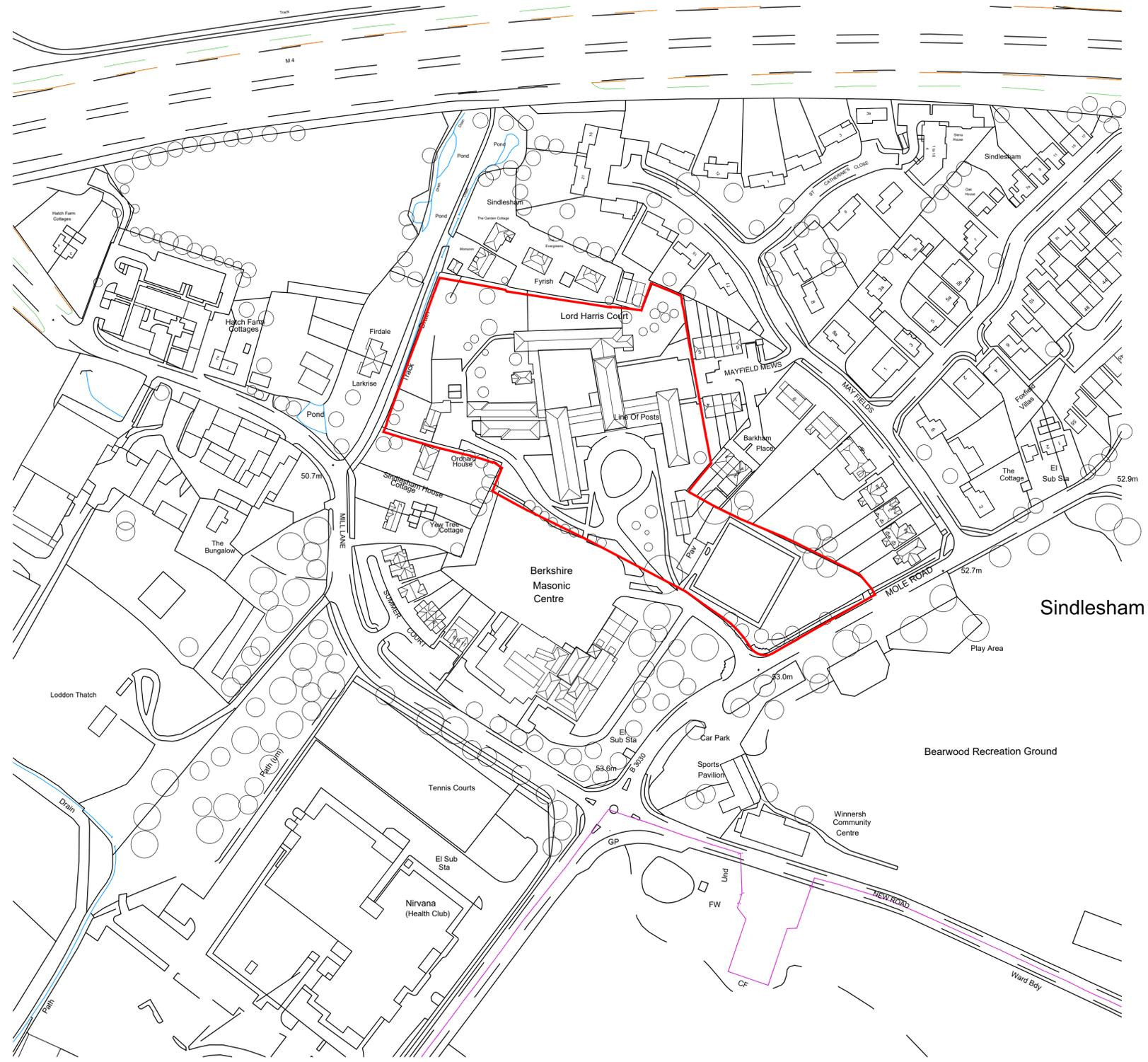
95. If the applicant elects to deliver the ESP rather than pay the financial contribution in lieu, the applicants/owners bound by the planning obligation will be required to notify the Council in writing of the agreed trigger points. This will include an update of the ESP demonstrating the achievements and providing evidence of each.

96. Should insufficient progress have been made on achieving this to the Council's satisfaction, the contribution in lieu (or part thereof dependant on partial met trigger points) shall in accordance with the s106 planning obligation become payable forthwith by the applicant.

CONCLUSION

The proposal is recommended for approval subject to completion of a s106 legal agreement as outlined at the start of this report.

This page is intentionally left blank



A	ISSUED FOR PLANNING APPLICATION	19.01.2018	FJG	
Rev.	Revision	Date	Drawn	Checked

CAP Carless & Adams Partnership

Architects
Building Surveyors
Quantity Surveyors
Project Managers
CDM Advisor

6 Progress Business Centre
Whittle Parkway
Bath Road
Slough
SL1 6QC

www.carless-adams.co.uk

Tel: 01628 665131
Fax: 01628 663327

Client **ROYAL MASONIC BENEVOLENT INSTITUTION**

Project **LORD HARRIS COURT**

Location **MOLE ROAD, SINDLESHAM, WOKINGHAM**

Title **LOCATION PLAN**

Scale A1@1:1250	Date June 2017	Drawn FJG	Checked
------------------------	-----------------------	------------------	---------

For construction purposes dimensions shall not be scaled & figured dimensions must be verified on site before work commences. This drawing is Copyright ©	Job No. A-731	Drawing No. 01	Revision A
---	-------------------------	--------------------------	----------------------

This page is intentionally left blank



LANDSCAPE KEY

	PROPOSED INTERNAL ROAD - TARMAC
	PROPOSED PARKING BAYS - BLOCK PAVING
	PROPOSED FOOTPATHS/PATIOS
	PROPOSED INTERNAL FOOTPATHS
	PROPOSED TALL NATIVE HEDGE PLANTING
	PROPOSED LAWN
	PROPOSED FLOWER BEDS
	PROPOSED FLOWER BEDS
	PROPOSED BULB PLANTING
	PROPOSED SEATS
	PROPOSED SHRUBS AND GROUND COVER
	PROPOSED SPECIMEN SHRUB
	PROPOSED PERGOLA

261

C	AMENDED RADII TO ACCESS CARE HOME DROP OFF PARKING AREA	29.10.2018	FJG	PK
B	AMENDED LANDSCAPE MASTERPLAN	19.10.2018	FJG	MM
A	ISSUED FOR PLANNING APPLICATION	09.10.2018	FJG	MM
Rev.	Revision	Date	Drawn	Checked

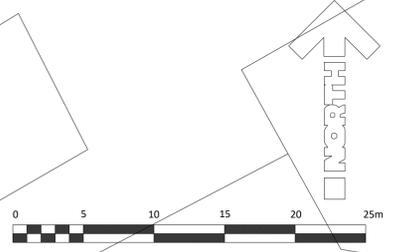
CAP Carless & Adams Partnership
 Architects
 Building Surveyors
 Quantity Surveyors
 Project Managers
 CDM Co-ordinators
 www.carless-adams.co.uk

© Progress Business Centre
 White Parkway
 Bath Road
 Slough
 SL1 6DQ
 Tel: 01628 665131
 Fax: 01628 663327

Client **ROYAL MASONIC BENEVOLENT INSTITUTION**
 Project **LORD HARRIS COURT**
 Location **MOLE ROAD, SINDLESHAM, WOKINGHAM**
 Title **MASTERPLAN**
CARE HOME SITE PLAN

Scale A1@1:250	Date July 2018	Drawn FJG	Checked MM
-----------------------	-----------------------	------------------	-------------------

For construction purposes dimensions shall not be scaled & figured dimensions must be verified on site before work commences. This drawing is Copyright ©	Job No. A-731	Drawing No. 57	Revision C
---	----------------------	-----------------------	-------------------



This page is intentionally left blank



- LANDSCAPE KEY**
- PROPOSED INTERNAL ROAD - TARMAC
 - PROPOSED PARKING BAYS - BLOCK PAVING
 - PROPOSED FOOTPATHS/PATIOS
 - PROPOSED INTERNAL FOOTPATHS
 - PROPOSED TALL NATIVE HEDGE PLANTING
 - PROPOSED LAWN
 - PROPOSED FLOWER BEDS
 - PROPOSED FLOWER BEDS
 - PROPOSED BULB PLANTING
 - PROPOSED SEATS
 - PROPOSED SHRUBS AND GROUND COVER
 - PROPOSED SPECIMEN SHRUB
 - PROPOSED PERGOLA
 - APPROXIMATE LOCATIONS OF HIBERNACULA FOR GREAT CRESTED NEWTS AND REPTILES

C	AMENDED RADII TO ACCESS CARE HOME DROP OFF PARKING AREA AND ADDITION OF HIBERNACULA LOCATIONS	29.10.2018	FJG	PK
B	AMENDED LANDSCAPE MASTERPLAN	19.10.2018	FJG	MM
A	ISSUED FOR PLANNING APPLICATION	09.10.2018	FJG	MM
Rev.	Revision	Date	Drawn	Checked

CAP Carless & Adams Partnership

Architects
Building Surveyors
Quantity Surveyors
Project Managers
CDM Co-ordinators

www.carless-adams.co.uk

© Progress Business Centre
White Parkway
Bath Road
Slough,
SL1 6DQ
Tel: 01628 665131
Fax: 01628 663327

Client **ROYAL MASONIC BENEVOLENT INSTITUTION**

Project **LORD HARRIS COURT**

Location **MOLE ROAD, SINDLESHAM, WOKINGHAM**

Title **MASTERPLAN
ASSISTED LIVING SITE PLAN**

Scale	A1@1:250	Date	July 2017	Drawn	FJG	Checked	MM
-------	----------	------	-----------	-------	-----	---------	----

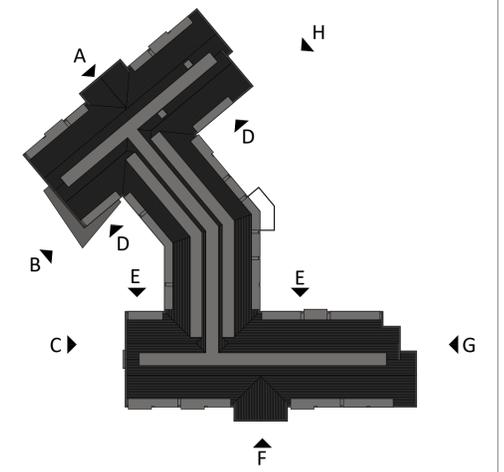
For construction purposes dimensions shall not be scaled & figured dimensions must be verified on site before work commences. This drawing is Copyright ©	Job No. A-731	Drawing No. 58	Revision C
---	-------------------------	--------------------------	----------------------

This page is intentionally left blank



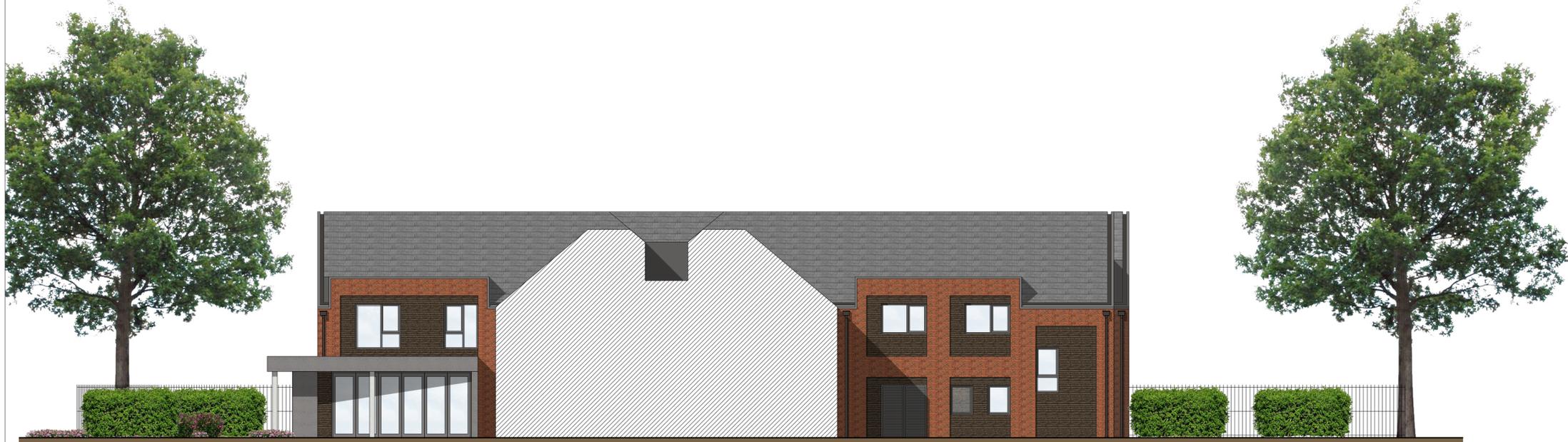
ELEVATION A

KEY PLAN



ELEVATION B

ELEVATION C



ELEVATION D



265

A	ISSUED FOR PLANNING APPLICATION	09.10.2018	FJG	MM
Rev.	Revision	Date	Drawn	Checked

CAP Carless & Adams Partnership
 Architects
 Building Surveyors
 Quantity Surveyors
 Project Managers
 CDM Co-ordinators

www.carless-adams.co.uk

6 Progress Business Centre
 White Parkway
 Bath Road
 Slough
 SL1 2PD
 Tel: 01628 665131
 Fax: 01628 663327

Client: ROYAL MASONIC BENEVOLENT INSTITUTION
 Project: LORD HARRIS COURT
 Location: MOLE ROAD, SINDLESHAM, WOKINGHAM
 Title: CARE HOME ELEVATIONS

Scale: A1@1:100	Date: July 2018	Drawn: FJG	Checked: MM
-----------------	-----------------	------------	-------------

Job No. A-731	Drawing No. 75	Revision A
------------------	-------------------	---------------

For construction purposes dimensions shall not be scaled & figured dimensions must be verified on site before work commences. This drawing is Copyright ©

This page is intentionally left blank



ELEVATION E



ELEVATION F FROM MOLE ROAD



ELEVATION F WITHOUT TREES ON MOLE ROAD

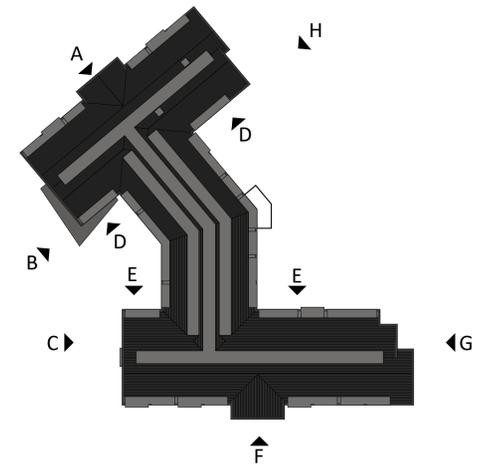


ELEVATION G

ELEVATION H



KEY PLAN



267

A	ISSUED FOR PLANNING APPLICATION	09.10.2018	FJG	MM
Rev.	Revision	Date	Drawn	Checked

CAP Carless & Adams Partnership

Architects
Building Surveyors
Quantity Surveyors
Project Managers
CDM Co-ordinators

6 Progress Business Centre
Winkle Parkway
Bath Road
Slough
SL1 2DD
Tel: 01628 665131
Fax: 01628 663327

www.carless-adams.co.uk

Client	ROYAL MASONIC BENEVOLENT INSTITUTION
Project	LORD HARRIS COURT
Location	MOLE ROAD, SINDLESHAM, WOKINGHAM
Title	CARE HOME ELEVATIONS

Scale	A1@1:100	Date	July 2018	Drawn	FJG	Checked	MM
-------	----------	------	-----------	-------	-----	---------	----

For construction purposes dimensions shall not be scaled & figured dimensions must be verified on site before work commences. This drawing is Copyright ©	Job No.	Drawing No.	Revision
	A-731	76	A

This page is intentionally left blank



SOUTH ELEVATION



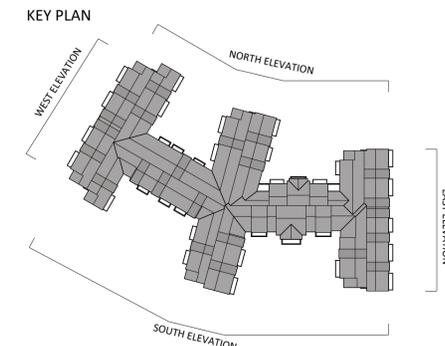
EAST ELEVATION



NORTH ELEVATION



WEST ELEVATION



A	ISSUED FOR PLANNING APPLICATION	09.10.2018	FJG	MM
Rev.	Revision	Date	Drawn	Checked

CAP Carless & Adams Partnership

Architects
Building Surveyors
Quantity Surveyors
Project Managers
CDM Co-ordinators

6 Progress Business Centre
White Parkway
Bath Road
Stough
SL1 2DQ

www.carless-adams.co.uk

Tel: 01628 665131
Fax: 01628 663327

Client: ROYAL MASONIC BENEVOLENT INSTITUTION
 Project: LORD HARRIS COURT
 Location: MOLE, SINDELSHAM, WOKINGHAM
 Title: ASSISTED LIVING ELEVATIONS

Scale	A1@1:200	Date	July 2018	Drawn	FJG	Checked	MM
-------	----------	------	-----------	-------	-----	---------	----

For construction purposes dimensions shall not be scaled & figured dimensions must be verified on site before work commences. This drawing is Copyright ©	Job No.	A- 731	Drawing No.	85	Revision	A
---	---------	--------	-------------	----	----------	---

This page is intentionally left blank

SCHEDULE OF ACCOMMODATION

SITE AREA
1.99 Ha/ 4.91 acres

CARE HOME

GROUND FLOOR 20 BEDS & ANCILLARY ACCOMMODATION
FIRST FLOOR 25 BEDS & ANCILLARY ACCOMMODATION
TOTAL 45 RESIDENT BEDROOMS

GROSS INTERNAL FLOOR AREA

GROUND FLOOR 1,220 m²
FIRST FLOOR 1,220 m²

TOTAL GIFA: 2,440 m²

SPACE PER RESIDENT: 54 m²

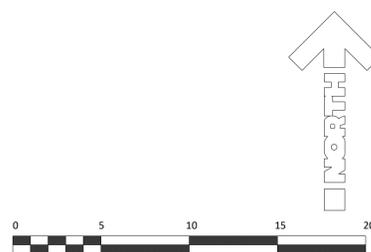


GROUND FLOOR

FIRST FLOOR

KEY:

- RESIDENT BEDROOM
- COMMUNAL / ACTIVITY SPACE
- STAFF AND SERVICE AREA
- CIRCULATION ROUTES
- EXTERNAL AREAS



A	ISSUED FOR PLANNING APPLICATION	09.10.2018	FJG	MM
Rev.	Revision	Date	Drawn	Checked

CAP Carless & Adams Partnership
Architects, Building Surveyors, Quantity Surveyors, Project Managers, CDM Co-ordinators
www.carless-adams.co.uk

6 Progress Business Centre
White Parkway
Bath Road
Strough
GL1 2PQ
Tel: 01628 665131
Fax: 01628 663327

Client	ROYAL MASONIC BENEVOLENT INSTITUTION
Project	LORD HARRIS COURT
Location	MOLE ROAD, SINDLESHAM, WOKINGHAM
Title	CARE HOME FLOOR PLANS

Scale	A1@1:200	Date	July 2017	Drawn	FJG	Checked	MM
-------	----------	------	-----------	-------	-----	---------	----

For construction purposes dimensions shall not be scaled & figured dimensions must be verified on site before work commences. This drawing is Copyright ©	Job No.	Drawing No.	Revision
	A-731	60	A

271

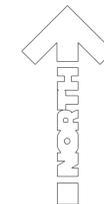
This page is intentionally left blank



FIRST FLOOR PLAN

KEY:

- 1 BED FLAT
- 2 BED FLAT
- COMMUNAL AREAS
- STAFF ANCILLARY
- CIRCULATION/ STORAGE



A	ISSUED FOR PLANNING APPLICATION	09.10.2018	FJG	MM
Rev.	Revision	Date	Drawn	Checked

www.carless-adams.co.uk

6 Progress Business Centre
Whitby Parkway
Bath Road
Stough
S.L. OPO
Tel: 01628 665131
Fax: 01628 663327

Client ROYAL MASONIC BENEVOLENT INSTITUTION
 Project LORD HARRIS COURT
 Location MOLE ROAD, SINDLESHAM, WOKINGHAM
 Title ASSISTED LIVING
 FIRST FLOOR PLAN

Scale A1@1:250	Date July 2018	Drawn FJG	Checked MM
----------------	----------------	-----------	------------

For construction purposes dimensions shall not be scaled & figured dimensions must be verified on site before work commences. This drawing is Copyright ©	Job No. A- 731	Drawing No. 63	Revision A
---	--------------------------	--------------------------	----------------------

This page is intentionally left blank

PLANNING REF : 190233
PROPERTY ADDRESS : Winnersh Community Centre
: New Road, Sindlesham, Wokingham
: RG41 5DX
SUBMITTED BY : Winnersh Parish Council
DATE SUBMITTED : 07/03/2019

COMMENTS:

1. Balconies overlooking residential properties: Mayfield Mews area; Bedrooms have balconies overlooking the residential properties. The same applies to the balconies, side on, overlooking properties in Mill Lane (track).
2. Oak trees T33 and T34: Why do they need to be removed? This is a screen for residential properties in Mayfields, they are both growing on the outside of the tree T35.
3. Impact of additional motorised traffic: The application states that it is proposed the morning changeover period be completed by 8am (prior to the morning rush hour). However, traffic queues on Mole Road/King Street Lane from 07:30am, therefore an impact on already congested roads.
4. Parking: It is understood that the current agreement with overflow parking in the Sindlesham Court area only relates to the current layout not the planned. If this is correct and overflow parking was not permitted in the Sindlesham Court area, then this additional parking would impact on nearby Mayfields St Marys and Winnersh Parish Council car park.
5. Smoking shelter: Why has the smoking shelter been located adjacent to residential houses, this needs to be relocated away from residential dwellings.
6. Shadowing of building: There is concern with the shadowing impact of the proposed building on the residential properties located in Mayfields.

This page is intentionally left blank

Agenda Item 94.

Application Number	Expiry Date	Parish	Ward
190395	15 th May 2019	Wokingham	Wescott;

Applicant	Bernie Pich (Wokingham Borough Council)
Site Address	Alexandra House, Alexandra Court, Wokingham RG40 2LD
Proposal	Full planning application for the proposed change of use of ground floor from 'Spin and Night club' to mixed use comprising retail (use class A1) and Business (use class B1a), plus changes to fenestration to ground floor.
Type	Full
PS Category	20
Officer	Nesha Burnham
Reason for determination by committee	Applicant is Wokingham Borough Council

FOR CONSIDERATION BY	Planning Committee on Wednesday, 8 May 2019
REPORT PREPARED BY	Assistant Director – Place

SUMMARY
<p>This planning application is before the planning committee as it is the Council's own development and involves an increase in staff numbers.</p> <p>This is a proposal for the change of use of the ground floor level of Alexandra House from Sui Generis (nightclub) to a mixed use comprising use classes A1 (Retail) and B1a (Business). The proposed external changes include new door entrances and fenestration changes.</p> <p>The proposal would provide 264 square metres of additional office floor space and four retail units ranging between 46 to 47 square metres. The proposed office and retail use classes are considered acceptable given the site's town centre location. The proposal would not result in the loss of any Use Class B floor space.</p> <p>The proposed external changes would respect the character and appearance of the application building and the surrounding Conservation Area, subject to a condition being secured requiring the external finishes to match those on the building. The proposed change of use would not cause undue harm upon the amenities enjoyed by neighbouring occupiers.</p> <p>This planning application does not provide any allocated parking. However, given its town centre location and close proximity to Elm's Car Park, this arrangement is considered acceptable and is unlikely to cause parking stress on the surrounding streets.</p> <p>The proposal would comply with development plan policy and is recommended for approval, subject to conditions.</p>

PLANNING STATUS

- Major Development Location
- Wokingham Town Centre Conservation Area
- Site of Special Scientific Interest Impact Risk Zones
- Archaeological Site
- Aerodrome Safeguarding for Wind
- Special Protection Area (7km linear)

RECOMMENDATION

That the committee authorise the GRANT OF PLANNING PERMISSION subject to the following conditions:

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.
Reason: In pursuance of s.91 of the Town and Country Planning Act 1990 (as amended by s.51 of the Planning and Compulsory Purchase Act 2004).
2. This permission is in respect of the submitted application plans and drawings numbered 4075.1002 PL1, 4075.1003 PL1, 4075.2001 PL1, 4075.2002 PL1, a Planning Statement (Project Ref: 4075) and a Design and Access Statement (Project Ref: 4075) all received by the local planning authority on 8th February 2019 and drawing numbered 4075.9001 PL2 received by the local planning authority on 26th February 2019. The development shall be carried out in accordance with the approved details unless other minor variations are agreed in writing after the date of this permission and before implementation with the Local Planning Authority.
Reason: For the avoidance of doubt and to ensure that the development is carried out in accordance with the application form and associated details hereby approved.
3. The materials to be used in the construction of the external surfaces of the development hereby permitted shall be of a similar appearance to those used in the existing building unless other minor variations are agreed in writing after the date of this permission and before implementation with the Local Planning Authority.
Reason: To ensure that the external appearance of the building is satisfactory.
Relevant policy: Policies CP1 and CP3 of the adopted Wokingham Borough Core Strategy (2010).
4. The development hereby permitted shall be used for purposes falling within A1 (retail) and B1a (Offices) Use Classes only, and for no any other purpose as defined in the Town and Country Planning (Use Classes) Order 1987 (as amended) or in any provision equivalent to that Class in any statutory instruction revoking and re-enacting that Order (with or without modification), without written consent from the Local Planning Authority first being obtained.
Reason: To enable the Local Planning Authority to regulate and control the development of land, having regard to Policies CP1, CP3, CP6 of the adopted Wokingham Borough Core Strategy (2010) and Policy CC01 of the adopted Wokingham Borough Managing Development Delivery Local Plan (2014).
5. The A1 (retail) use hereby permitted shall not operate other than between the hours of 9.00am to 18.00pm Mondays to Saturdays and 10.00am to 16.00pm on Sundays and Bank or National Holidays.
Reason: To safeguard residential amenities. Relevant policy: Policies

CP1 and CP3 of the adopted Wokingham Borough Core Strategy (2010) and Policy CC06 of the adopted Wokingham Borough Managing Development Delivery Local Plan (2014).

Informatives:

1. The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received. This planning application has been the subject of positive and proactive discussions with the applicant in terms of:

- Extending the determination period of the application to allow for the proposal to be presented at Planning Committee

The decision to grant planning permission in accordance with the presumption in favour of sustainable development as set out in the NPPF is considered to be a positive outcome of these discussions.

PLANNING HISTORY		
Application Number	Proposal	Decision
F/2001/3437	Proposed three storey extension to office building for (B1) office use (at Exa House)	Withdrawn
F/2005/4915	Proposed extension to office building (B1a) and change of use of ground floor from B1a office use to A1, A2, A3 & A4 (shops, financial and professional service, restaurants and pub/bar) use (at Exa House)	Approved on 14 th December 2005
VAR/2008/1257	Proposed variation to condition 9 of consent F/2002/4915 to extend opening hours for the supply of food and drink from 9:00-23:20 to 9.00 to 2.30 (at Exa House)	Withdrawn
F/2008/1976	Proposed change of use from A4 (drinking establishments) to Sui Generis (nightclub) including extension of hours (at Exa House)	Refused on 4 th September 2008 Allowed at appeal on 29 th April 2009
VAR/2010/0984	Application for variation of Condition 1 of appeal planning consent F/2008/1976 to extend opening hours from 02.30 to 03.30 daily (at Spin Bar Exa House)	Approved on 22 nd July 2010
170695	Full planning application for the part change of ground floor night club from A5 back to Sui Generis	Not proceeded with

SUMMARY INFORMATION	
For Commercial	
Site Area	887 m ²
Previous land use(s) and floor space(s)	Sui Generis (418 m ²) and Office entrance (41 m ²)
Proposed floor space of each use	Office (264m ²) and Retail (195m ²)
Change in floorspace (+/-)	No increase in floor space
Number of jobs created/lost	10 full-time jobs created
Existing parking spaces	10 car parking spaces
Proposed parking spaces	None

CONSULTATION RESPONSES	
WBC Conservation	No objection
WBC Highways	No objection
WBC Environmental Health	No objection

REPRESENTATIONS
Town/Parish Council: No comments received
Local Members: No comments received
Neighbours: No comments received

PLANNING POLICY			
National Policy	NPPF	National Planning Policy Framework	
Adopted Core Strategy DPD 2010	CP1	Sustainable Development	
	CP2	Inclusive Communities	
	CP3	General Principles for Development	
	CP6	Managing Travel Demand	
	CP7	Biodiversity	
	CP8	Thames Basin Heaths Special Protection Area	
	CP9	Scale and Location of Development Proposals	
	CP13	Town Centres and Shopping	
	CP14	Growth and Renaissance of Wokingham Town Centre	
	CP15	Employment Development	
	Adopted Managing Development Delivery Local Plan 2014	CC01	Presumption in Favour of Sustainable Development
		CC02	Development Limits
		CC03	Green Infrastructure, Trees and Landscaping
		CC04	Sustainable Design and Construction

	CC05	Renewable energy and decentralised energy networks
	CC06	Noise
	CC07	Parking
	CC09	Development and Flood Risk (from all sources)
	CC10	Sustainable Drainage
	TB01	Development within the Green Belt
	TB06	Development of private residential gardens
	TB07	Internal Space standards
	TB15	Major Town, and Small Town/District Centre Development
	TB16	Development for Town Centre Uses
	TB20	Service Arrangements and Deliveries for Employment and Retail Use
	TB21	Landscape Character
	TB23	Biodiversity and Development
	TB24	Designated Heritage Assets
Supplementary Planning Documents (SPD)	BDG	Borough Design Guide
		DCLG – National Internal Space Standards

PLANNING ISSUES

Description of Development:

1. The application site is located within the Wokingham Town Centre Conservation Area, and comprises a three storey building known as Alexandra House. The building contains a vacant night club at ground floor level, and office space at first and second floor level.
2. This proposal is for the change of use of the ground floor level of Alexandra House from Sui Generis to a mixed use comprising Use Classes A1 (Retail) and B1a (Business). The proposed office space would measure 264 square metres and serve as a reception area, meeting rooms, kitchenette and store room. Four retail units are proposed measuring between 46 and 47 square metres.
3. Access to the retail units would be from the northern elevation facing the courtyard. There would be a separate entrance to the office on the eastern elevation. Changes to the fenestration and door arrangements are proposed in order to facilitate the proposed new access points to the building.
4. The proposal would create 10 full-time job opportunities.

Principle of Development:

5. The National Planning Policy Framework has an underlying presumption in favour of sustainable development which is carried through to the local Development Plan. The Managing Development Delivery Local Plan (MDD) Policy CC01 states that planning applications that accord with the policies in the Development Plan for Wokingham Borough will be approved without delay, unless material considerations indicate otherwise.
6. Policy CC02 of the MDD sets out the development limits for each settlement as defined on the policies map and therefore replaces the proposals map adopted through the Core Strategy, as per the requirement of policy CP9. Policy CP9 of the Core Strategy seeks to ensure the scale of development proposals in Wokingham Borough reflect the existing or proposed levels of facilities and services at or in the location, together with their accessibility.
7. Policy CP13 of the Core Strategy states that Wokingham town centre, in conjunction with other town centres, district centres and village centres within the Borough are suitable in principle to accommodate town centre uses. The supporting text defines town centre uses as retailing, entertainment, arts and culture, indoor recreation, leisure, health, community and office uses. Policy CP14 of the Core Strategy states that Wokingham, as a major town centre in Berkshire, is considered suitable for growth.
8. Policy TB15 of the MDD establishes that proposals for main town centre uses should demonstrate that they are “of a scale and form that is compatible with the retail character of the centre and its role in the hierarchy of retail centres”. They should also demonstrate that “they contribute to the provision of day and evening/night-time uses” and are compatible with other uses and “enhance vitality and viability”.
9. The application site is situated in the town centre of Wokingham, a Major Development Location. The proposed use of the ground floor space for retail (use class A1) and office (use class B1a) purposes would provide an appropriate town centre use within a building that is situated in a town centre location. At present, the nightclub at the application site is not in operation and has been vacant for a period of time.
10. There are no policies set out in the Development Plan which seeks the protection of nightclubs, and it should be noted that there is an existing Public House in close proximity to the application site (i.e. The Gig House). It is therefore considered that the proposed office and retail uses would complement the surrounding context, particularly noting the strategic mixed use scheme currently being constructed to the south of the application site at ‘Land between Wellington Road and Shute End (Elms Field and Paddocks Car Park)’.
11. Given the above, it is considered that the principle of the proposed change of use is acceptable in planning policy terms.

Character of the Area:

12. Policy CP1 of the Core Strategy states that planning permission will be granted for development proposals that maintain or enhance the high quality of the environment. Policy CP3 of the Core Strategy states planning permission will be granted if development is of an appropriate scale of activity, mass, layout, built form, height, materials and character to the area together with a high quality of design without detriment to the amenities of adjoining land users including open spaces or occupiers and their quality of life.
13. Policy TB24 of the Managing Development Delivery Local Plan (MDD) states that the Borough Council will conserve and seek the enhancement of designated heritage assets in the Borough and their setting.
14. Alexandra House is not a designated heritage asset, but it is located within the Wokingham Town Centre Conservation Area. It is noted that the site is also located near to a number of neighbouring listed buildings. The proposed changes involves new door openings and fenestration changes along the northern and eastern elevations. It is considered that the proposed external changes would be in-keeping with the architectural style of the application building, and would respect the overall character and appearance of the Conservation Area.
15. The Council's Conservation Officer has raised no objection to this application, subject to a condition requiring the materials to match those on the building.
16. As such, it is considered that the proposal would not have a harmful impact upon the character and appearance of the building or surrounding Conservation Area. This would be compliant with policies CP1 and CP3 of the Core Strategy and Policy TB24 of the MDD.

Residential Amenities:

17. Policy CP3 of the Core Strategy aims to ensure that there is no detrimental impact upon neighbour amenity. Policy CC06 of the MDD states that the noise impact of the development must be assessed. The application site's approved use class at ground floor level is Sui Generis, i.e. a nightclub. It is also noted that the site is within close proximity to other commercial units and business premises, notably a Public House, hair dressers, offices and restaurants.
18. The nearest residential development are the flats located at The Plaza and Regent Court. However, given the surrounding context and the extant use of the application site, it is considered that the proposed mixed retail (use class A1) and office (use class B1a) uses would not result in an unacceptable impact upon the living conditions of neighbouring occupiers in terms of noise and disturbance, and would be reflective of the other uses nearby.
19. If approved, a condition would be secured restricting the hours of operation of the proposed retail units.
20. Overall, it is considered that the proposal would not cause undue harm upon the living conditions of neighbouring occupiers, in accordance with policy CP3 of the Core Strategy and Policy CC06 of the MDD.

Parking and Highway:

21. Policy CP6 of the Core Strategy seeks to manage travel demand with sustainable travel modes, appropriate vehicle parking and minimisation of traffic impacts. Policy CC07 and Appendix 2 of the MDD sets out the minimum parking requirements for development proposals.
22. It is acknowledged that no parking provision would be provided as part of this planning application. However, consideration must be given to the location of the site.
23. The site is located in the town centre meaning that potential customers of the proposed retail units and users of the office space would have good access to public transport, and would not necessarily be reliant upon driving. For those customers or employees who wish to drive, it is noted that 'Elm Road Car Park' is within a relatively short walking distance to the site. With regards to servicing, there is an existing vehicular access point along Elm Road that other commercial units currently use for deliveries and servicing.
24. The Council's Highway Officer has raised no objection to this application on parking or highway grounds.
25. On this basis, it is considered that proposal would not result in unacceptable parking pressures on surrounding streets or prejudice highway safety.

Thames Basin Heaths Special Protection Area:

26. The Thames Basin Heaths Special Protection Area (SPA) was designated under European Directive due to its importance for heathland bird species. Policy CP8 of the Core Strategy specifies proposals that provide 50 or more residential units within 7km (linear) will be individually assessed for whether a significant effect upon the SPA is likely to occur.
27. In this regard, the site is within the 7km buffer of the Thames Basin Heath Special Protection Area but given the nature of the development, the proposal does not trigger further mitigation or assessment.

Equalities and people with disabilities:

28. In determining this application the Council is required to have regard to its obligations under the Equality Act 2010. The key equality protected characteristics include age, disability, gender, gender reassignment, marriage and civil partnership, pregnancy and maternity, race, religion or belief, sexual orientation.
29. The proposed retail units and office space would be accessible to wheelchair users from the ramp on Denmark Road or the relatively flat pavement on Elm Road. The proposed door entrances would be at least 1.5 metres wide, which would provide sufficient clearance for wheel chairs. Furthermore, a lift would be located on the ground floor level of the building for those requiring its service to the first and second floor offices.

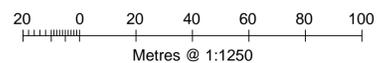
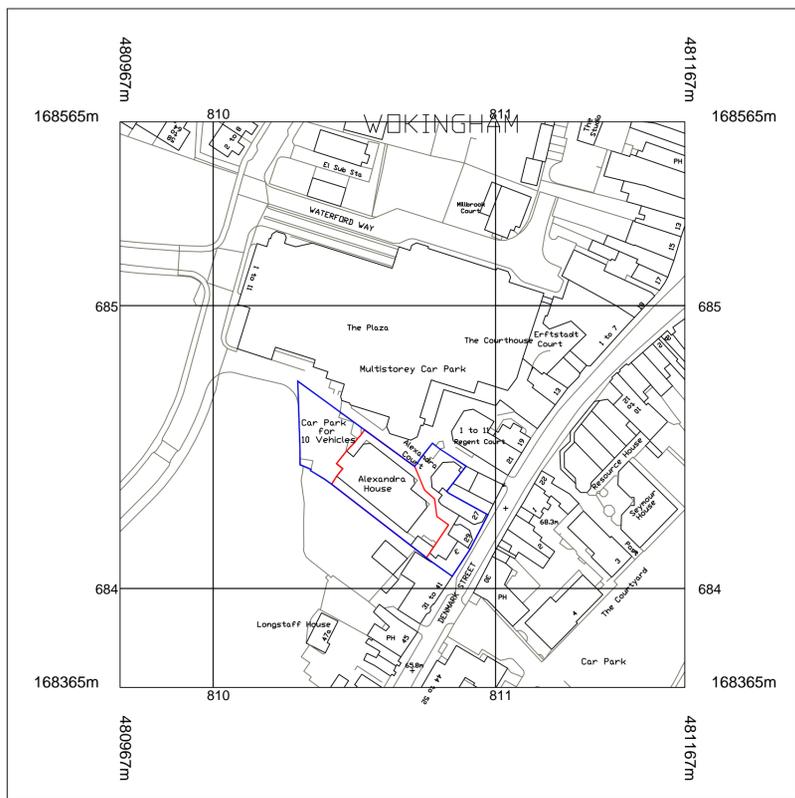
30. There is no indication or evidence (including from consultation on the application) that protected groups have or will have any other different needs, experiences, issues and priorities in relation to this planning application, other than those mentioned in the preceding paragraph. It is therefore considered that the proposal would not conflict with the Equality Act 2010.

CONCLUSION

The proposal complies with the relevant development plan policies and is consistent with the provisions of the NPPF specifically in relation to promoting sustainable development, conserving the environment, protecting neighbouring amenity and highway safety. In the absence of any material considerations of sufficient weight, it is recommended that the proposal be approved subject to conditions.

This page is intentionally left blank

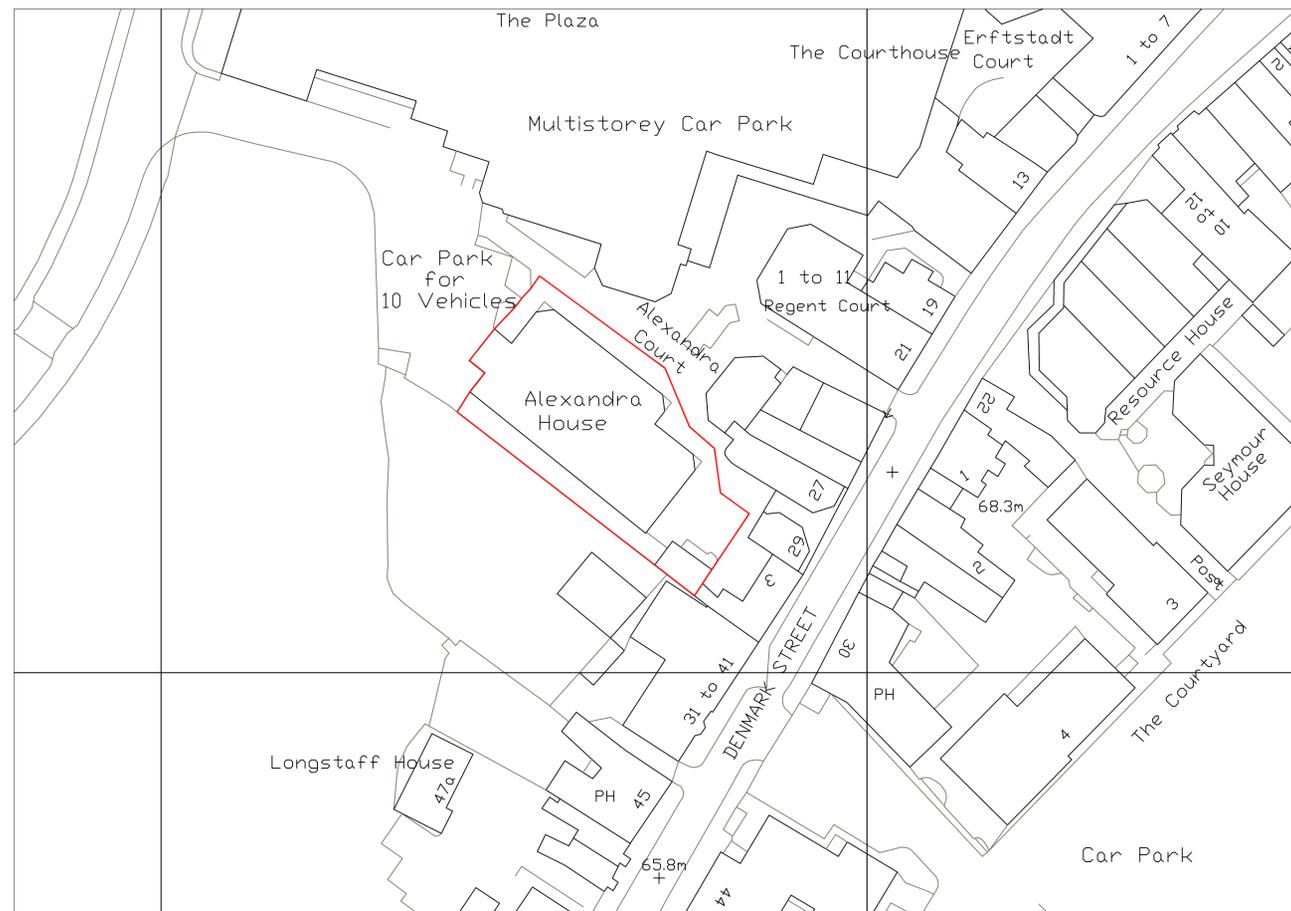
Stanfords VectorMap



© Crown copyright and database rights 2019 OS 100035409

Reproduction in whole or in part is prohibited
without the permission of Ordnance Survey.

Location / Block Plan
Scale 1:1250 @ A1



Site Plan
Scale 1:500 @ A1



The Anthony Smith Partnership
Construction Consultants
Crestfield House
13 Clarendon Hill
Reading
Berkshire
RG1 1YJ
Telephone 0118 980 111
Fax 0118 980 411

Client Wokingham Borough Council
Civic Offices
Wokingham
RG40 1BN

Project Refurbishment of Alexandra House
Denmark Street
Wokingham
RG40 2SL

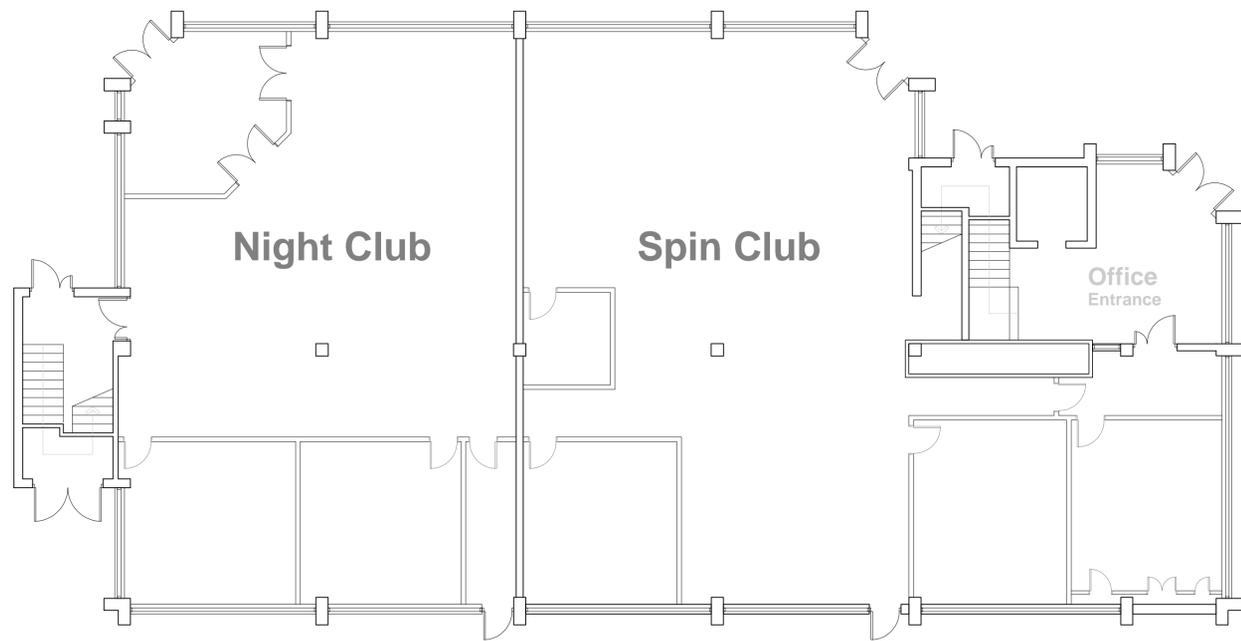
Drawing Location and Site Plans

Drawn By Psa **Date** February 2019

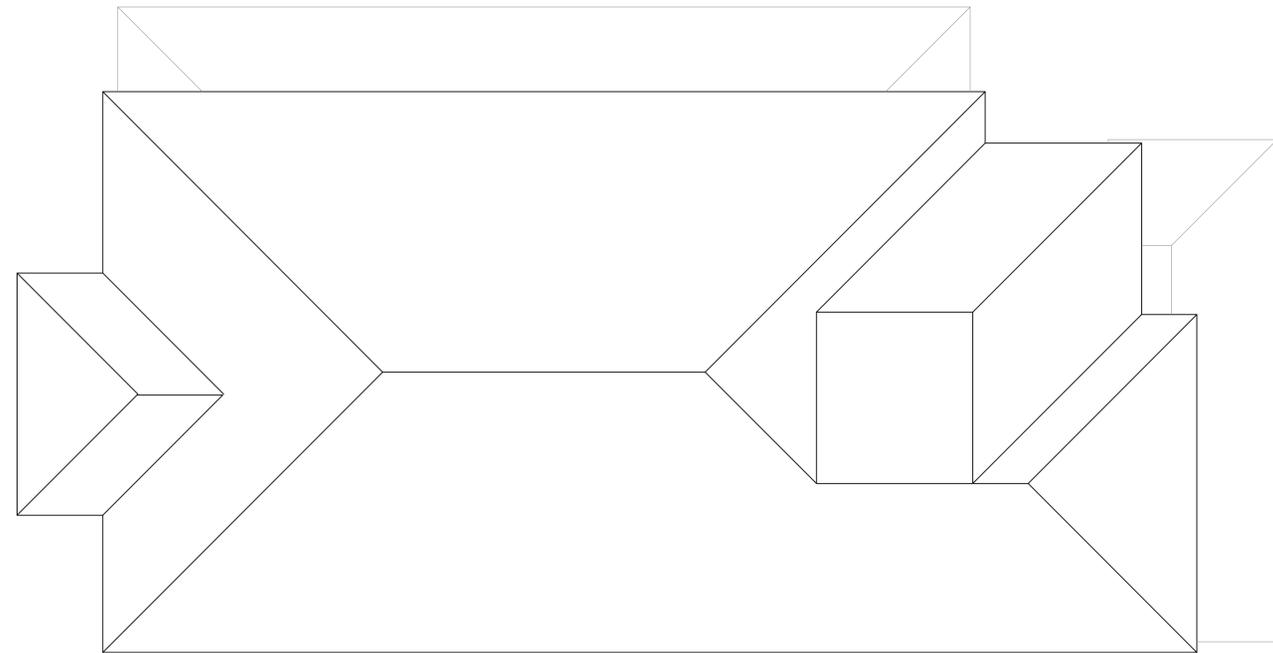
Scale As indicated **DWG. No.** 4075.9001 PL

Rev	Provisions	Date
R1	Issued for Planning Application	Feb 19
R2	Scales corrected - Issued for Planning Application	Feb 19

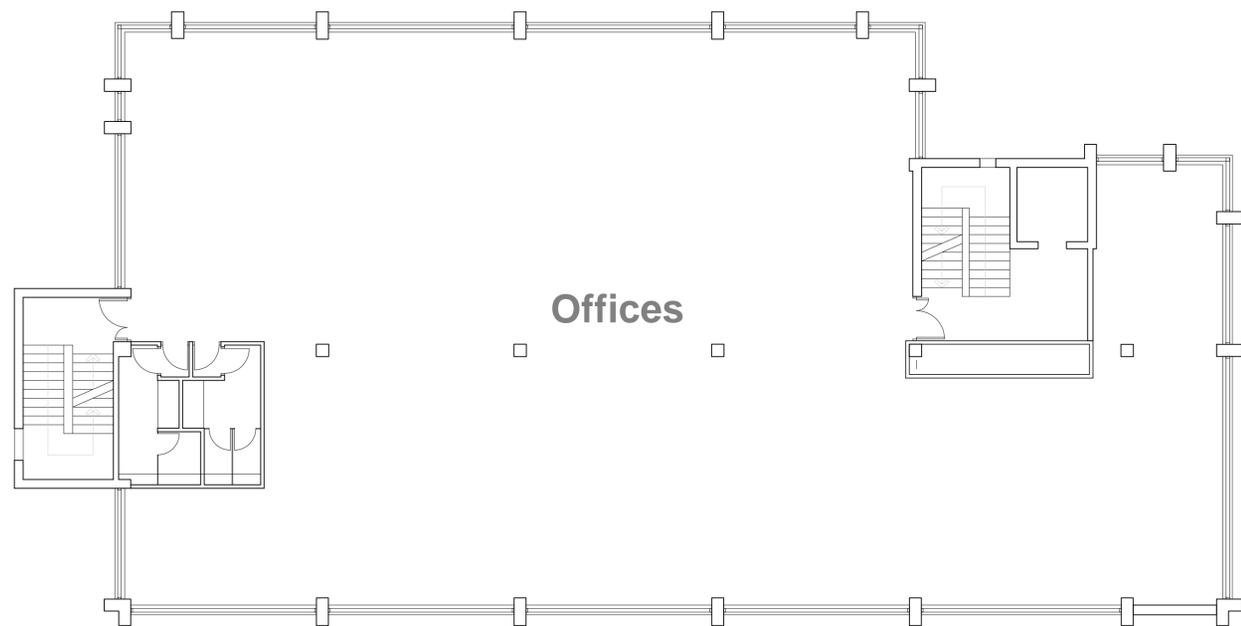
This page is intentionally left blank



GROUND FLOOR PLAN
Scale 1:100



ROOF PLAN
Scale 1:100



FIRST FLOOR PLAN
Scale 1:100



SECOND FLOOR PLAN
Scale 1:100

The Anthony Smith Partnership
Construction Consultants
Crestfield House
13 Clarendon Hill
Reading
RG2 1YJ
Telephone 0118 950 111
Fax 0118 950 411

Client Wokingham Borough Council
Civic Offices
Wokingham
RG40 1BN

Project Refurbishment of Alexandra House
Denmark Street
Wokingham
RG40 2SL

Drawing Floor Plans (existing)

Drawn By Psa **Date** January 2019

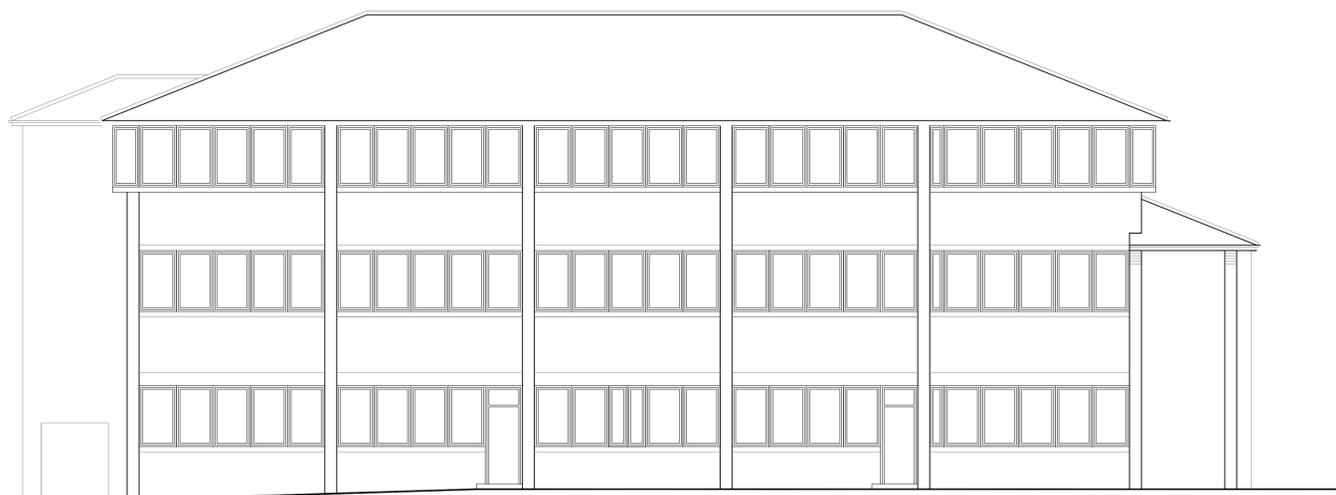
Scale 1:100 **DWG. No.** 4075.1002 PL

Rev	Revisions	Date
P1	Issued for Review	Nov 18
PL1	Issued for Planning Application	Feb 19

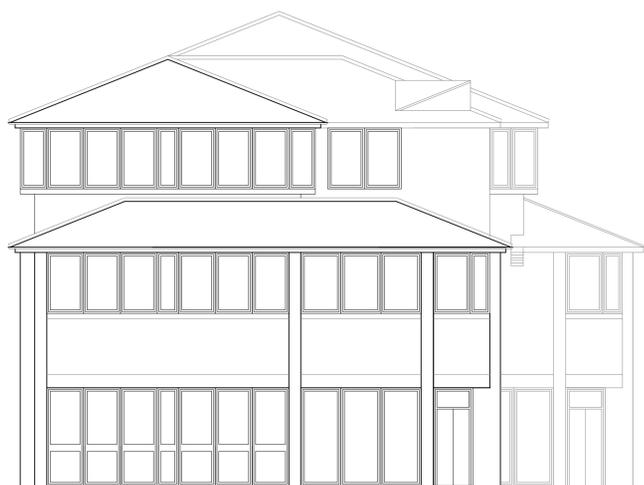
This page is intentionally left blank



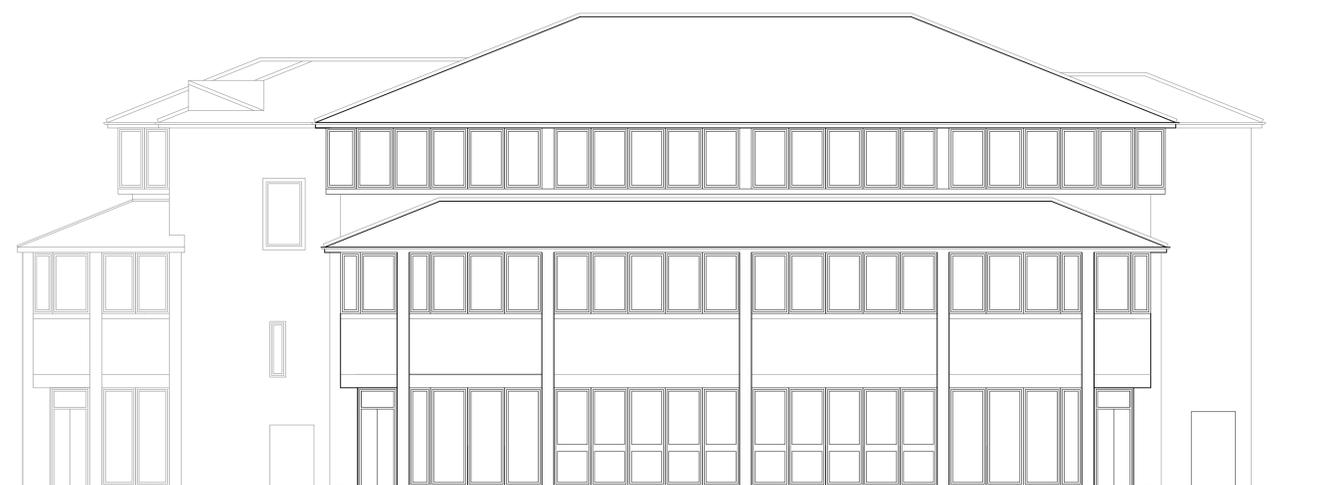
WEST ELEVATION.
Existing



SOUTH ELEVATION
Existing



EAST ELEVATION.
Existing



NORTH ELEVATION
Existing

291

The Anthony Smith Partnership
 Construction Consultants
 13 Clarendon Hill
 Reading
 Berkshire
 RG1 1YJ
 Telephone 0118 950 111
 Fax 0118 950 41

Client Wokingham Borough Council
 Civic Offices
 Wokingham
 RG40 1BN

Project Refurbishment of Alexandra House
 Denmark Street
 Wokingham
 RG40 2SL

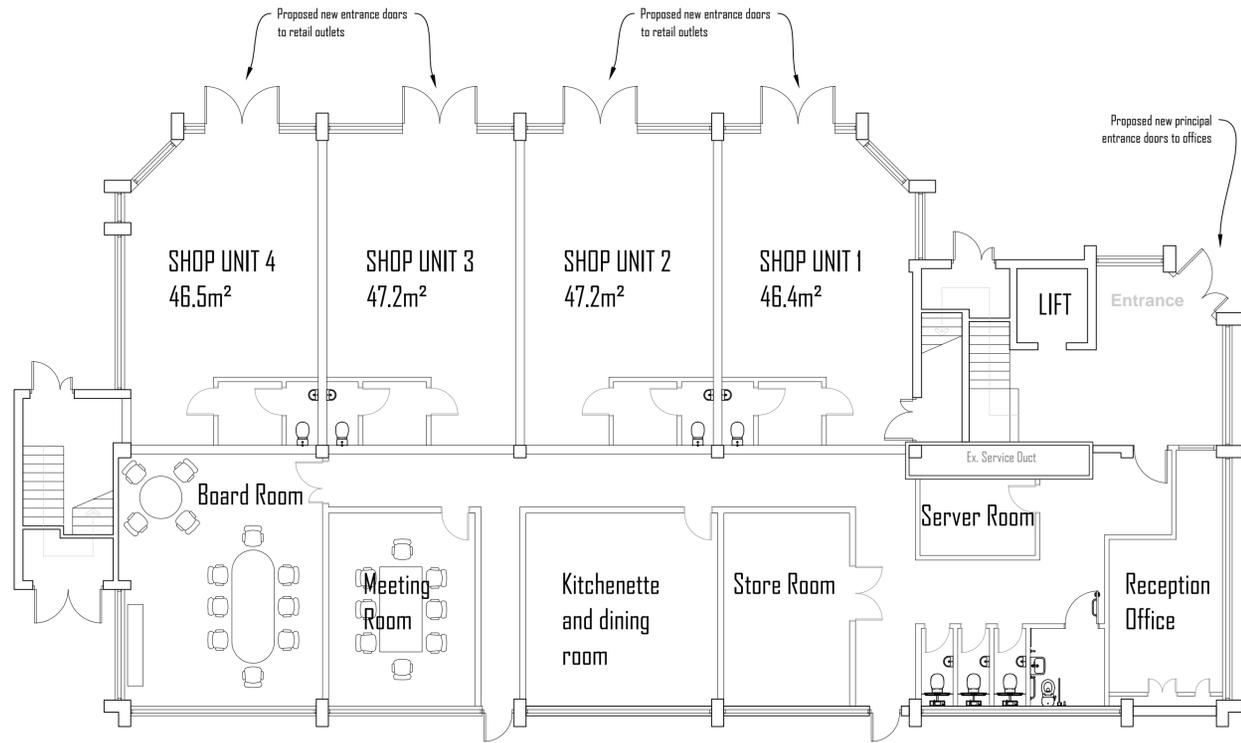
Drawing
 Elevations (existing)

Drawn By Psa **Date** January 2019

Scale 1:100 **DWG. No.** 4075.2001 PL

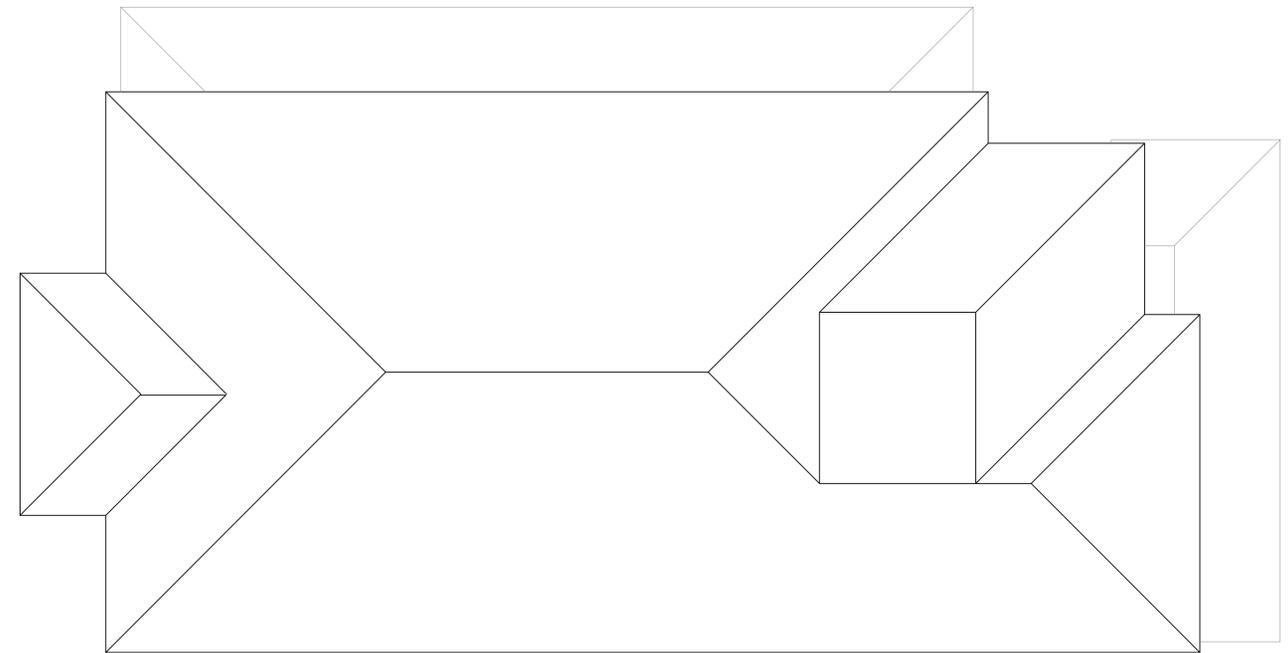
Rev	Revisions	Date
P1	Issued for Review	Jan 19
R1	Issued for Planning Application	Feb 19

This page is intentionally left blank



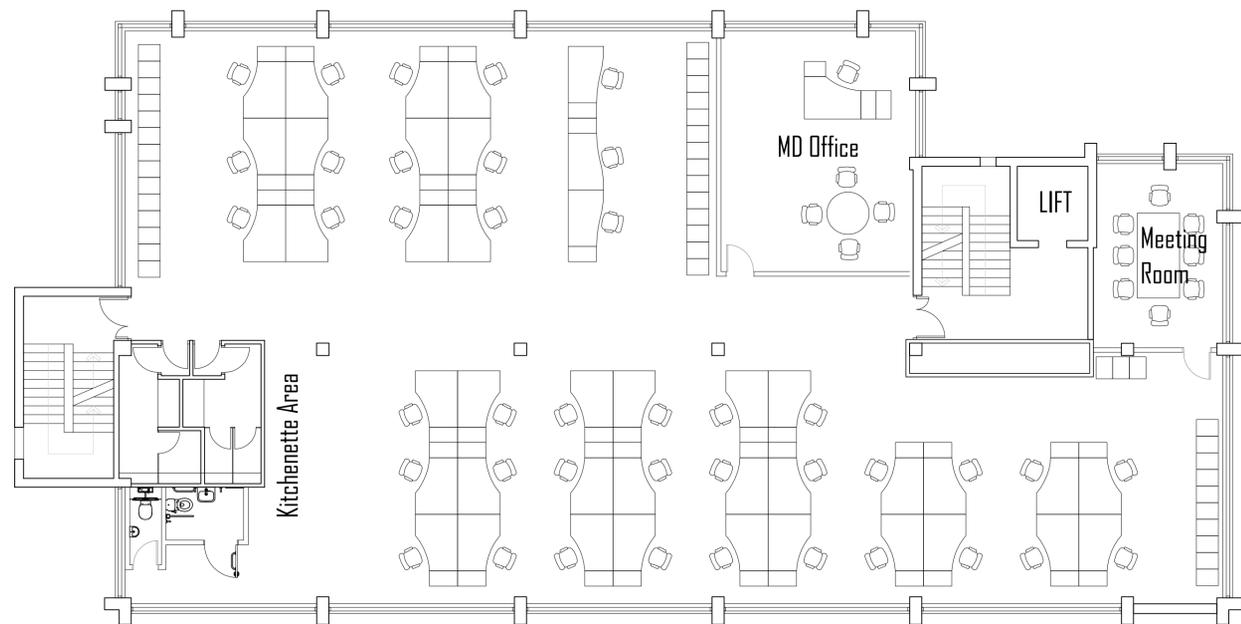
GROUND FLOOR PLAN

Scale 1:100



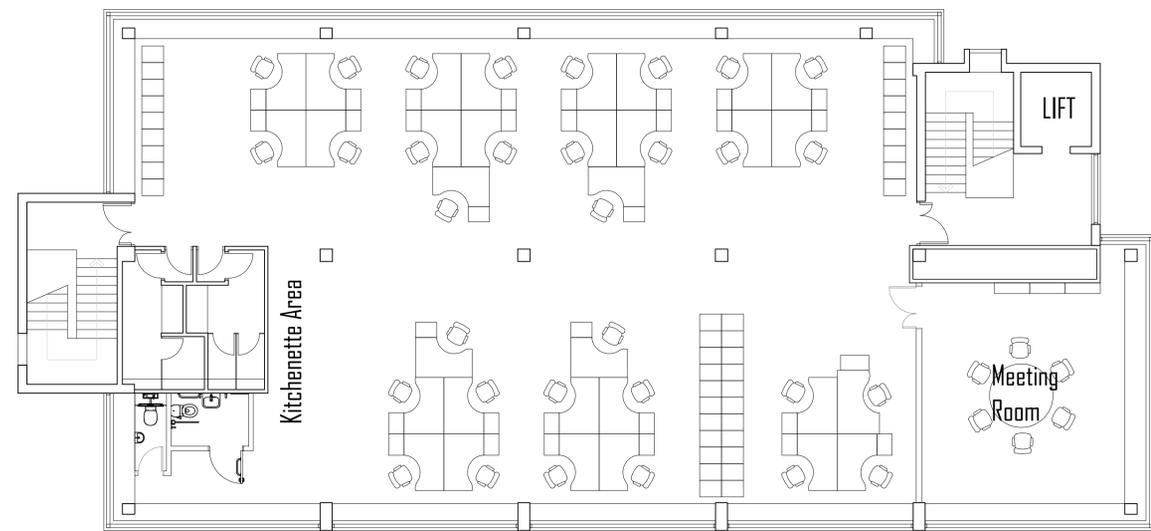
ROOF PLAN

Scale 1:100



FIRST FLOOR PLAN

Scale 1:100



SECOND FLOOR PLAN

Scale 1:100

The Anthony Smith Partnership
Construction Consultants
Crestfield House
111 Clarendon Hill
Reading
Berkshire
RG2 1YJ
Telephone 0118 980 111
Fax 0118 980 411

Client Wokingham Borough Council
Civic Offices
Wokingham
RG40 1BN

Project Refurbishment of Alexandra House
Denmark Street
Wokingham
RG40 2SL

Drawing Floor Plans (proposed)

Drawn By Psa **Date** January 2019

Scale 1:100 **DWG. No.** 4075.1003 PL

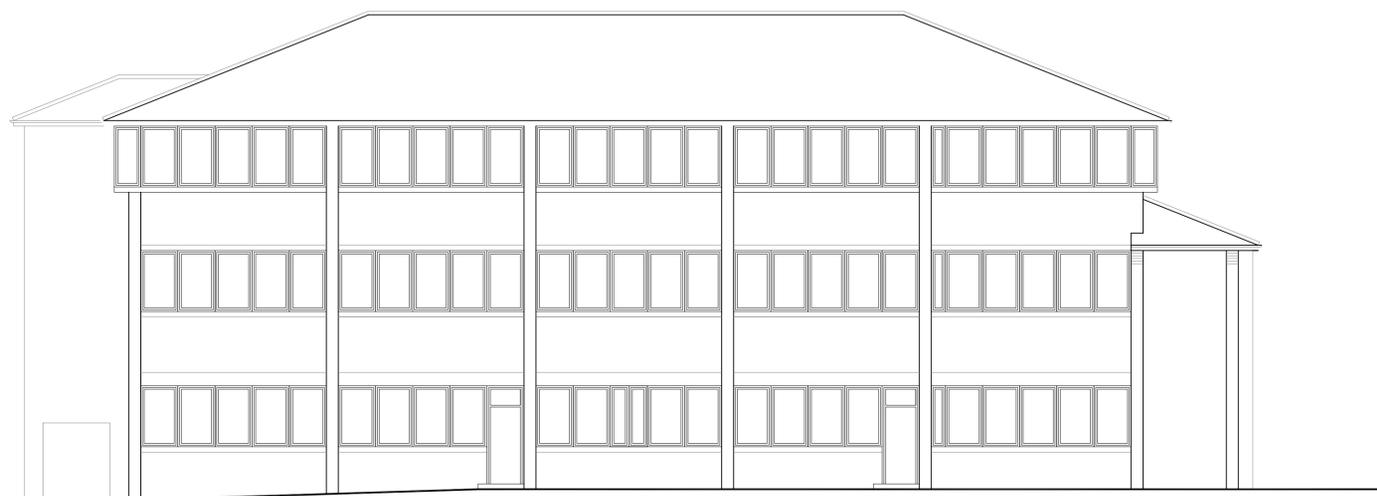
Cad Ref.

Date	Revisions	By
Jan 19	RT Issued for Review	
Jan 19	RS Issued for Review	
Jan 19	RS Issued for Review	
Jan 19	PLT Issued for Planning Application	

This page is intentionally left blank



WEST ELEVATION.
 Proposed

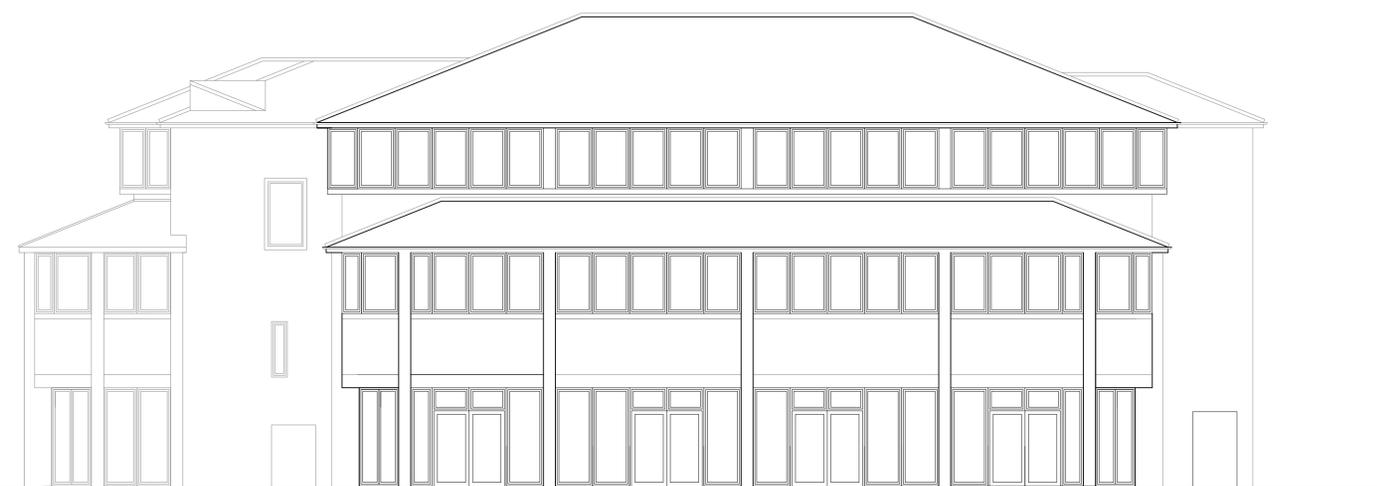


SOUTH ELEVATION
 Proposed



EAST ELEVATION.
 Proposed

Proposed new principal
 entrance doors to offices



NORTH ELEVATION
 Proposed

Proposed new entrance doors
 to retail outlets

Proposed new entrance doors
 to retail outlets

295

The Anthony Smith Partnership
 Construction Consultants
 13 Clarendon Hill
 Reading
 RG2 1YJ
 Telephone 0118 950 111
 Fax 0118 950 411

Client Wokingham Borough Council
 Civic Offices
 Wokingham
 RG40 1BN

Project Refurbishment of Alexandra House
 Denmark Street
 Wokingham
 RG40 2SL

Drawing
 Elevations (proposed)

Drawn By Psa **Date** January 2019

Scale 1:100 **DWG. No.** 4075.2002 PL

Rev	Revisions	Date
P1	Issued for Review	Jan 19
P1	Issued for Planning Application	Feb 19

This page is intentionally left blank